

ADDENDUM NO. 1

Date: May 24, 2024

To the Contract Documents for the construction of
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
ROGUE VALLEY INTERNATIONAL – MEDFORD AIRPORT
AIP NO. 3-41-0037-063-2024; CONTRACT NO: AIP-063-FY24

TO ALL PLANHOLDERS AND/OR PROSPECTIVE BIDDERS:

The following changes, additions, and/or deletions are hereby made a part of the Contract Documents for the construction of “TAXIWAY C EXTENSION AND NORTHWEST TAXILANES” dated 2024, as fully and completely as if the same were fully set forth therein.

DRAWINGS

- Sheet No. 1 – 71
 - Replace entire sheet set with the revised Sheet No. 1 – 71, labeled “*Addendum No. 1*”

GENERAL INFORMATION

All Bidders shall acknowledge receipt and acceptance of this Addendum No. 1 in the Proposal, or by submitting the Addendum with the bid package. Proposals submitted without Addendum acknowledgement, or without this Addendum, will be considered nonresponsive.



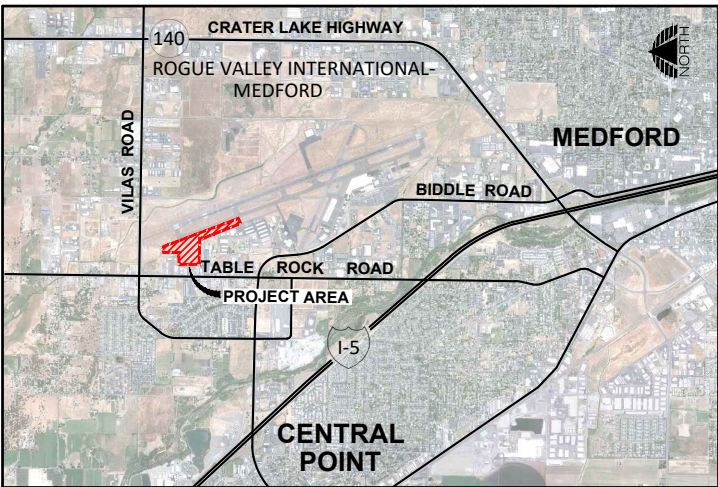
Josh Lekkerkerker, PE
Precision Approach Engineering, Inc.

ROGUE VALLEY INTERNATIONAL - MEDFORD

TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

AIP NO. 3-41-0037-063

SITE MAP



VICINITY MAP



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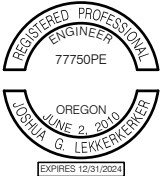
OWNER



JACKSON COUNTY - AIRPORT AUTHORITY
ROGUE VALLEY INTERNATIONAL - MEDFORD AIRPORT
1000 TERMINAL LOOP PARKWAY
MEDFORD, OREGON 97504

1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



1 ADDENDUM NO. 1 5/24/2024 JGL
REVISIONS: DATE APPD.

DATE: MAY 2024
DESIGN: KJK
DRAWN: JAW
CHECKED: JGL
REVISION NUMBER: 0

SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

COVER SHEET

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.

1
OF 71

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LEGEND

CIVIL SYMBOLS LEGEND

| EXISTING | NEW | |
|----------|-----|--|
| | | EDGE OF PAVEMENT |
| | | EDGE OF GRAVEL |
| | | CONTOUR |
| | | FENCE |
| | | STORM DRAIN |
| | | UNDER DRAIN |
| | | CULVERT |
| | | SANITARY SEWER |
| | | WATER |
| | | GAS |
| | | TELEPHONE |
| | | IRRIGATION |
| | | SLOPE SYMBOL |
| | | CATCH BASIN |
| | | STORM DRAIN MANHOLE |
| | | CLEANOUT |
| | | CULVERT END STRUCTURE |
| | | SURFACE FLOW DIRECTION |
| | | SANITARY MANHOLE |
| | | FIRE HYDRANT |
| | | WATER METER |
| | | WATER VAULT |
| | | WATER VALVE |
| | | IRRIGATION VALVE |
| | | TIE DOWN |
| | | SIGN |
| | | SURVEY CONTROL POINT |
| | | SURVEY FOUND MONUMENT |
| | | BRASS CAP |
| | | BENCHMARK |
| | | AIRPORT BEACON |
| | | SHRUBS |
| | | TREES |
| | | ABANDONED FEATURE LINE |
| | | DEMO FEATURE LINE |
| | | DEMO FEATURE LINE |
| | | SECURITY IDENTIFICATION DISPLAY AREA (SIDA) LINE |

ELECTRICAL LEGEND

| EXISTING | NEW | |
|----------|-----|--|
| | | HANDHOLE |
| | | POWER VAULT |
| | | DUCT MARKER |
| | | JUNCTION CAN |
| | | ELECTRICAL RISER |
| | | RUNWAY EDGE LIGHT AND THRESHOLD LIGHT C = CLEAR R = RED G = GREEN Y = YELLOW O = OPAQUE |
| | | RUNWAY LIGHT IN PAVEMENT (FLUSH MOUNT) |
| | | REIL (RUNWAY END IDENTIFIER LIGHT) |
| | | PAPI (PRECISION APPROACH PATH INDICATOR) |
| | | TAXIWAY LIGHT (BLUE LENS) |
| | | TAXIWAY LIGHT IN PAVEMENT (FLUSH MOUNT) |
| | | RETROREFLECTIVE MARKER |
| | | POWER CIRCUIT / OVERHEAD |
| | | POWER UNDERGROUND |
| | | POWER POLE |
| | | GUY WIRE |
| | | STREET LIGHT |
| | | GUIDANCE SIGN |

AVIATION LEGEND

| EXISTING | FUTURE | |
|----------|--------|---------------------------|
| | | OBJECT FREE AREA (OFA) |
| | | OBJECT FREE ZONE (OFZ) |
| | | OFZ-OFA |
| | | RUNWAY OBJECT FREE AREA |
| | | RUNWAY OBJECT FREE ZONE |
| | | RUNWAY PROTECTION ZONE |
| | | RUNWAY SAFETY AREA |
| | | TAXIWAY SAFETY AREA |
| | | TAXILANE OBJECT FREE AREA |
| | | TAXIWAY OBJECT FREE AREA |

GENERAL NOTES

CONSTRUCTION NOTES:

- CONTRACTOR SHALL SUBMIT SCHEDULE AND SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO OWNER IN ACCORDANCE WITH THE SPECIFICATIONS. WORK SHALL NOT START UNTIL ACCEPTANCE OF THE SCHEDULE, SPCD, AND ISSUANCE OF "NOTICE-TO-PROCEED" (NTP) BY THE OWNER.
- DAMAGE TO HAUL ROUTES AND EXISTING FACILITIES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL COMPLETE THE WORK WITHIN THE NUMBER OF CALENDAR DAYS FROM THE NTP AS SPECIFIED IN THE BIDDER'S PROPOSAL.
- THE CONTRACTOR SHALL OBTAIN PERMITS AND OWNER SHALL PAY PERMIT FEES ASSOCIATED WITH IMPROVEMENTS.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY ALL UTILITY COMPANIES AND AIRPORT OPERATIONS TO COORDINATE UTILITY LOCATES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL UTILITIES AND REPLACING UTILITIES DAMAGED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SURVEY WORK REQUIRED FOR THE CONSTRUCTION OF THE PROJECT AND TO ENSURE CONSTANT GRADE ACROSS IMPROVEMENTS TO MAINTAIN DRAINAGE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO WORK IMPROVEMENTS WHEN IN DOUBT ABOUT GRADES AND DRAINAGE. SEE GENERAL REQUIREMENTS.
- CONTRACTOR SHALL BE RESPONSIBLE TO FIELD VERIFY CONDUIT AND CABLE ROUTING PRIOR TO INSTALLATION. TRACING OF EXISTING LINES MAY BE REQUIRED.
- CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN AND CLEAN STREETS AND PAVEMENT OPEN TO TRAFFIC, KEEP FREE OF SOIL OR OTHER DEBRIS FROM CONSTRUCTION, AND REPAIR ALL DAMAGE TO STREETS AND OTHER FACILITIES RESULTING FROM CONSTRUCTION. CLEANING AND REPAIR SHALL BE MADE IN A TIMELY MANNER AS DETERMINED BY THE ENGINEER.
- CONTRACTOR SHALL RECORD ALL CONSTRUCTION CHANGES WHICH DEVIATE FROM THE PLANS AND PROVIDE THE OWNER WITH A CLEARLY MARKED SET OF RECORD DRAWINGS AT THE COMPLETION OF THE PROJECT.

CONSTRUCTION FUNDING:

- EXECUTION OF ALL WORK IN THIS CONTRACT IS CONTINGENT UPON JACKSON COUNTY'S RECEIPT OF FEDERAL FUNDS. THESE FUNDS WILL NOT BE ASSURED UNTIL A GRANT IS EXECUTED BETWEEN THE FAA AND JACKSON COUNTY AFTER THE CONTRACT BID OPENING. GRANT EXECUTION IS EXPECTED TO OCCUR IN THE SUMMER/FALL OF 2024. OWNER SHALL HAVE THE FINAL AUTHORITY REGARDING START DATE OF CONSTRUCTION.
- ELEMENTS OF WORK ARE AIRPORT IMPROVEMENT PROGRAM (AIP) ELIGIBLE UNLESS OTHERWISE NOTED.

CONSTRUCTION SCHEDULE:

- THE CONTRACTOR SHALL PREPARE AND SUBMIT A "CONSTRUCTION SCHEDULE" TO THE ENGINEER AND OWNER FOR REVIEW A MINIMUM OF 14 DAYS PRIOR TO STARTING CONSTRUCTION OR PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, WHICHEVER OCCURS EARLIER. THE PLAN MUST BE ACCEPTABLE TO THE OWNER PRIOR TO BEGINNING WORK.
- THE CONSTRUCTION SCHEDULE SHALL BE REVISED ANY TIME AN ELEMENT OF WORK DOES NOT COINCIDE WITH THE INITIAL TIME LINE PRESENTED, OR IF REQUESTED BY THE ENGINEER OR OWNER.
- ALL MATERIAL SUBMITTALS SHALL BE SUBMITTED, REVIEWED AND RETURNED WITH "NO EXCEPTIONS NOTED" PRIOR TO USE IN CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE REQUIRED INFORMATION WITH APPROPRIATE TIME PRIOR TO THE DESIRED USE.

WEEKLY CONSTRUCTION MEETING:

- THE CONTRACTOR, ENGINEER, OWNER, OWNER'S REPRESENTATIVE, AND OTHER INVITED REPRESENTATIVES OR GUESTS WILL CONDUCT A WEEKLY CONSTRUCTION MEETING TO DISCUSS PROJECT ACTIVITIES. THE MEETING SHALL BE USED TO DISCUSS CONSTRUCTION ISSUES, PROJECT SCHEDULE, AND AIRCRAFT OPERATIONS DURING CONSTRUCTION AND OTHER AGENDA ITEMS RELATED TO THE PROJECT. SPECIFIC ACTIVITIES ANTICIPATED FOR THE FOLLOWING WEEK SHALL BE DISCUSSED, PARTICULARLY THOSE IMPACTING AIRPORT OPERATIONS AND SAFETY. A TWO-WEEK LOOK AHEAD SHALL ALSO BE PROVIDED.

CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICER:

- THE CONTRACTOR SHALL HAVE A FULL TIME SUPERINTENDENT ON THE PROJECT SITE ANY TIME WORK IS BEING ACCOMPLISHED ON THE AIRPORT. THIS INCLUDES WORK BEING PERFORMED BY SUBCONTRACTORS OR SERVICE PROVIDERS, UNLESS OTHERWISE APPROVED BY THE OWNER.
- THE CONTRACTOR SHALL ASSIGN ONE PERSON TO BE THE SAFETY OFFICER DURING THE PROJECT. THE SAFETY OFFICER SHALL BE ONSITE AT ALL TIMES WHEN WORK IS OCCURRING IN AN ACTIVE AOA UNLESS OTHERWISE APPROVED BY THE OWNER. THE SAFETY OFFICER SHALL BE RESPONSIBLE FOR THE EDUCATION AND TRAINING OF ALL PERSONNEL WHO WILL ACCESS AIRPORT PROPERTY. THE OFFICER SHALL MAINTAIN RECORDS INDICATING THAT PERSONNEL HAVE BEEN TRAINED, AND ARE FAMILIAR WITH SAFETY RULES AND REGULATIONS RELATED TO OPERATIONS AT THE AIRPORT DURING CONSTRUCTION. PERSONNEL WHO VIOLATE SAFETY REQUIREMENTS MAY BE REMOVED FROM THE PROJECT AT THE DISCRETION OF THE OWNER. THE SAFETY OFFICER SHALL ALSO BE RESPONSIBLE FOR FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT. SEE CONSTRUCTION SAFETY AND PHASING PLAN FOR ADDITIONAL REQUIREMENTS.
- THE CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICER SHALL ATTEND THE WEEKLY CONSTRUCTION MEETING.

1 REPLACE ENTIRE SHEET

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BASE BID SCHEDULE

| BID ITEM | SPECIFICATION SECTION | DESCRIPTION | UNIT | BID QUANTITY | CONSTRUCTION QUANTITY |
|----------|-----------------------|--|------|--------------|-----------------------|
| 1 | E-020 | CONSTRUCTION SURVEY AND STAKING | LS | 1 | |
| 2 | E-020 | SECURITY BADGE | EA | 30 | |
| 3 | C-100 | CONTRACTOR QUALITY CONTROL PROGRAM (CQCP) | LS | 1 | |
| 4 | C-102 | INSTALLATION AND REMOVAL OF SILT FENCE | LF | 1,680 | |
| 5 | C-102 | FIBER MATTING | SY | 2,680 | |
| 6 | C-102 | TEMPORARY EROSION CONTROL | LS | 1 | |
| 7 | C-105 | MOBILIZATION | LS | 1 | |
| 8 | E-110 | CABLE REMOVAL | LF | 340 | |
| 9 | E-110 | CLEANOUT REMOVAL | EA | 1 | |
| 10 | E-110 | FENCE REMOVAL | LF | 52 | |
| 11 | E-110 | POROUS PAVER REMOVAL | SY | 160 | |
| 12 | E-110 | REMOVAL OF EXISTING EDGE LIGHT AND BASE CAN | EA | 2 | |
| 13 | E-110 | REMOVAL OF EXISTING EDGE LIGHT FIXTURE | EA | 1 | |
| 14 | E-110 | REMOVAL OF EXISTING GUIDANCE SIGN AND PANELS | EA | 1 | |
| 15 | E-110 | REMOVAL OF EXISTING GUIDANCE SIGN PANEL | EA | 3 | |
| 16 | E-110 | REMOVAL OF PIPES | LF | 170 | |
| 17 | E-110 | RETROREFLECTIVE MARKER REMOVAL | EA | 1 | |
| 18 | E-110 | TRASH RACK REMOVAL | EA | 2 | |
| 19 | E-180 | DRAINAGE GEOTEXTILE FABRIC | SY | 13,670 | |
| 20 | E-180 | RIPRAP GEOTEXTILE FABRIC | SY | 340 | |
| 21 | E-180 | SUBGRADE GEOTEXTILE FABRIC | SY | 13,670 | |
| 22 | ODOT 00390 | CLASS 50 RIPRAP PROTECTION | SY | 10 | |
| 23 | ODOT 00390 | CLASS 100 RIPRAP PROTECTION | SY | 340 | |
| 24 | ODOT 00390 | QUARRY SPALL PROTECTION | SY | 10 | |
| 25 | P-101 | ASPHALT PAVEMENT REMOVAL, FULL DEPTH | SY | 1,040 | |
| 26 | P-101 | REMOVAL OF FOREIGN SUBSTANCES/CONTAMINATES | SF | 300 | |
| 27 | P-152 | MUCK EXCAVATION | CY | 4,000 | |
| 28 | P-152 | STRUCTURAL FILL | CY | 4,000 | |
| 29 | P-152 | UNCLASSIFIED EXCAVATION | CY | 10,880 | |
| 30 | P-209 | CRUSHED AGGREGATE BASE COURSE | CY | 2,240 | |
| 31 | P-401 | ASPHALT SURFACE COURSE | TON | 3,330 | |
| 32 | P-407 | ASPHALT TREATED PERMEABLE BASE COURSE | TON | 3,990 | |
| 33 | P-603 | EMULSIFIED ASPHALT TACK COAT | TON | 3 | |
| 34 | P-620 | METHACRYLYTE MARKING, YELLOW, TWO COAT | SF | 1,100 | |
| 35 | P-620 | WATERBORNE MARKING, BLACK, ONE COAT | SF | 250 | |
| 36 | P-620 | WATERBORNE MARKING, YELLOW/WHITE, TWO COAT | SF | 3,320 | |
| 37 | F-162 | DOUBLE SWING GATE AND FENCELINE | LS | 1 | |
| 38 | D-701 | 6-INCH HDPE PIPE | LF | 30 | |
| 39 | D-701 | 12-INCH HDPE PIPE | LF | 200 | |
| 40 | D-701 | 12-INCH REINFORCED CONCRETE PIPE | LF | 210 | |
| 41 | D-701 | 18-INCH HDPE PIPE | LF | 330 | |
| 42 | D-701 | PIPE CONNECTION TO EXISTING STRUCTURE | EA | 1 | |
| 43 | D-705 | 6-INCH PERFORATED HDPE PIPE, NEW PAVEMENT AREA | LF | 310 | |
| 44 | D-705 | 6-INCH PERFORATED HDPE PIPE, NON-PAVED AREA | LF | 1,600 | |
| 45 | D-705 | UNDERDRAIN CONNECTION TO PIPE | EA | 3 | |
| 46 | D-751 | ADJUSTABLE POND INLET SYSTEM | EA | 1 | |
| 47 | D-751 | CATCH BASIN | EA | 4 | |
| 48 | D-751 | CLEANOUT | EA | 13 | |
| 49 | D-751 | CONCRETE DRAINAGE COLLAR | EA | 1 | |
| 50 | D-751 | CONCRETE PIPE SLOPED END | EA | 4 | |
| 51 | D-751 | CONTROL STRUCTURE TRASH RACK | EA | 2 | |
| 52 | D-751 | DRAINAGE INLET/OUTLET STRUCTURE | EA | 2 | |
| 53 | D-751 | DRAINAGE STRUCTURE ADJUSTMENT | EA | 2 | |
| 54 | D-751 | SOUTH TREATMENT LOW FLOW BYPASS SYSTEM | LS | 1 | |
| 55 | D-754 | CONCRETE PYLONS | EA | 18 | |
| 56 | D-754 | CONCRETE FLUME | LF | 90 | |
| 57 | D-754 | CONCRETE POND VALLEY GUTTER EXTENSION | SF | 2,400 | |
| 58 | D-754 | CONCRETE VALLEY GUTTER | SF | 1,100 | |
| 59 | D-754 | NORTH TREATMENT BYPASS CONTROL STRUCTURE | LS | 1 | |
| 60 | D-754 | NORTH TREATMENT FLOW CONTROL STRUCTURE | LS | 1 | |
| 61 | D-754 | POND BYPASS FLOW CONTROL STRUCTURE MODIFICATIONS | LS | 1 | |
| 62 | D-754 | SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS | LS | 1 | |
| 63 | T-901 | SEEDING | AC | 9 | |
| 64 | E-920 | WATER QUALITY HIGH FLOW MEDIA SYSTEM | SF | 1,320 | |
| 65 | T-905 | TOPSOIL | CY | 5,870 | |
| 66 | L-108 | NO. 8 AWG CABLE INSTALLED IN CONDUIT, BLACK | LF | 150 | |
| 67 | L-108 | NO. 8 AWG CABLE INSTALLED IN CONDUIT, TERRA COTTA | LF | 2,730 | |
| 68 | L-108 | NO. 8 AWG CABLE INSTALLED IN CONDUIT, GRAY | LF | 140 | |

BASE BID SCHEDULE (cont.)

| BID ITEM | SPECIFICATION SECTION | DESCRIPTION | UNIT | BID QUANTITY | CONSTRUCTION QUANTITY |
|----------|-----------------------|--|------|--------------|-----------------------|
| 69 | L-110 | 2-INCH PVC CONDUIT | LF | 2,930 | |
| 70 | L-110 | CONCRETE ENCASEMENT OF DUCT | LF | 300 | |
| 71 | L-110 | ELECTRICAL TRENCH, NEW PAVEMENT AREA | LF | 2,340 | |
| 72 | L-110 | ELECTRICAL TRENCH, NON-PAVED AREA | LF | 230 | |
| 73 | E-112 | DIRECTIONAL BORE | LS | 1 | |
| 74 | L-115 | HANDHOLE | EA | 2 | |
| 75 | L-115 | JUNCTION CAN | EA | 2 | |
| 76 | L-115 | INSTALL SOLID LID ON EXISTING BASE CAN | EA | 1 | |
| 77 | L-125 | RETROREFLECTIVE TAXIWAY END SIGN | EA | 1 | |
| 78 | L-125 | TAXIWAY EDGE LIGHT ON NEW BASE CAN | EA | 35 | |
| 79 | L-125 | TAXIWAY FLUSH-MOUNTED EDGE LIGHT ON NEW BASE CAN | EA | 1 | |
| 80 | L-125 | TAXIWAY GUIDANCE SIGN ON EXISTING FOUNDATION | EA | 1 | |
| 81 | L-125 | TAXIWAY GUIDANCE SIGN PANEL REPLACEMENT | EA | 3 | |
| 82 | L-125 | TAXIWAY GUIDANCE SIGN, 3 MODULE | EA | 1 | |

ADDITIVE BID SCHEDULE

| BID ITEM | SPECIFICATION SECTION | DESCRIPTION | UNIT | BID QUANTITY | CONSTRUCTION QUANTITY |
|----------|-----------------------|---|------|--------------|-----------------------|
| A-1 | E-020 | CONSTRUCTION SURVEY AND STAKING | LS | 1 | |
| A-2 | C-100 | CONTRACTOR QUALITY CONTROL PROGRAM (CQCP) | LS | 1 | |
| A-3 | C-102 | INSTALLATION AND REMOVAL OF STRAW WATTLE | LF | 810 | |
| A-4 | C-102 | TEMPORARY EROSION CONTROL | LS | 1 | |
| A-5 | C-105 | MOBILIZATION | LS | 1 | |
| A-6 | E-110 | CABLE REMOVAL | LF | 1,780 | |
| A-7 | E-110 | RETROREFLECTIVE MARKER REMOVAL | EA | 21 | |
| A-8 | E-180 | SUBGRADE GEOTEXTILE FABRIC | SY | 3,100 | |
| A-9 | P-101 | ASPHALT PAVEMENT REMOVAL, FULL DEPTH | SY | 500 | |
| A-10 | P-101 | ASPHALT PAVEMENT REMOVAL, PARTIAL DEPTH | SY | 4,110 | |
| A-11 | P-101 | PAVEMENT CRACK SEALING | LF | 3,000 | |
| A-12 | P-152 | MUCK EXCAVATION | CY | 400 | |
| A-13 | P-152 | STRUCTURAL FILL | CY | 400 | |
| A-14 | P-152 | UNCLASSIFIED EXCAVATION | CY | 1,950 | |
| A-15 | P-154 | SUBBASE COURSE | CY | 1,060 | |
| A-16 | P-209 | CRUSHED AGGREGATE BASE COURSE | CY | 540 | |
| A-17 | P-401 | ASPHALT SURFACE COURSE | TON | 1,300 | |
| A-18 | P-603 | EMULSIFIED ASPHALT TACK COAT | TON | 3 | |
| A-19 | P-620 | METHACRYLYTE MARKING, YELLOW, TWO COAT | SF | 420 | |
| A-20 | P-620 | WATERBORNE MARKING, BLACK, ONE COAT | SF | 50 | |
| A-21 | P-620 | WATERBORNE MARKING, YELLOW/WHITE, TWO COAT | SF | 2,170 | |
| A-22 | D-701 | 6-INCH HDPE PIPE | LF | 60 | |
| A-23 | D-705 | 6-INCH PERFORATED HDPE PIPE, EXISTING PAVEMENT AREA | LF | 190 | |
| A-24 | D-705 | 6-INCH PERFORATED HDPE PIPE, NON-PAVED AREA | LF | 1,990 | |
| A-25 | D-705 | UNDERDRAIN CONNECTION TO PIPE | EA | 3 | |
| A-26 | D-751 | CATCH BASIN | EA | 1 | |
| A-27 | D-751 | CLEANOUT | EA | 7 | |
| A-28 | T-901 | SEEDING | AC | 1 | |
| A-29 | T-905 | TOPSOIL | CY | 500 | |
| A-30 | L-108 | NO. 8 AWG CABLE INSTALLED IN CONDUIT, TERRA COTTA | LF | 2,540 | |
| A-31 | L-110 | 2-INCH PVC CONDUIT | LF | 510 | |
| A-32 | L-110 | CONCRETE ENCASEMENT OF DUCT | LF | 50 | |
| A-33 | L-110 | ELECTRICAL TRENCH, EXISTING PAVEMENT AREA | LF | 40 | |
| A-34 | L-110 | ELECTRICAL TRENCH, NEW PAVEMENT AREA | LF | 480 | |
| A-35 | L-125 | TAXIWAY EDGE LIGHT ON NEW BASE CAN | EA | 12 | |
| A-36 | L-125 | TAXIWAY FLUSH-MOUNTED EDGE LIGHT ON NEW BASE CAN | EA | 3 | |
| A-37 | L-125 | SALVAGED RETROREFLECTIVE MARKER PLACEMENT | EA | 6 | |

1 REPLACE ENTIRE SHEET



1

ADDENDUM NO. 1

5/24/2024

JGL

REVISIONS:

DATE

APPD.

DATE:

MAY 2024

DESIGN:

KJK

DRAWN:

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CHECKED:

JGL

REVISION NUMBER:

0

SCALE:

AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

BID SCHEDULE

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

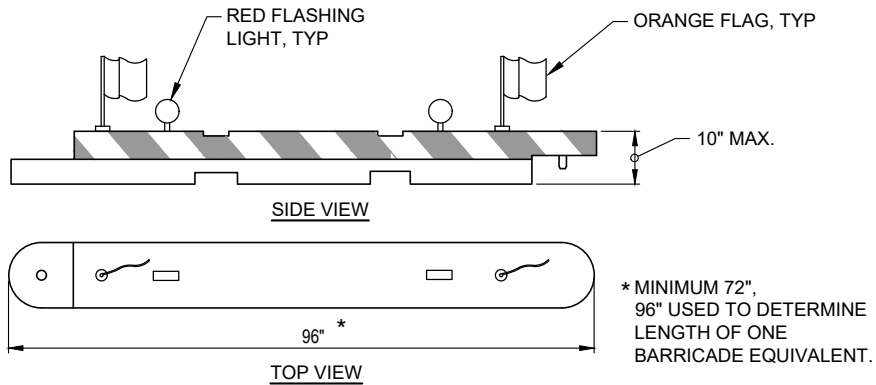
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CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES:

1. THE CONSTRUCTION SAFETY AND PHASING PLAN DRAWINGS SUPPLEMENT THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) TEXT AND OTHER REQUIREMENTS CONTAINED WITHIN THE CONTRACT DOCUMENT.
2. EXCEPT FOR NOTED CLOSURES, ACTIVE AIRPORT AND AIR OPERATION AREAS (AOA) TO REMAIN OPEN TO AIRCRAFT OPERATIONS DURING ENTIRE COURSE OF WORK. SEE SPECIFICATIONS AND CONSTRUCTION SAFETY AND PHASING PLAN TEXT FOR ADDITIONAL REQUIREMENTS, CONTRACTOR SHALL COORDINATE WORK TO ASSURE MINIMUM INCONVENIENCE TO AIRPORT OPERATIONS. IN ALL CIRCUMSTANCES SAFETY SHALL TAKE PRECEDENCE.
3. THE CONTRACTOR SHALL NOTIFY THE ENGINEER WITH REQUESTS FOR NOTICES TO AIRMEN (NOTAMS) A MINIMUM OF 48 HOURS PRIOR TO IMPLEMENTATION. CONTRACTOR SHALL COORDINATE WITH THE ENGINEER FOR CONSTRUCTION RELATED AOA CLOSURES. CLOSURES ARE SUBJECT TO APPROVAL BY THE OWNER. SEE CONSTRUCTION OPERATIONS AND SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
4. CONTRACTOR SHALL DELINEATE LOCATION OF THE TOFA AT TRAFFIC ACCESS POINTS AND PROVIDE OTHER AIRFIELD DELINEATION TO SEPARATE CONSTRUCTION ACTIVITIES FROM AIRPORT OPERATIONS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER. CONES, STAKES, OR OTHER METHODS, AS APPROVED BY THE ENGINEER, MAY BE USED OUTSIDE THE TOFA.
5. BARRICADES, LIGHTS AND OTHER CONSTRUCTION CONTROL DEVICES FURNISHED, PLACED AND MAINTAINED BY THE CONTRACTOR SHALL BE PROVIDED AT VARIOUS LOCATIONS AS NECESSARY TO ADEQUATELY SEPARATE CONSTRUCTION ACTIVITIES FROM ACTIVE AIRPORT OPERATIONS AREAS (AOA). BARRICADES SHOWN ON THE DRAWINGS ARE FOR REFERENCE AND THE NUMBER AND LOCATION OF BARRICADES MAY CHANGE TO MEET SAFETY REQUIREMENTS. SEE CONSTRUCTION OPERATIONS AND SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
6. LOCATION OF CONTRACTOR'S STAGING AND STOCKPILE AREAS ARE APPROXIMATE. VERIFY LIMITS AND LOCATION WITH ENGINEER PRIOR TO MOBILIZATION. ALL VEHICLES NOT ESSENTIAL FOR CONSTRUCTION, INCLUDING CONTRACTOR-EMPLOYEE VEHICLES, SHALL REMAIN OUTSIDE THE SECURITY FENCE. CONSTRUCTION EQUIPMENT PARKING SHALL BE CONFINED TO THE CONTRACTOR'S STAGING AREA OR THE WORK AREA. VERIFY ALL EQUIPMENT PARKING NOT WITHIN THE DESIGNATED STAGING AREA WITH ENGINEER.
7. PRIOR TO REOPENING A CLOSED AREA FOR OPERATIONS THE CONTRACTOR SHALL PROVIDE ADEQUATE TIME FOR THE ENGINEER OR OWNER TO INSPECT FOR CLEANLINESS AND CONFORMANCE TO REGULATIONS INCLUDING GRADING REQUIREMENTS OF THE ASSOCIATED SAFETY AREAS. THE CONTRACTOR SHALL ALSO VERIFY THAT AIRFIELD LIGHTING AND OTHER NAVAIDS ARE OPERATING PROPERLY. THE TIME NEEDED FOR INSPECTION AND POSSIBLE NECESSARY CORRECTIVE ACTION SHALL BE INCLUDED WITHIN THE ALLOWED CLOSURE PERIOD.
8. AIRFIELD SECURITY, INCLUDING FENCING AND GATES, SHALL BE MAINTAINED AT ALL TIMES; SEE CSPP TECHNICAL DOCUMENT FOR ADDITIONAL REQUIREMENTS. CONTRACTOR SHALL NOT BLOCK VEHICLE ACCESS ROADS OR GATES AT ANY TIME. GATE GUARD REQUIRED ANYTIME GATE REMAINS OPEN DUE TO CONSTRUCTION.
9. CONTRACTOR SHALL PROVIDE CONTINUOUS VEHICLE ROUTING, MEETING OWNER PROVIDED REQUIREMENTS, AROUND OR THROUGH WORK AREAS AT ALL TIMES, AS APPROVED BY THE ENGINEER. THIS MAY REQUIRE TEMPORARY DELINEATION, SIGNAGE, AND ROADWAY CONSTRUCTION.
10. CONTRACTOR SHALL MAKE AVAILABLE AT ALL TIMES A VACUUM SWEEPER TRUCK AND/OR OTHER ENGINEER-APPROVED EQUIPMENT AS REQUIRED TO KEEP ALL ON-AIRPORT CONSTRUCTION ROUTES CLEAN AND FREE OF FOD. IT IS ANTICIPATED THAT CONTINUOUS MONITORING OF SURFACES WITHIN, AND ADJACENT TO WORK AREAS WILL BE REQUIRED DURING CONSTRUCTION.
11. UNLESS OTHERWISE APPROVED BY THE OWNER, CONTRACTOR ACCESS TO THE PROJECT SITE SHALL BE LIMITED TO DESIGNATED LOCATIONS AND ROUTES SHOWN ON THE PLANS. SEE CONSTRUCTION SAFETY AND PHASING PLAN, AND EROSION AND SEDIMENT CONTROL PLAN FOR SPECIFIC RESTRICTIONS.
12. ALL STAGING AREAS AND HAUL ROUTES, INCLUDING PUBLIC ROADS USED FOR ACCESS, SHALL BE MAINTAINED DURING CONSTRUCTION AND SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION AT THE COMPLETION OF PROJECT WORK.
13. CONTRACTOR SHALL USE STEEL PLATES, LIGHTER LOADS, LIGHTER EQUIPMENT, OR OTHER APPROVED METHODS TO PROTECT EXISTING PAVEMENTS. CONTRACTOR SHALL REPAIR, BY METHOD APPROVED BY ENGINEER AND OWNER, AT CONTRACTOR'S EXPENSE, ALL DAMAGE TO EXISTING PAVEMENTS RESULTING FROM CONSTRUCTION ACTIVITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY EXISTING PAVEMENT CONDITIONS WITH ENGINEER AND OWNER PRIOR TO CONSTRUCTION AND TO DEVELOP AN OWNER-APPROVED PLAN TO PROTECT EXISTING PAVEMENTS FROM DAMAGE DUE TO CONSTRUCTION ACTIVITIES.
14. WORK AREAS SHOWN IN THE CSPP ARE APPROXIMATE. SEE APPROPRIATE DRAWING FOR SPECIFIC WORK LIMITS. CONTRACTOR SHALL COORDINATE ALL WORK ELEMENTS COMPATIBLE WITH INTENDED PHASE UNLESS OTHERWISE APPROVED BY THE ENGINEER.
15. TRAFFIC ROUTES SHOWN ARE APPROXIMATE, AND ARE NOT LIMITED TO LOCATIONS SHOWN. CONTRACTOR SHALL REMAIN CLEAR OF AIRCRAFT AND AIRPORT OPERATIONS AT ALL TIMES.
16. SEQUENCING OF PHASE NUMBERS IS FOR GENERAL INFORMATION ONLY. ACTUAL SEQUENCE OF CONSTRUCTION PHASES WILL VARY BASED ON AIRPORT DEMAND AND WILL BE DETERMINED SOLELY BY THE OWNER AFTER BID OPENING. PHASE 1 IS THE ANTICIPATED START.
17. ALL PORTIONS OF WORK SHOWN ON THE CSPP NOT COVERED BY PAYMENT UNDER A SPECIFIC BID ITEM OR LISTED AS INCIDENTAL TO A BID ITEM SHALL BE CONSIDERED INCIDENTAL TO THE MOBILIZATION BID ITEM.



NOTES:

1. FOR DELINEATION ON PAVEMENT BETWEEN ACTIVE AIRCRAFT OPERATIONS AREAS AND CONSTRUCTION SITES.
2. BARRICADE COLORS SHALL ALTERNATE ORANGE AND WHITE.

| | |
|------------------|---|
| BARRICADE DETAIL | 1 |
| NTS | 5 |

1 REPLACE ENTIRE SHEET

PRINT IN COLOR

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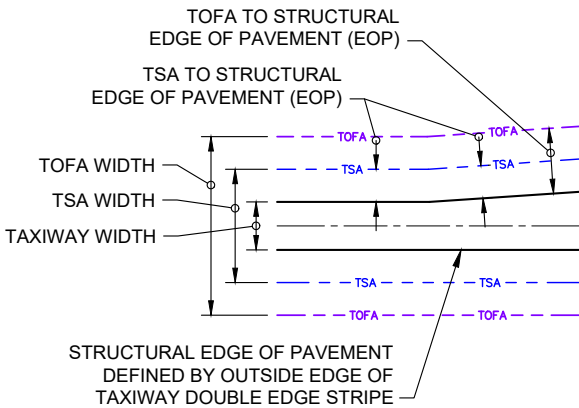
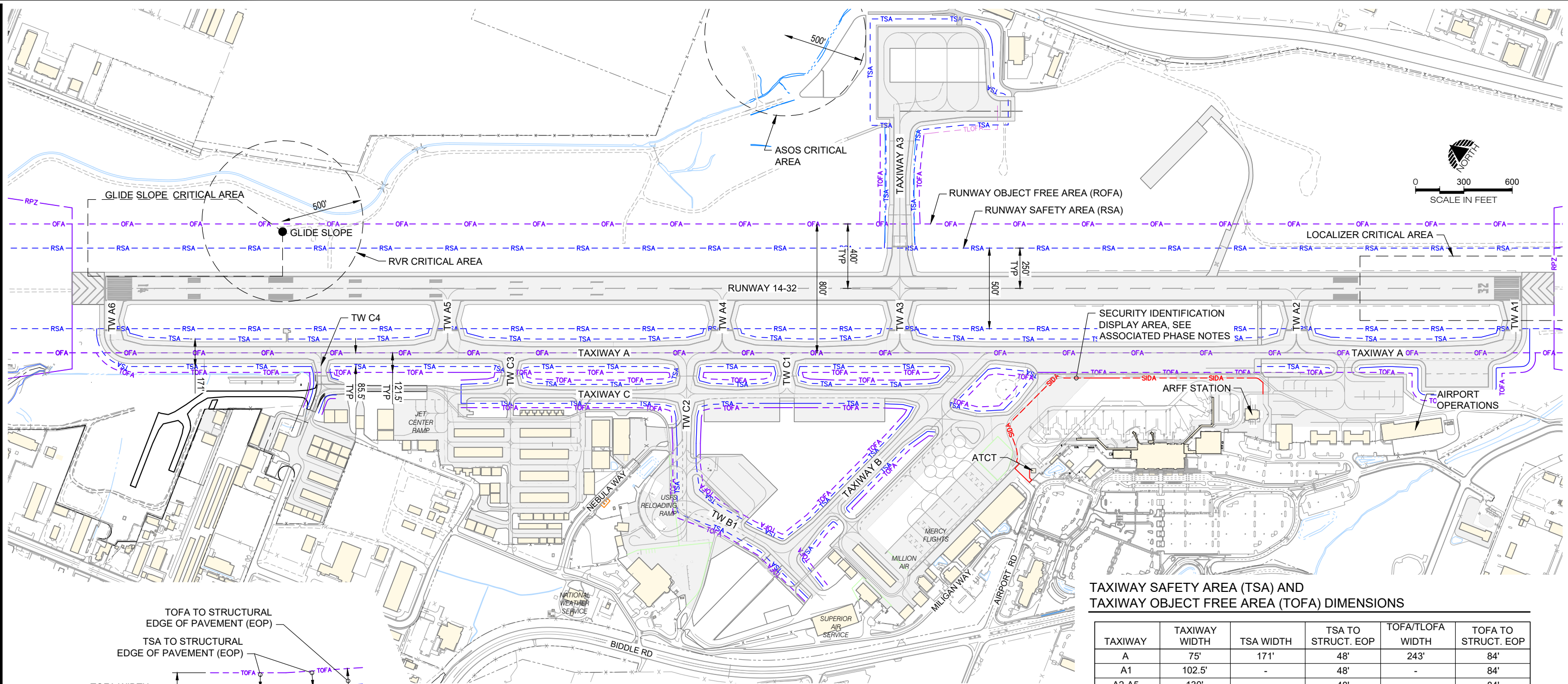
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| DRAWN: | JAW |
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| SCALE: | AS SHOWN |

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| ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES |
| CONSTRUCTION SAFETY AND PHASING PLAN - NOTES |
| PRECISION APPROACH ENGINEERING, INC. |
| AIP NO. 3-41-0037-063 |

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| PROJECT NUMBER: RVI-2020-W04 |
| SHEET NO. 5 |
| OF 71 |

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CRITICAL AREA DETAIL

NTS

1
6

TAXIWAY SAFETY AREA (TSA) AND
TAXIWAY OBJECT FREE AREA (TOFA) DIMENSIONS

| TAXIWAY | TAXIWAY WIDTH | TSA WIDTH | TSA TO STRUCT. EOP | TOFA/TLOFA WIDTH | TOFA TO STRUCT. EOP |
|----------|---------------|-----------|--------------------|------------------|---------------------|
| A | 75' | 171' | 48' | 243' | 84' |
| A1 | 102.5' | - | 48' | - | 84' |
| A2-A5 | 130' | - | 48' | - | 84' |
| A6 | 102.5' | - | 48' | - | 84' |
| B | 50' | 118' | 34' | 171' | 60.5' |
| B1 | 50' | 118' | 34' | 171' | 60.5' |
| B2 | 40' | - | 34' | - | 60.5' |
| B3 | 55'-60' | - | 34' | - | 60.5' |
| C | 50' | 118' | 34' | 171' | 60.5' |
| C1-C2 | 50' | 118' | 34' | 171' | 60.5' |
| C3 | 35' | 79' | 22' | 124' | 44.5' |
| C4 | 35' | 79' | 22' | 124' | 44.5' |
| TAXILANE | 25' | 49' | - | 89'/79' | - |

NOTE: SEE CRITICAL AREA DETAIL 1 FOR DEFINITION OF WIDTHS

1
6
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ENGINEERING
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Corvallis, OR 97333
541•754•0043



1 ADDENDUM NO. 1

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

CONSTRUCTION SAFETY AND
PHASING PLAN - CRITICAL AREAS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

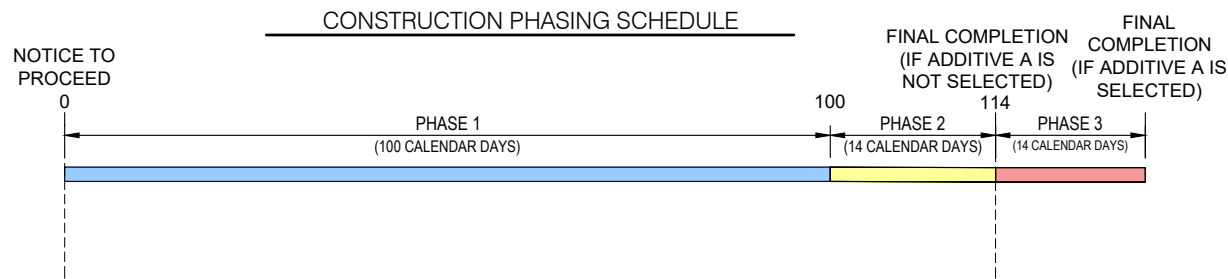
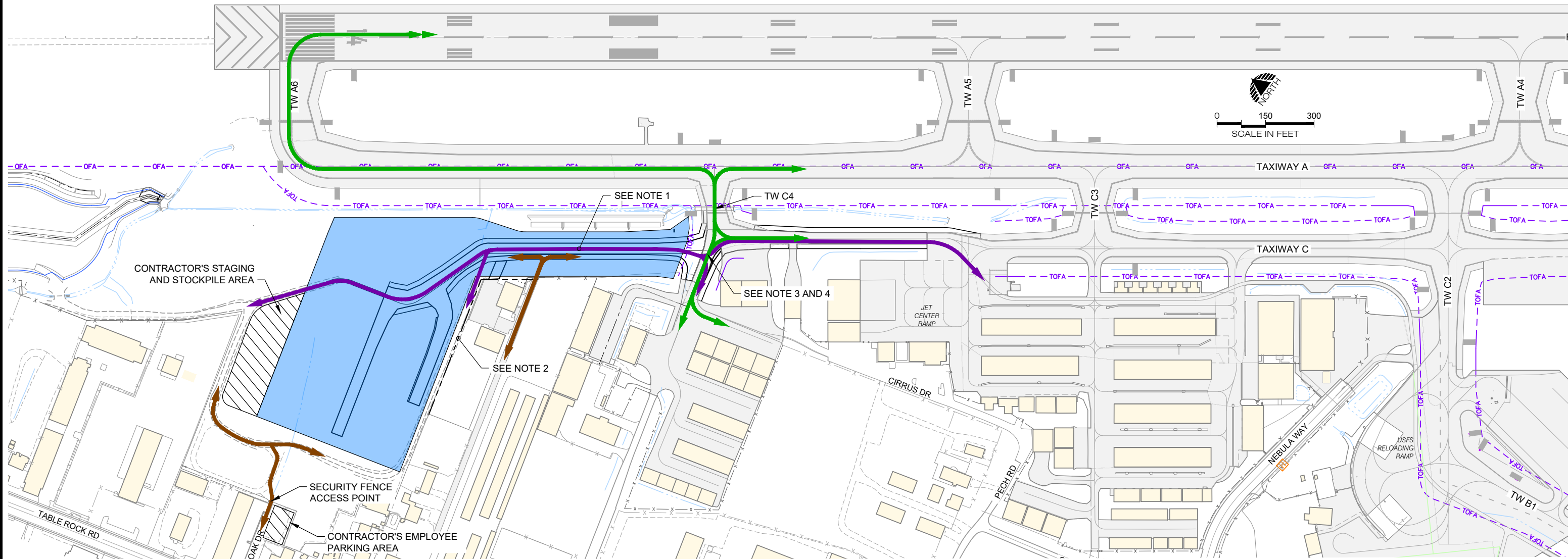
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PROJECT CONSTRUCTION TIME

| PHASE | AIR OPERATIONS AREA (AOA) IMPACT | ALLOWABLE DAILY WORK PERIOD | MAXIMUM NO. OF CALENDAR DAYS |
|-------|--|-----------------------------|------------------------------|
| 1 | NO AOA IMPACT | ANY * | 100 |
| 2 | TAXIWAY C AND C3 CLOSED | ANY * | 14 |
| 3 | TAXIWAY C BETWEEN TW B3 AND TW C4 CLOSED | ANY * | 14 |

* IF CONTRACTOR ELECTS TO PERFORM NIGHTTIME WORK, A WORK AREA LIGHTING PLAN SHALL BE SUBMITTED TO THE RPR. WORK LIGHTING PLAN SHALL INCLUDE AT A MINIMUM THE QUANTITY, LOCATION, AND LIGHT BEAM DIRECTION FOR ALL REQUIRED PROJECT LIGHTING. SEE CSPP TECHNICAL SECTION 2.21 FOR MORE INFORMATION.

PHASING LEGEND

- PHASE 1 WORK AREA
- PHASE 2 WORK AREA
- PHASE 3 WORK AREA
- AIRCRAFT TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
- CONSTRUCTION TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
- AIRPORT VEHICLE TRAFFIC, SEE GENERAL NOTE 15, SHEET 5
- AIRFIELD SIGN TO BE SCREENED, OR PARTIALLY SCREENED BY APPROVED METHODS PRIOR TO STARTING PHASE
- PORTABLE LOW PROFILE BARRICADES WITH RED LIGHTS TO BE PLACED BY THE CONTRACTOR DURING RUNWAY, TAXIWAY AND/OR OTHER WORK AREA CLOSURES FOR EACH PHASE OR AS DIRECTED BY THE ENGINEER. MAY REQUIRE ADJUSTMENT DURING CONSTRUCTION ACTIVITIES; SEE CONSTRUCTION SAFETY AND PHASING PLAN AND DETAIL
- REPLACE ENTIRE SHEET

PHASE 1 NOTES - 100 CALENDAR DAYS

- CONTRACTOR SHALL PROVIDE CONTINUOUS VEHICLE ACCESS ROUTE FOR AIRPORT VEHICLES THROUGH THE CONSTRUCTION ZONE DURING ALL PHASES OF THE WORK.
- AIRPORT PROPERTY LEASED TO TENANT, CONSTRUCTION ACCESS AND ACTIVITY SHALL BE LIMITED. COORDINATE WITH RPR PRIOR TO ENTERING LEASED SPACE.
- LIMITED CONSTRUCTION WITHIN TAXIWAY C4 TOFA MAY BE PERMITTED IF TAXIWAY C4 IS TEMPORARILY CLOSED FOR OPERATION. COORDINATE WITH RPR.
- CONSTRUCTION ACCESS WITHIN TAXIWAY A AND TAXIWAY C4 TOFA IS NOT PERMITTED WITHIN PHASE 1 WORK. AOA SHALL REMAIN OPEN.

PHASE 2 NOTES - 14 CALENDAR DAYS

- CONSTRUCTION ACCESS WITHIN TAXIWAY A TOFA IS NOT PERMITTED DURING PHASE 2. TAXIWAY A SHALL REMAIN OPEN AT ALL TIMES.
- AT THE COMPLETION OF PHASE 2. ALL PAVEMENTS SHOULD BE INSTALLED, ELECTRICAL SYSTEMS SHOULD BE OPERATIONAL, ALL DRAINAGE AND TREATMENT SYSTEMS SHOULD BE INSTALLED AND OPERATIONAL, PAVEMENT MARKING SHOULD BE PLACED, AND THE PROJECT SHOULD BE CONSIDERED SUBSTANTIALLY COMPLETE.

PHASE 3 - 14 CALENDAR DAYS

- PHASE 3 WORK AREA IS ONLY ALLOWED IF ADDITIVE A IS SELECTED.
- CONSTRUCTION TRAFFIC CROSSES ACTIVE TAXIWAY C4 DURING THIS PHASE. TRAFFIC SHALL YIELD TO AIRCRAFT AT ALL TIMES. FOD SHALL BE REMOVED AND CLEANED FROM ACTIVE SURFACE IMMEDIATELY UPON TRIGGERING EVENT.

PRINT IN COLOR

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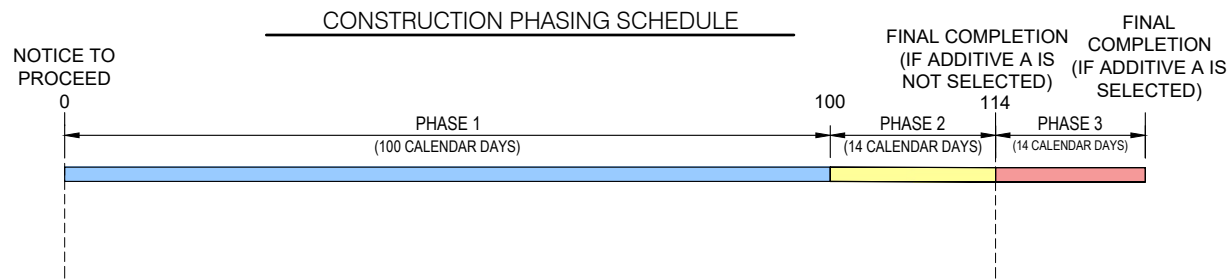
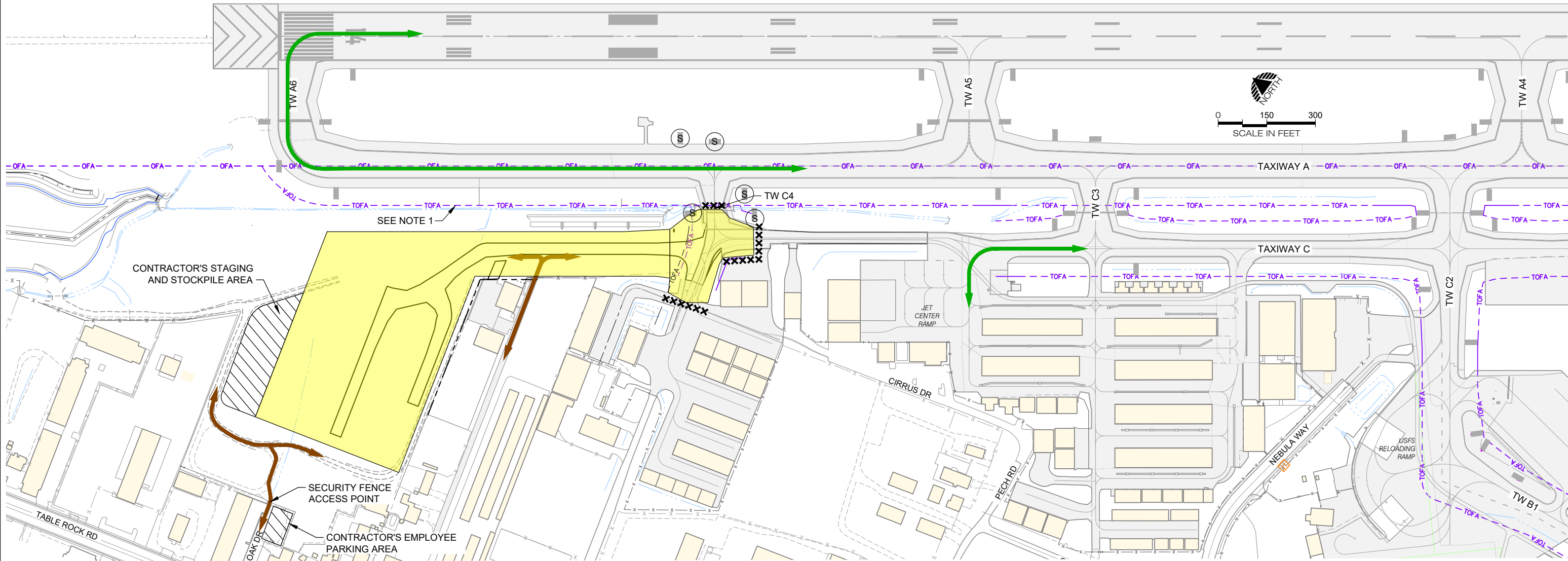
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 1
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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PROJECT CONSTRUCTION TIME

| PHASE | AIR OPERATIONS AREA (AOA) IMPACT | ALLOWABLE DAILY WORK PERIOD | MAXIMUM NO. OF CALENDAR DAYS |
|-------|--|-----------------------------|------------------------------|
| 1 | NO AOA IMPACT | ANY * | 100 |
| 2 | TAXIWAY C AND C3 CLOSED | ANY * | 14 |
| 3 | TAXIWAY C BETWEEN TW B3 AND TW C4 CLOSED | ANY * | 14 |

* IF CONTRACTOR ELECTS TO PERFORM NIGHTTIME WORK, A WORK AREA LIGHTING PLAN SHALL BE SUBMITTED TO THE RPR. WORK LIGHTING PLAN SHALL INCLUDE AT A MINIMUM THE QUANTITY, LOCATION, AND LIGHT BEAM DIRECTION FOR ALL REQUIRED PROJECT LIGHTING. SEE CSPP TECHNICAL SECTION 2.21 FOR MORE INFORMATION.

PHASING LEGEND

- PHASE 1 WORK AREA
- PHASE 2 WORK AREA
- PHASE 3 WORK AREA
- AIRCRAFT TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
- CONSTRUCTION TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
- AIRPORT VEHICLE TRAFFIC, SEE GENERAL NOTE 15, SHEET 5
- AIRFIELD SIGN TO BE SCREENED, OR PARTIALLY SCREENED BY APPROVED METHODS PRIOR TO STARTING PHASE
- PORTABLE LOW PROFILE BARRICADES WITH RED LIGHTS TO BE PLACED BY THE CONTRACTOR DURING RUNWAY, TAXIWAY AND/OR OTHER WORK AREA CLOSURES FOR EACH PHASE OR AS DIRECTED BY THE ENGINEER. MAY REQUIRE ADJUSTMENT DURING CONSTRUCTION ACTIVITIES; SEE CONSTRUCTION SAFETY AND PHASING PLAN AND DETAIL
- REPLACE ENTIRE SHEET

PHASE 1 NOTES - 100 CALENDAR DAYS

- CONTRACTOR SHALL PROVIDE CONTINUOUS VEHICLE ACCESS ROUTE FOR AIRPORT VEHICLES THROUGH THE CONSTRUCTION ZONE DURING ALL PHASES OF THE WORK.
- AIRPORT PROPERTY LEASED TO TENANT, CONSTRUCTION ACCESS AND ACTIVITY SHALL BE LIMITED. COORDINATE WITH RPR PRIOR TO ENTERING LEASED SPACE.
- LIMITED CONSTRUCTION WITHIN TAXIWAY C4 TOFA MAY BE PERMITTED IF TAXIWAY C4 IS TEMPORARILY CLOSED FOR OPERATION. COORDINATE WITH RPR.
- CONSTRUCTION ACCESS WITHIN TAXIWAY A AND TAXIWAY C4 TOFA IS NOT PERMITTED WITHIN PHASE 1 WORK. AOA SHALL REMAIN OPEN.

PHASE 2 NOTES - 14 CALENDAR DAYS

- CONSTRUCTION ACCESS WITHIN TAXIWAY A TOFA IS NOT PERMITTED DURING PHASE 2. TAXIWAY A SHALL REMAIN OPEN AT ALL TIMES.
- AT THE COMPLETION OF PHASE 2. ALL PAVEMENTS SHOULD BE INSTALLED, ELECTRICAL SYSTEMS SHOULD BE OPERATIONAL, ALL DRAINAGE AND TREATMENT SYSTEMS SHOULD BE INSTALLED AND OPERATIONAL, PAVEMENT MARKING SHOULD BE PLACED, AND THE PROJECT SHOULD BE CONSIDERED SUBSTANTIALLY COMPLETE.

PHASE 3 - 14 CALENDAR DAYS

- PHASE 3 WORK AREA IS ONLY ALLOWED IF ADDITIVE A IS SELECTED.
- CONSTRUCTION TRAFFIC CROSSES ACTIVE TAXIWAY C4 DURING THIS PHASE. TRAFFIC SHALL YIELD TO AIRCRAFT AT ALL TIMES. FOD SHALL BE REMOVED AND CLEANED FROM ACTIVE SURFACE IMMEDIATELY UPON TRIGGERING EVENT.

PRINT IN COLOR

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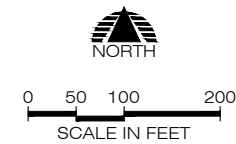
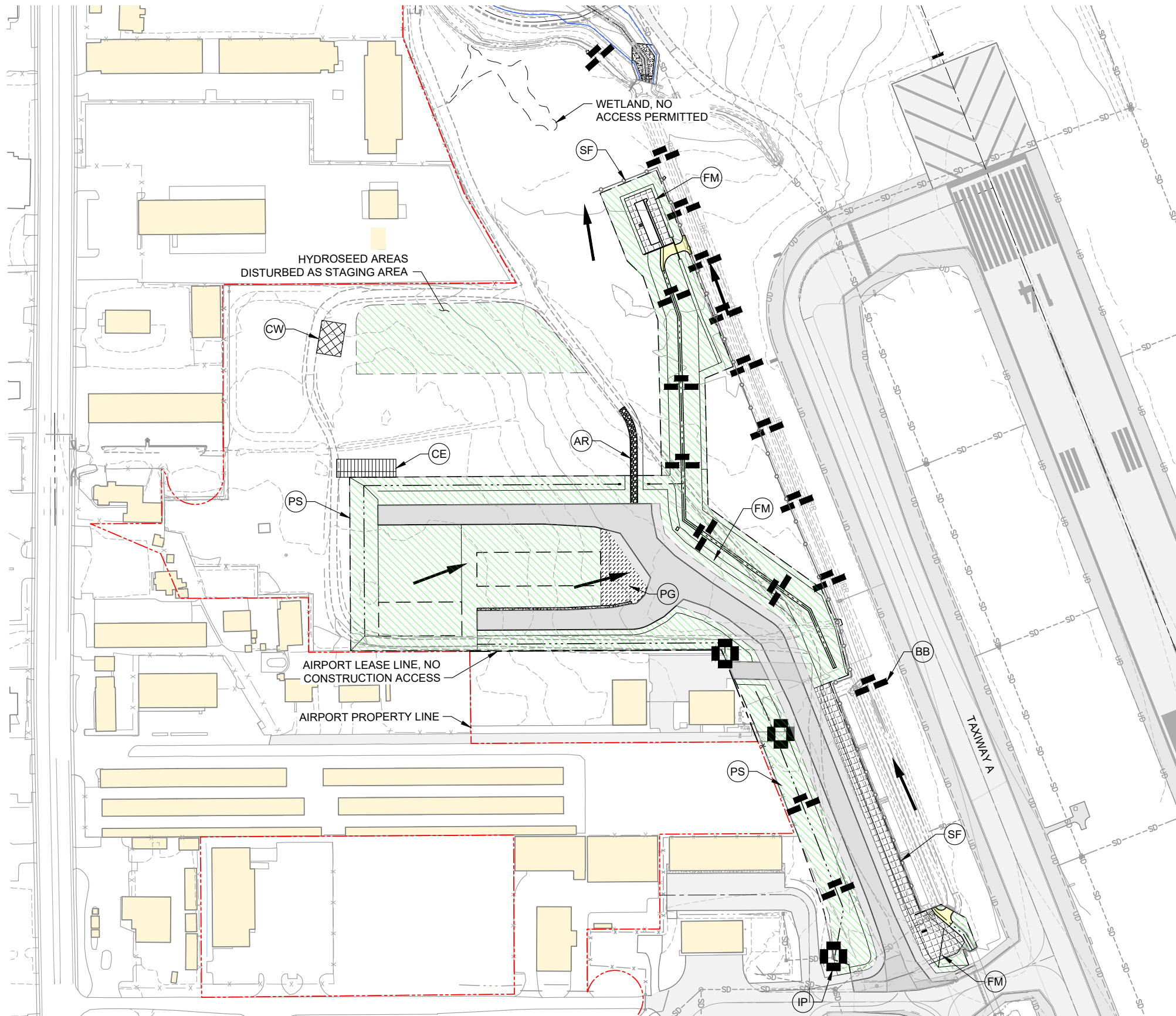
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DATE APPD.

DATE: MAY 2024
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REVISION NUMBER: 0
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 2
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
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LEGEND - STANDARD PRACTICE CODING SYSTEM:

| CODE | SYMBOL | |
|------|--------|---|
| BB | | BIOFILTER BAGS, SEE DETAIL ¹ / ₁₄ |
| IP | | STORM DRAIN INLET PROTECTION, SEE DETAIL ² / ₁₄ |
| SF | | SILT FENCE, SEE DETAIL ¹ / ₁₃ |
| PS | | TEMPORARY AND PERMANENT SEEDING |
| FM | | FIBER MATTING AND PERMANENT SEEDING, SEE SPECIFICATIONS |
| PG | | PERMANENT GRINDINGS, MIN. 6" DEPTH, SEE NOTE 6 |
| CE | | CONSTRUCTION ENTRANCE, SEE DETAIL ³ / ₁₄ |
| CW | | CONCRETE WASHOUT, SEE DETAIL ² / ₁₃ |
| AR | | RELOCATED AIRPORT ROADWAY |
| SW | | STRAW WATTLE, SE DETAIL ³ / ₁₃ |
| | | DIRECTION OF FLOW |
| | | SALVAGED EXISTING ROUND AGGREGATE, SEE NOTE 9 |
| | | RIPRAP PROTECTION, SEE PLANS |

NOTES

1. BIOFILTER BAGS SHOULD BE INSTALLED AS OUTLET PROTECTION ONCE NEW STORM OUTFALLS ARE INSTALLED, SEE DRAINAGE PLANS.
2. ADD BIOFILTER BAGS AND OTHER EROSION CONTROL FACILITIES AS REQUIRED OR DIRECTED BY THE RPR.
3. EROSION CONTROL ITEMS TO BE PLACED PRIOR TO CONSTRUCTION IN EACH WORK AREA. ADDITIONAL EROSION CONTROL ITEMS TO BE PLACED IN COMPLETED DITCHES AND SWALES AFTER THEIR COMPLETION.
4. HYDROSEEDING SHALL BE PERFORMED AT THE COMPLETION OF ALL WORK IN EACH CONSTRUCTION AREA.
5. EROSION CONTROL ITEMS TO BE REMOVED ONCE CORRESPONDING CONSTRUCTION WORK HAS BEEN COMPLETED AND THE SITE HAS BEEN STABILIZED WITH PERMANENT EROSION CONTROL MEASURES AS DETERMINED BY THE RPR.
6. GRINDINGS SHALL BE PLACED AT A MINIMUM OF 6" DEPTH AND COMPACTED TO A FIRM, UNYIELDING CONDITION, SUBJECT TO APPROVAL BY THE RPR.
7. PUBLIC RIGHTS-OF-WAY AND AIRCRAFT OPERATIONS AREAS SHALL BE KEPT IN A CLEAN AND SERVICEABLE CONDITION AT ALL TIMES. IN THE EVENT MATERIALS ARE INADVERTENTLY DEPOSITED ON ROADWAYS, TAXIWAYS OR APRONS, THE MATERIAL SHALL BE PROMPTLY REMOVED.
8. AREAS DISTURBED BY CONSTRUCTION ACTIVITY, INCLUDING STAGING AND DISPOSAL AREAS, SHALL BE COVERED BY HYDRAULICALLY APPLIED SEED AND TACKING AGENTS, VERIFY LOCATIONS WITH RPR, SEE SPECIFICATIONS. ANY AREAS DISTURBED BY THE CONTRACTOR OUTSIDE DISPOSAL AREAS, OR OUTSIDE OTHER AREAS AS INDICATED ON THE PLANS SHALL BE RESEEDD AT THE CONTRACTOR'S EXPENSE WITH T-901 HYDROSEEDING.
9. THE CONTRACTOR SHALL REMOVE AND STOCKPILE EXISTING ROUND AGGREGATE DURING GRADING ACTIVITIES. ROUND AGGREGATE SHALL BE REAPPLIED TO THE SURFACE AT A MINIMUM 4-INCH DEPTH.

PRINT IN COLOR

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1 REPLACE ENTIRE SHEET

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ENGINEERING
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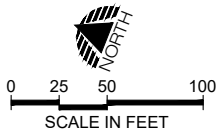
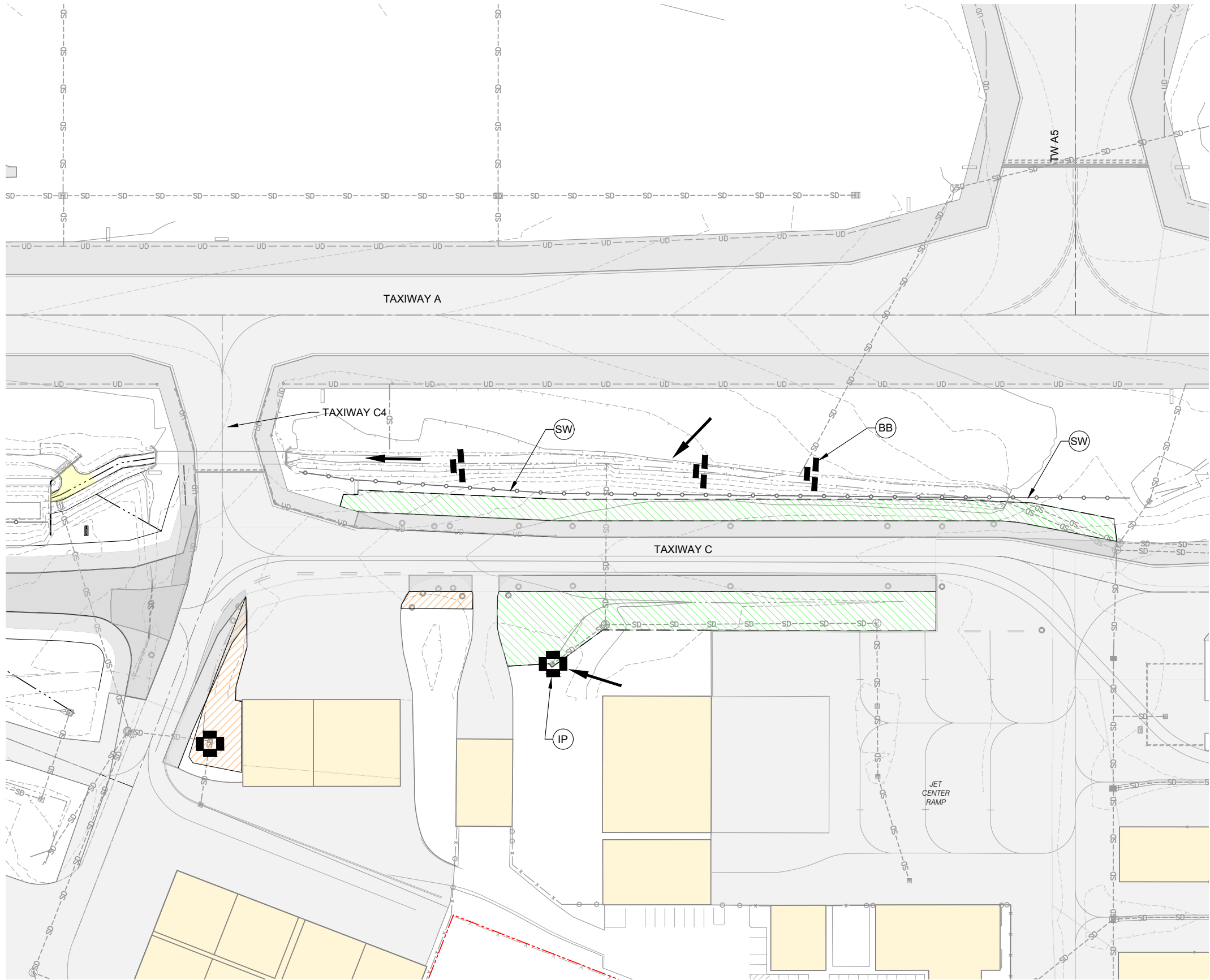
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REVISION NUMBER: 0
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
**EROSION AND SEDIMENT
CONTROL PLAN**
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
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LEGEND - STANDARD PRACTICE CODING SYSTEM:

| CODE | SYMBOL | |
|------|--------|---|
| (BB) | | BIOFILTER BAGS, SEE DETAIL ⁽¹⁾ / ₁₄ |
| (IP) | | STORM DRAIN INLET PROTECTION, SEE DETAIL ⁽²⁾ / ₁₄ |
| (SF) | | SILT FENCE, SEE DETAIL ⁽¹⁾ / ₁₃ |
| (PS) | | TEMPORARY AND PERMANENT SEEDING |
| (FM) | | FIBER MATTING AND PERMANENT SEEDING, SEE SPECIFICATIONS |
| (PG) | | PERMANENT GRINDINGS, MIN. 6" DEPTH, SEE NOTE 6 |
| (CE) | | CONSTRUCTION ENTRANCE, SEE DETAIL ⁽³⁾ / ₁₄ |
| (CW) | | CONCRETE WASHOUT, SEE DETAIL ⁽²⁾ / ₁₃ |
| (AR) | | RELOCATED AIRPORT ROADWAY |
| (SW) | | STRAW WATTLE, SE DETAIL ⁽³⁾ / ₁₃ |
| | | DIRECTION OF FLOW |
| | | SALVAGED EXISTING ROUND AGGREGATE, SEE NOTE 9 |
| | | RIPRAP PROTECTION, SEE PLANS |

NOTES

- BIOFILTER BAGS SHOULD BE INSTALLED AS OUTLET PROTECTION ONCE NEW STORM OUTFALLS ARE INSTALLED, SEE DRAINAGE PLANS.
- ADD BIOFILTER BAGS AND OTHER EROSION CONTROL FACILITIES AS REQUIRED OR DIRECTED BY THE RPR.
- EROSION CONTROL ITEMS TO BE PLACED PRIOR TO CONSTRUCTION IN EACH WORK AREA. ADDITIONAL EROSION CONTROL ITEMS TO BE PLACED IN COMPLETED DITCHES AND SWALES AFTER THEIR COMPLETION.
- HYDROSEEDING SHALL BE PERFORMED AT THE COMPLETION OF ALL WORK IN EACH CONSTRUCTION AREA.
- EROSION CONTROL ITEMS TO BE REMOVED ONCE CORRESPONDING CONSTRUCTION WORK HAS BEEN COMPLETED AND THE SITE HAS BEEN STABILIZED WITH PERMANENT EROSION CONTROL MEASURES AS DETERMINED BY THE RPR.
- GRINDINGS SHALL BE PLACED AT A MINIMUM OF 6" DEPTH AND COMPACTED TO A FIRM, UNYIELDING CONDITION, SUBJECT TO APPROVAL BY THE RPR.
- PUBLIC RIGHTS-OF-WAY AND AIRCRAFT OPERATIONS AREAS SHALL BE KEPT IN A CLEAN AND SERVICEABLE CONDITION AT ALL TIMES. IN THE EVENT MATERIALS ARE INADVERTENTLY DEPOSITED ON ROADWAYS, TAXIWAYS OR APRONS, THE MATERIAL SHALL BE PROMPTLY REMOVED.
- AREAS DISTURBED BY CONSTRUCTION ACTIVITY, INCLUDING STAGING AND DISPOSAL AREAS, SHALL BE COVERED BY HYDRAULICALLY APPLIED SEED AND TACKING AGENTS, VERIFY LOCATIONS WITH RPR, SEE SPECIFICATIONS. ANY AREAS DISTURBED BY THE CONTRACTOR OUTSIDE DISPOSAL AREAS, OR OUTSIDE OTHER AREAS AS INDICATED ON THE PLANS SHALL BE RESEEDED AT THE CONTRACTOR'S EXPENSE WITH T-901 HYDROSEEDING.
- THE CONTRACTOR SHALL REMOVE AND STOCKPILE EXISTING ROUND AGGREGATE DURING GRADING ACTIVITIES. ROUND AGGREGATE SHALL BE REAPPLIED TO THE SURFACE AT A MINIMUM 4-INCH DEPTH.

PRINT IN COLOR

THIS DRAWING IS INTENDED TO BE PRINTED IN COLOR. PRINTING IN BLACK AND WHITE MAY REDUCE READABILITY AND ALTER ENTITY DEFINITION OR REPRESENTATION.



PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



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| 1 ADDENDUM NO. 1 | 5/24/2024 | JGL |
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| DATE: | MAY 2024 |
| DESIGN: | KJK |
| DRAWN: | JAW |
| CHECKED: | JGL |
| REVISION NUMBER: | 0 |
| SCALE: | AS SHOWN |

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

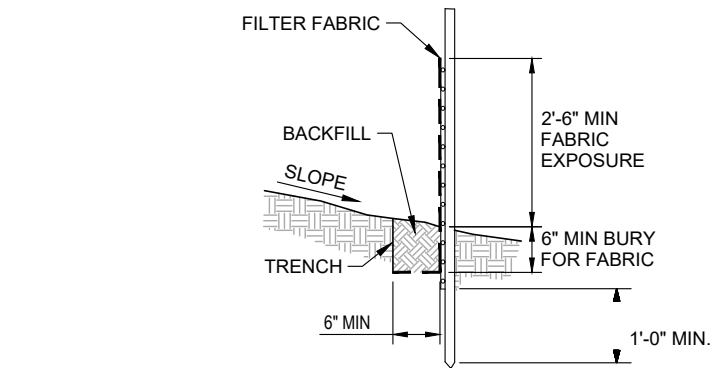
**EROSION AND SEDIMENT
CONTROL PLAN - ADDITIVE**

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

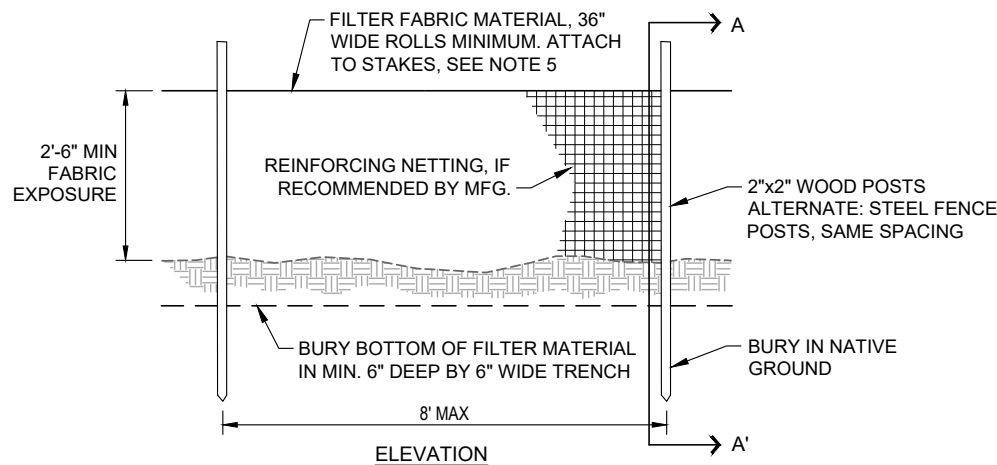
PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
11
OF 71

05/24/24 - 2:07pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_ESCP.dwg



SECTION A-A'



ELEVATION

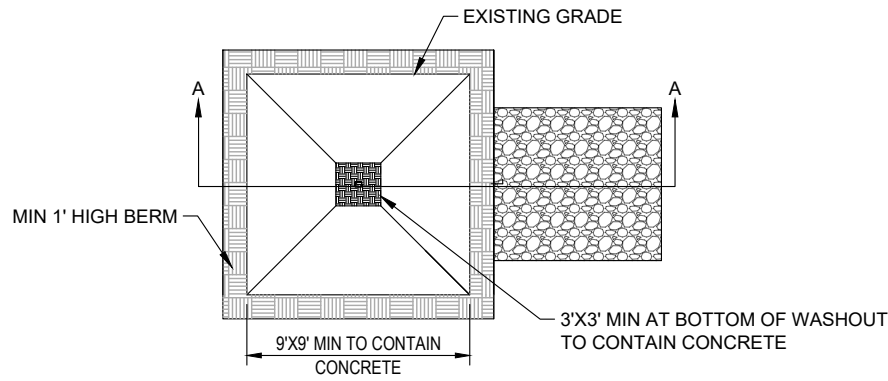
SEDIMENT FENCE NOTES:

1. INSTALLATION SHALL FOLLOW THE MANUFACTURER'S RECOMMENDATIONS.
2. POSTS SHALL BE INSTALLED SO THAT A MIN. OF 3'-0" EXTENDS ABOVE THE GROUND WITH A MIN. OF 2'-0" EMBEDMENT.
3. FILTER FABRIC SHALL BE PURCHASED IN A CONTINUOUS ROLL TO AVOID JOINTS.
4. SPLICE JOINTS AT SUPPORT POSTS ONLY, WITH A MIN. 6" OVERLAP.
5. SEDIMENT FENCE SHALL BE HIGH VISIBILITY ORANGE FENCE.

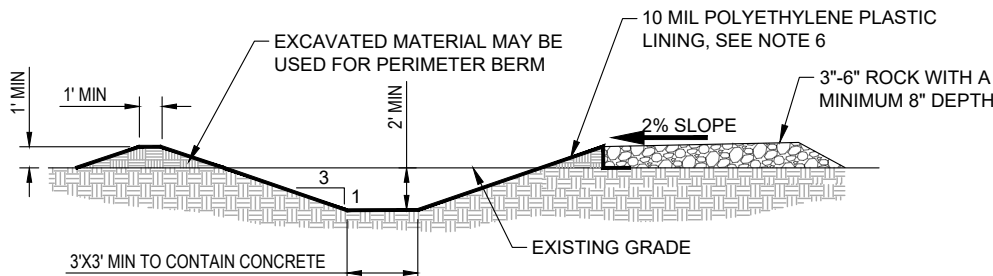
SILT FENCE DETAIL

NTS

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13



PLAN



SECTION A-A'

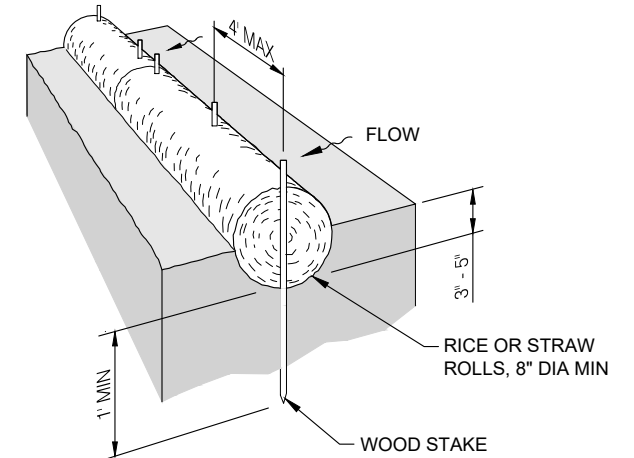
NOTES:

1. WASHOUT FACILITIES SHALL BE MAINTAINED TO PROVIDE ADEQUATE HOLDING CAPACITY WITH A MINIMUM FREEBOARD OF 12 INCHES.
2. WASHOUT FACILITY MUST BE CLEANED OR NEW FACILITIES MUST BE CONSTRUCTED AND READY FOR USE ONCE THE WASHOUT IS 75% FULL.
3. IF THE WASHOUT IS NEARING CAPACITY, VACUUM AND DISPOSE OF THE WASTE MATERIAL IN AN APPROVED MANNER.
4. TEMPORARY CONCRETE WASHOUT FACILITIES SHALL BE LOCATED A MINIMUM OF 50 FT FROM SENSITIVE AREAS INCLUDING OPEN DRAINAGE FACILITIES AND WATER SOURCES.
5. CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED AND MAINTAINED IN SUFFICIENT QUANTITY AND SIZE TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS.
6. PLASTIC LINER SHALL BE SECURED TO THE TOP OF THE PERIMETER BERM, AT 2' SPACING, ON ALL SIDES, BY RPR APPROVED METHOD.
7. TEMPORARY CONCRETE WASHOUTS MAY BE A PREFABRICATED CONTAINER THAT IS PORTABLE AND REUSABLE. SUBMIT TO RPR FOR APPROVAL.
8. AT COMPLETION OF CONSTRUCTION, CONCRETE WASHOUT AND ALL WASHOUT MATERIAL SHALL BE REMOVED AND DISPOSED OF OFF SITE. WASHOUT AREA AND AREAS DIRECTLY ADJACENT SHALL MEET OR EXCEED EXISTING CONDITIONS.

CONCRETE WASHOUT DETAIL

NTS

2
13



NOTES:

1. WATTLES SHALL BE IN ACCORDANCE WITH OREGON DEQ SEDIMENT CONTROL BMP SC-7. INSTALL WATTLES AS SHOWN ON PLANS OR AS DIRECTED BY THE RPR.
2. SECURELY KNOT EACH END OF THE WATTLE. ABUT ADJACENT WATTLES TIGHTLY, END TO END, WITHOUT OVERLAPPING THE ENDS.
3. USE A STRAIGHT BAR TO DRIVE PILOT HOLES THROUGH THE ROLL AND INTO THE SOIL.
4. DRIVE THE STAKE THROUGH THE PREPARED HOLE, AND INTO THE SOIL. LEAVE ONLY 1 OR 2 INCHES OF THE STAKE EXPOSED ABOVE THE ROLL.
5. WATTLES SHALL BE INSPECTED REGULARLY, AND IMMEDIATELY AFTER A RAINFALL THAT PRODUCES RUNOFF, TO ENSURE THAT THEY REMAIN THOROUGHLY ENTRENCHED AND IN CONTACT WITH THE SOIL.

STRAW WATTLE DETAIL

NTS

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13

1 REPLACE ENTIRE SHEET



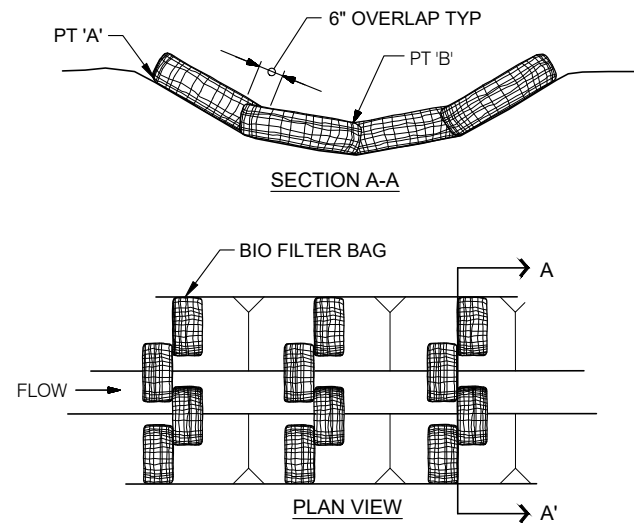
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| ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES |
| EROSION AND SEDIMENT CONTROL NOTES AND DETAILS |
| PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063 |

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| PROJECT NUMBER: RVI-2020-W04 |
| SHEET NO. 13 OF 71 |

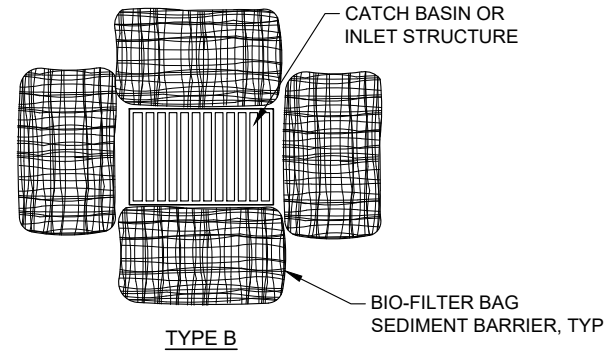
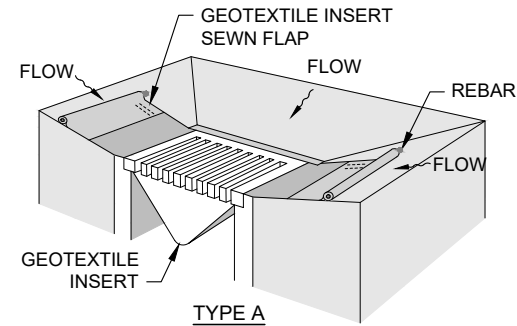
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- NOTES:
1. STAKING OF BAGS MAY BE REQUIRED USING (2) 1\"X2\" WOOD STAKES OR APPROVED EQUAL PER BAG
 2. PT \"A\" MUST BE 6\" MIN HIGHER THAN PT \"B\"

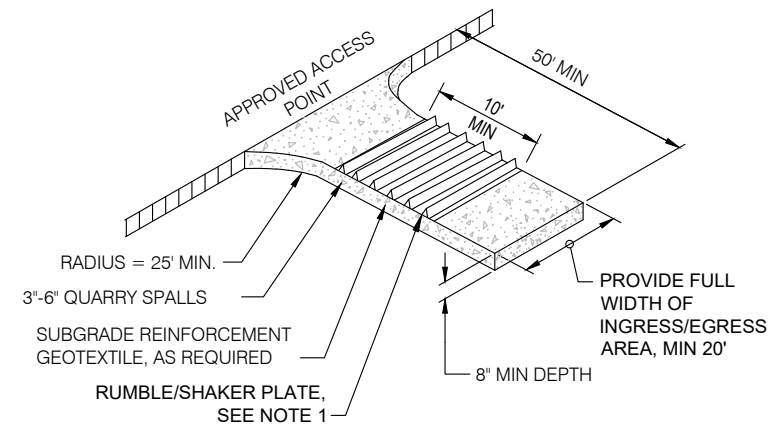
BIO FILTER BAG DITCHES AND SWALES
NTS

1
14



INLET PROTECTION
NTS

2
14



- NOTES:
1. CONTRACTOR SHALL USE A STABILIZED CONSTRUCTION ENTRANCE AT SITE ACCESS POINTS. CONTRACTOR MAY USE SHAKER PLATE FOR CONSTRUCTION ACCESS AS AN ALTERNATIVE OR IN CONJUNCTION WITH STABILIZED CONSTRUCTION ENTRANCE IF NEEDED TO CONTROL TRACKING OF MATERIAL ONTO PAVEMENT. VERIFY WITH RPR PRIOR TO USE.

STABILIZED CONSTRUCTION ENTRANCE
NTS

3
14

1 REPLACE ENTIRE SHEET

PRECISION APPROACH
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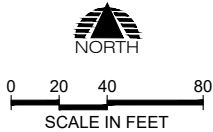
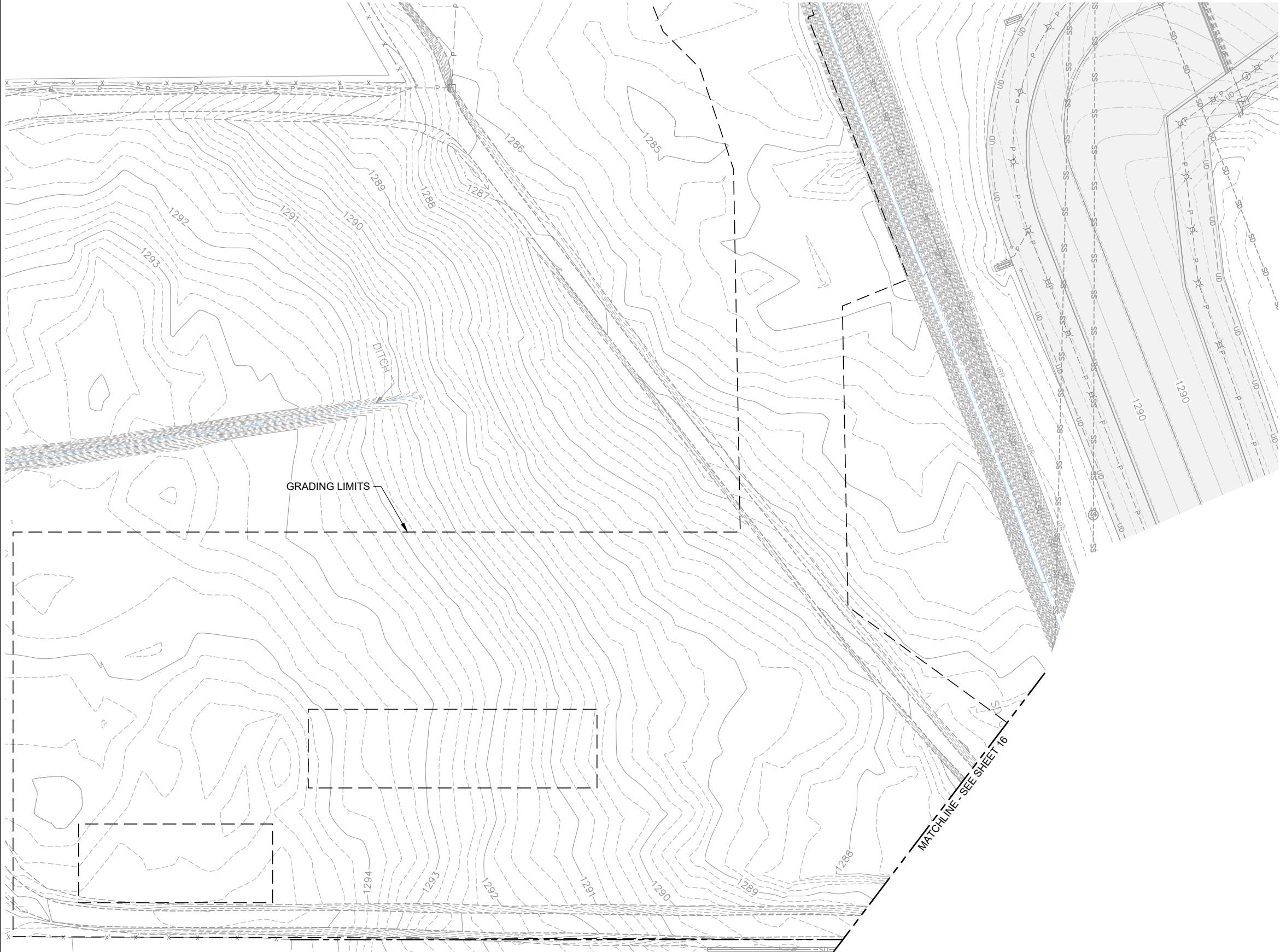
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DATE: MAY 2024
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
**EROSION AND SEDIMENT
CONTROL NOTES AND DETAILS**
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
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OF 71

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LEGEND

- EXISTING ASPHALT PAVEMENT
- FULL DEPTH PAVEMENT REMOVAL
- PARTIAL DEPTH PAVEMENT REMOVAL, ADDITIVE A, SEE DETAIL 4/23
- EXISTING MARKING TO BE REMOVED
- STRUCTURE TO BE REMOVED
STRUCTURE ID LEGEND
CO - CLEANOUT
GS - GUIDANCE SIGN
- LIGHT AND BASE CAN TO BE REMOVED
- RETROREFLECTIVE MARKER REMOVAL, SEE NOTE 8
- GUIDANCE SIGN PANEL REMOVAL AND REPLACEMENT, SEE ELECTRICAL PLANS
- STORM DRAIN TO BE REMOVED
- ELECTRICAL SYSTEMS TO BE REMOVED, SEE ELECTRICAL PLANS
- FENCE TO BE REMOVED
- PROTECT DURING CONSTRUCTION

DEMOLITION NOTES

- LIMITS OF PAVEMENT REMOVAL ARE APPROXIMATE. CONTRACTOR SHALL LAYOUT LIMITS WITH SURVEY AND FIELD VERIFY WITH ENGINEER PRIOR TO REMOVAL. SEE LAYOUT PLAN SHEETS FOR ADDITIONAL INFORMATION.
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- POROUS PAVER REMOVAL NOT SHOWN ON THIS SHEET, SEE DRAINAGE PLANS.

1 REPLACE ENTIRE SHEET

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

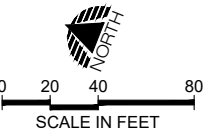
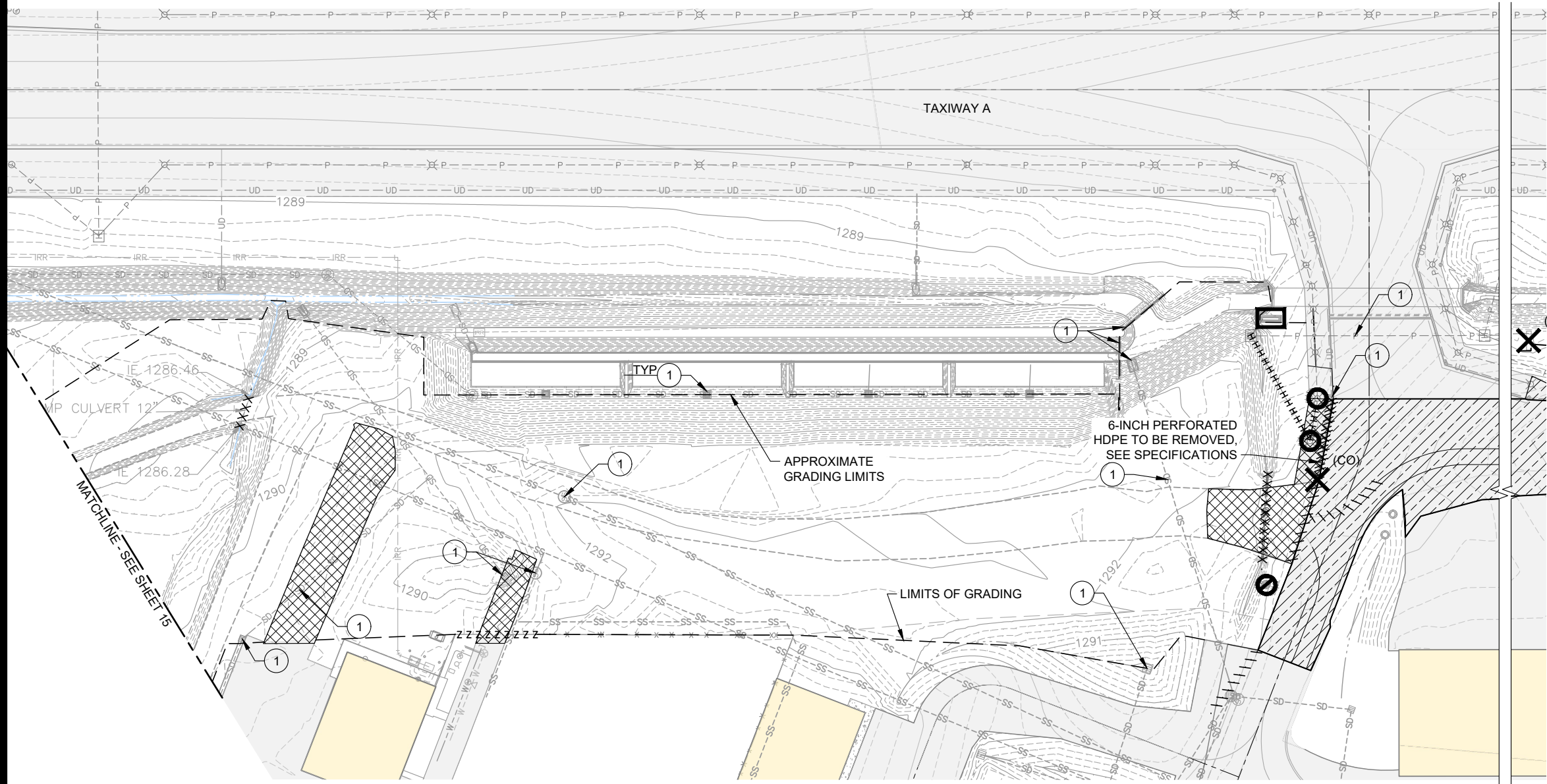
DEMOLITION PLAN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
15
OF 71

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LEGEND

- EXISTING ASPHALT PAVEMENT
- FULL DEPTH PAVEMENT REMOVAL
- PARTIAL DEPTH PAVEMENT REMOVAL, ADDITIVE A, SEE DETAIL 4/23
- EXISTING MARKING TO BE REMOVED
- (ID) STRUCTURE TO BE REMOVED
STRUCTURE ID LEGEND
CO - CLEANOUT
GS - GUIDANCE SIGN
- LIGHT AND BASE CAN TO BE REMOVED
- RETROREFLECTIVE MARKER REMOVAL, SEE NOTE 8
- GUIDANCE SIGN PANEL REMOVAL AND REPLACEMENT, SEE ELECTRICAL PLANS
- STORM DRAIN TO BE REMOVED
- ELECTRICAL SYSTEMS TO BE REMOVED, SEE ELECTRICAL PLANS
- FENCE TO BE REMOVED
- PROTECT DURING CONSTRUCTION

DEMOLITION NOTES

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1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



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5/24/2024 JGL
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

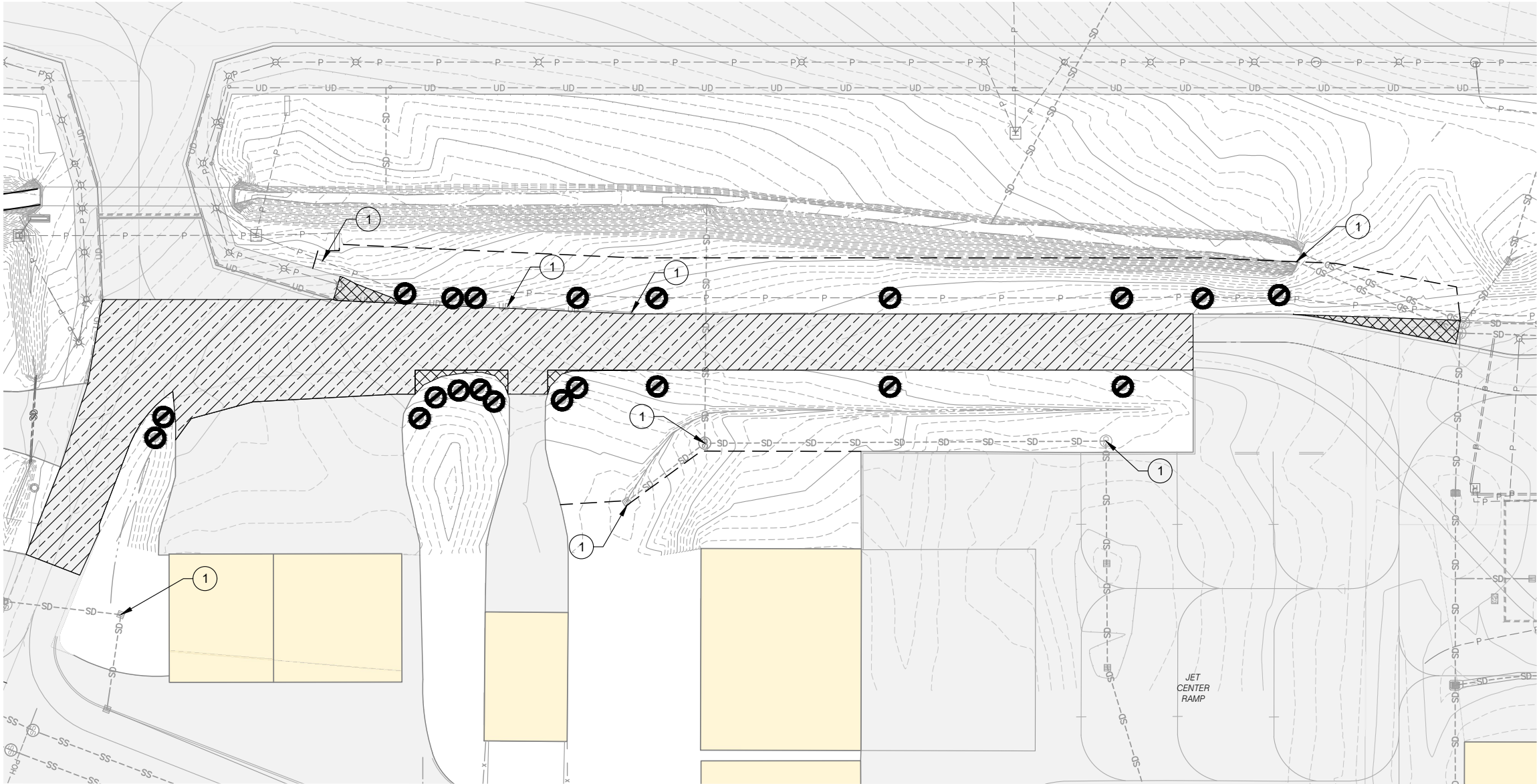
DEMOLITION PLAN

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AIP NO. 3-41-0037-063

PROJECT NUMBER:
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OF 71

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EXISTING ASPHALT PAVEMENT

FULL DEPTH PAVEMENT REMOVAL

PARTIAL DEPTH PAVEMENT REMOVAL, ADDITIVE A, SEE
DETAIL 4
23

EXISTING MARKING TO BE REMOVED

(ID)
X

STRUCTURE TO BE REMOVED
STRUCTURE ID LEGEND
CO - CLEANOUT
GS - GUIDANCE SIGN

LIGHT AND BASE CAN TO BE REMOVED

RETROREFLECTIVE MARKER REMOVAL, SEE NOTE 8

GUIDANCE SIGN PANEL REMOVAL AND REPLACEMENT,
SEE ELECTRICAL PLANS

XXXXXX

STORM DRAIN TO BE REMOVED

HHHHHH

ELECTRICAL SYSTEMS TO BE REMOVED, SEE
ELECTRICAL PLANS

ZZZZZZ

FENCE TO BE REMOVED

1

PROTECT DURING CONSTRUCTION

0 20 40 80

SCALE IN FEET

LEGEND

- DEMOLITION NOTES
1. LIMITS OF PAVEMENT REMOVAL ARE APPROXIMATE. CONTRACTOR SHALL LAYOUT LIMITS WITH SURVEY AND FIELD VERIFY WITH ENGINEER PRIOR TO REMOVAL. SEE LAYOUT PLAN SHEETS FOR ADDITIONAL INFORMATION.

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9. POROUS PAVER REMOVAL NOT SHOWN ON THIS SHEET, SEE DRAINAGE PLANS.
- 1

REPLACE ENTIRE SHEET

PRECISION APPROACH

ENGINEERING

5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043

REGISTERED PROFESSIONAL
ENGINEER
77750PE
DIGITALLY SIGNED
JOSHUA G. LEYKKEKEMER
OREGON
JUNE 2, 2010
EXPIRES 12/31/2024

1

ADDENDUM NO. 1

5/24/2024

JGL

REVISIONS:

DATE

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

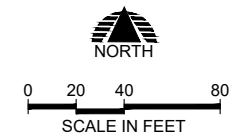
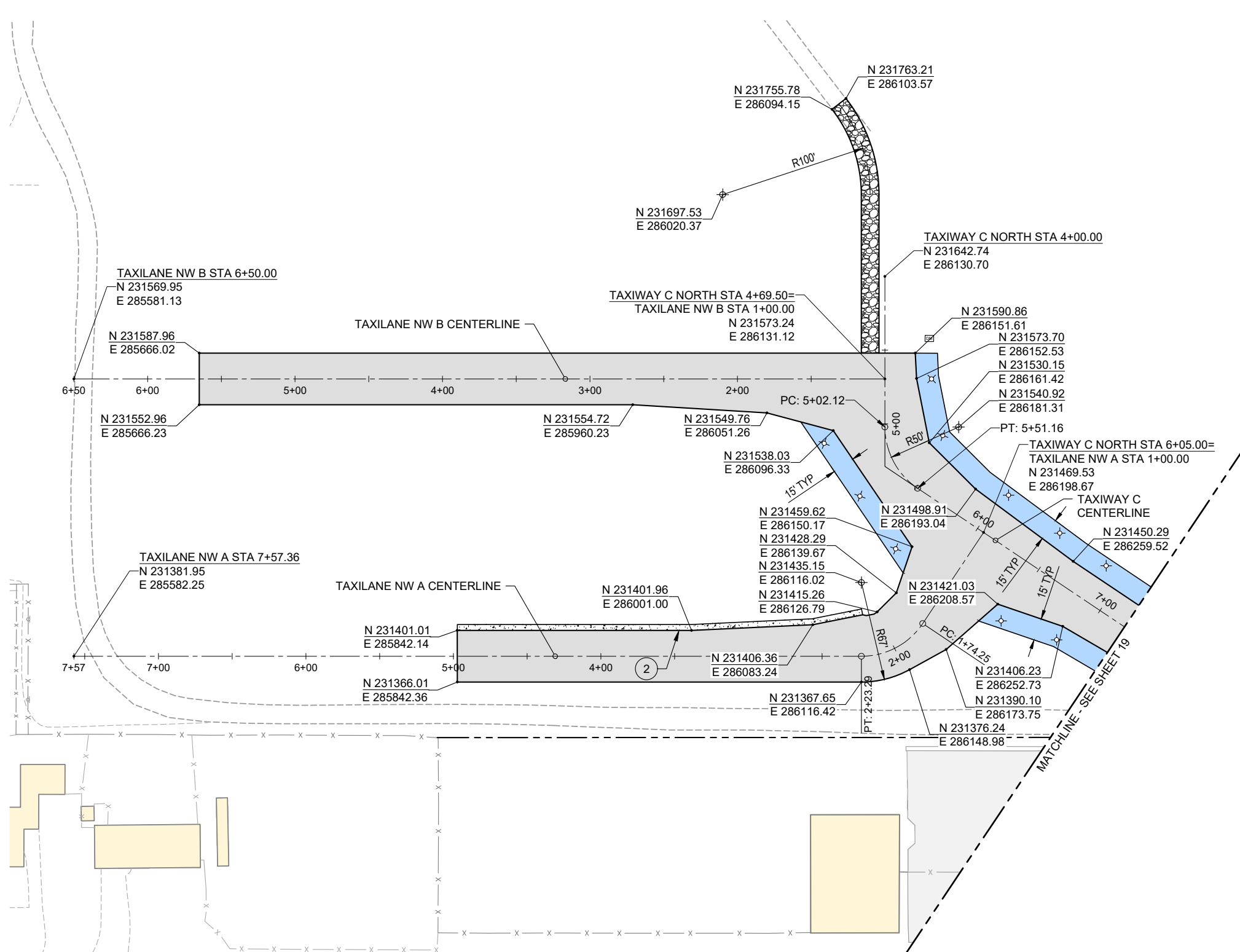
DEMOLITION PLAN - ADDITIVE

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

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OF 71

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LEGEND

EXISTING ASPHALT PAVEMENT

FULL DEPTH ASPHALT PAVEMENT SECTION,
SEE DETAIL 1/22

ASPHALT OVERLAY PAVEMENT SECTION,
SEE DETAIL 4/23

FULL DEPTH ASPHALT SHOULDER PAVEMENT SECTION,
SEE DETAIL 1/22

FULL DEPTH ADDITIVE PAVEMENT SECTION,
SEE DETAIL 2/22

FULL DEPTH ADDITIVE SHOULDER PAVEMENT SECTION,
SEE DETAIL 2/22

GRAVEL ROAD SECTION, SEE DETAIL 3/23

VALLEY GUTTER, SEE DETAIL 2/45

1

FULL DEPTH PAVEMENT SECTION TO EXISTING PAVEMENT
SECTION CONNECTION, SEE DETAIL 4/22

2

FULL DEPTH PAVEMENT SECTION TO VALLEY GUTTER
CONNECTION, SEE DETAIL 2/45

3

INSTALL WILDLIFE FENCE, SEE DETAIL 1/69

4

CONNECT TO EXISTING FENCE, SEE DETAIL 1/70

5

INSTALL 18' SWING GATE, SEE DETAIL 1/71

- NOTES
1.

SEE DEMOLITION PLAN FOR ITEMS TO BE REMOVED.
2.

FOR PAVEMENT FINISH GRADE INFORMATION, SEE PAVING PLANS,
PLAN AND PROFILE SHEETS, AND TAXIWAY OVERLAY TYPICAL
SECTION, SHEETS 25-28 AND 31.
3.

FOR NON-PAVED SHOULDER GRADING SEE GRADING AND DRAINAGE
PLANS.
4.

FENCE LAYOUT IS APPROXIMATE AND SHOULD INCLUDE THE
18-FOOT DOUBLE SWING GATE, CONNECTION TO THE NORTH
CORNER OF THE PERIMETER FENCE, AND CONNECTION TO THE
EXISTING SOUTH PERIMETER FENCE AT LEAST TWO EXISTING
FENCE POSTS FROM THE NEW GATE, OR A MAXIMUM DISTANCE OF
52 LINEAR FEET FROM THE NORTH CORNER OF THE PERIMETER
FENCE.

1 REPLACE ENTIRE SHEET

PRECISION APPROACH

ENGINEERING

5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043

REGISTERED PROFESSIONAL
ENGINEER
77750PE
DIGITALLY SIGNED
JOSHUA G. LEIKER
JUNE 2, 2010
OREGON
EXPIRES 12/31/2024

1 ADDENDUM NO. 1

5/24/2024

JGL

REVISIONS:

DATE

APPD.

DATE: MAY 2024

DESIGN: KJK

DRAWN: JAW

CHECKED: JGL

REVISION NUMBER: 0

SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

LAYOUT PLAN

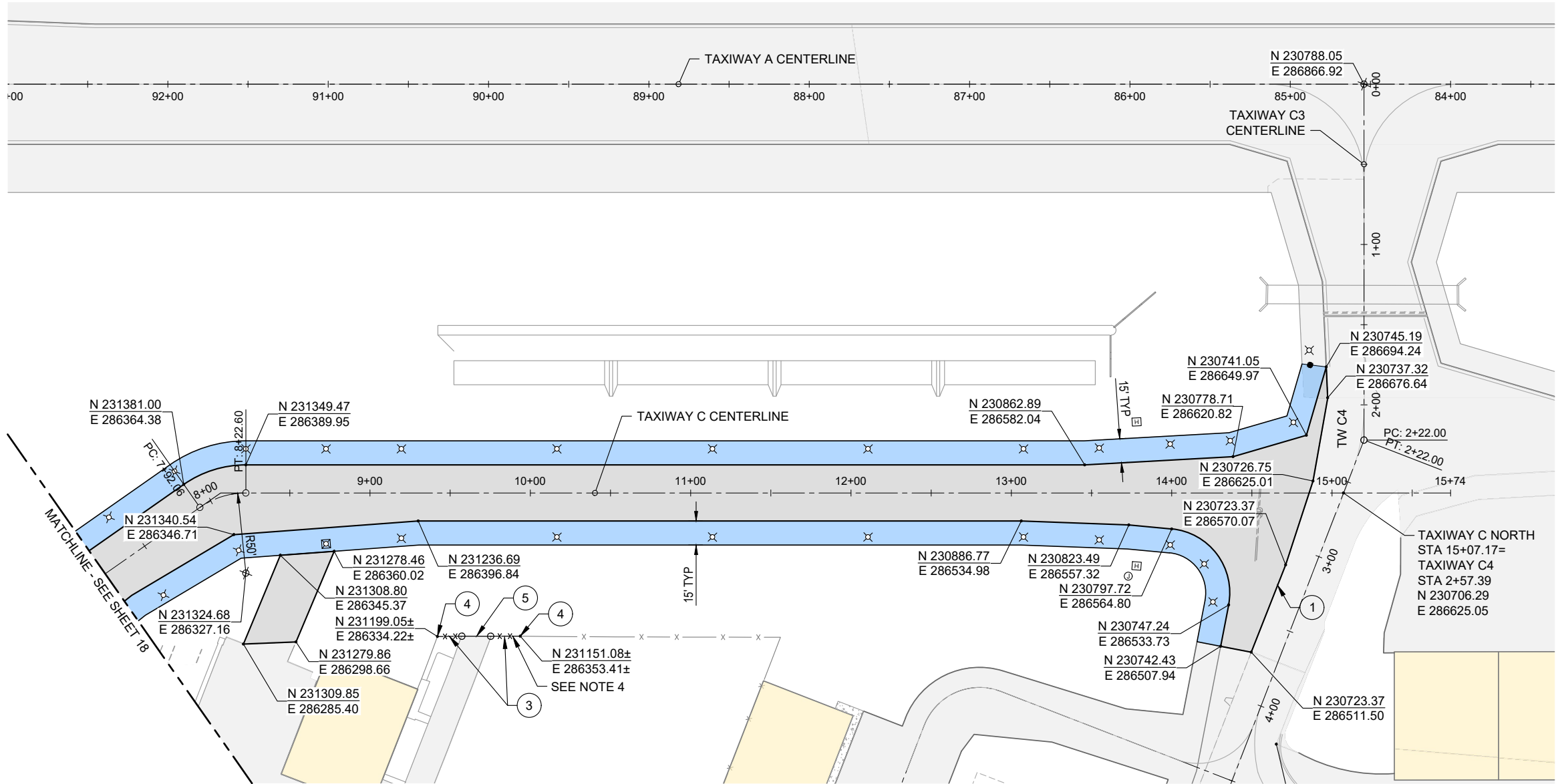
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
18

OF 71

05/24/24 - 2:08pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_Layout.dwg



LEGEND

- EXISTING ASPHALT PAVEMENT
- FULL DEPTH ASPHALT PAVEMENT SECTION, SEE DETAIL (1/22)
- ASPHALT OVERLAY PAVEMENT SECTION, SEE DETAIL (4/23)
- FULL DEPTH ASPHALT SHOULDER PAVEMENT SECTION, SEE DETAIL (1/22)
- FULL DEPTH ADDITIVE PAVEMENT SECTION, SEE DETAIL (2/22)
- FULL DEPTH ADDITIVE SHOULDER PAVEMENT SECTION, SEE DETAIL (2/22)
- GRAVEL ROAD SECTION, SEE DETAIL (3/23)
- VALLEY GUTTER, SEE DETAIL (2/45)
- (1) FULL DEPTH PAVEMENT SECTION TO EXISTING PAVEMENT SECTION CONNECTION, SEE DETAIL (4/22)
- (2) FULL DEPTH PAVEMENT SECTION TO VALLEY GUTTER CONNECTION, SEE DETAIL (2/45)
- (3) INSTALL WILDLIFE FENCE, SEE DETAIL (1/69)
- (4) CONNECT TO EXISTING FENCE, SEE DETAIL (1/70)
- (5) INSTALL 18' SWING GATE, SEE DETAIL (1/71)

NOTES

- SEE DEMOLITION PLAN FOR ITEMS TO BE REMOVED.
- FOR PAVEMENT FINISH GRADE INFORMATION, SEE PAVING PLANS, PLAN AND PROFILE SHEETS, AND TAXIWAY OVERLAY TYPICAL SECTION, SHEETS 25-28 AND 31.
- FOR NON-PAVED SHOULDER GRADING SEE GRADING AND DRAINAGE PLANS.
- FENCE LAYOUT IS APPROXIMATE AND SHOULD INCLUDE THE 18-FOOT DOUBLE SWING GATE, CONNECTION TO THE NORTH CORNER OF THE PERIMETER FENCE, AND CONNECTION TO THE EXISTING SOUTH PERIMETER FENCE AT LEAST TWO EXISTING FENCE POSTS FROM THE NEW GATE, OR A MAXIMUM DISTANCE OF 52 LINEAR FEET FROM THE NORTH CORNER OF THE PERIMETER FENCE.

1 REPLACE ENTIRE SHEET

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ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



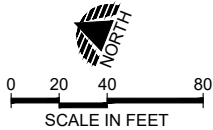
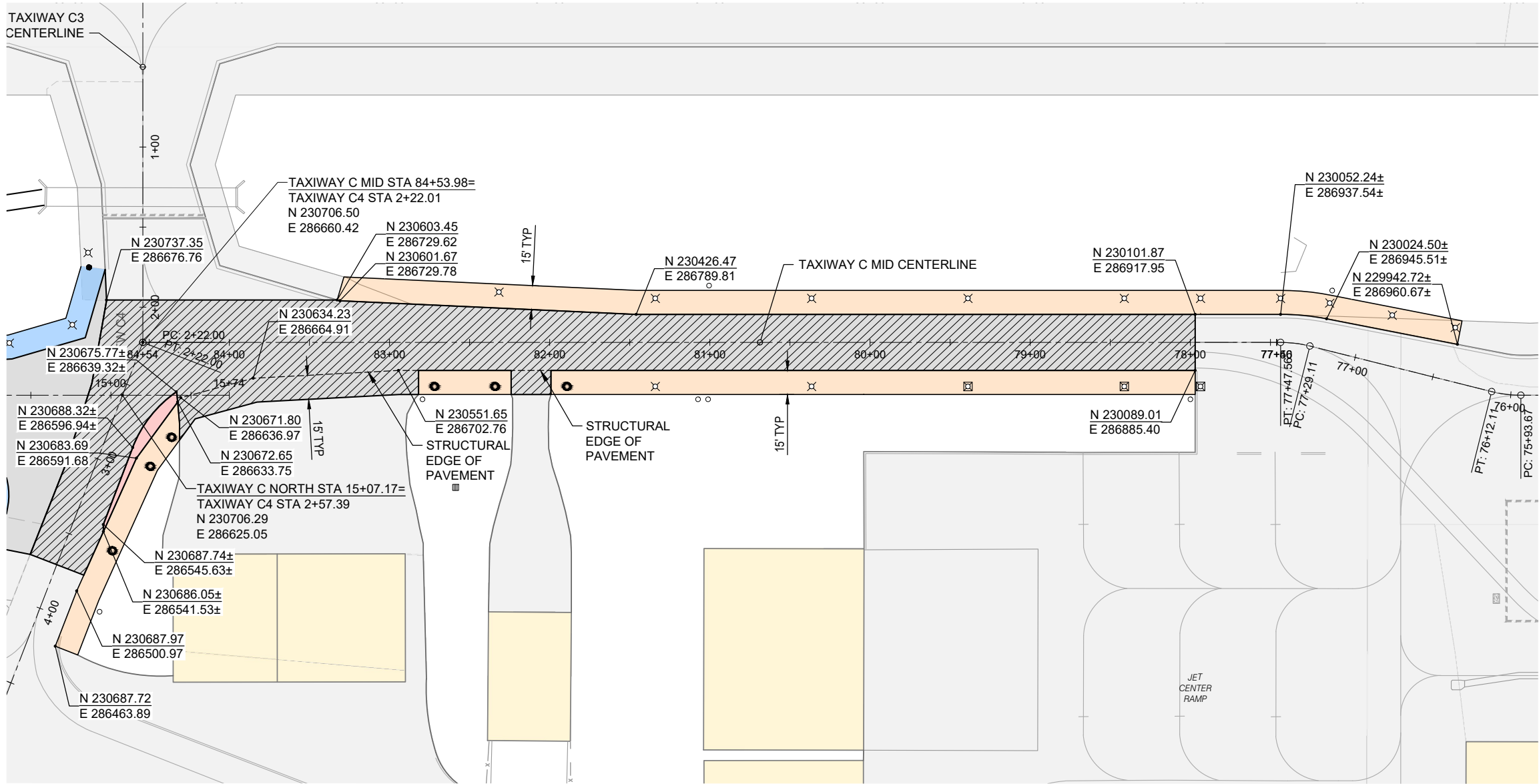
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DATE: MAY 2024
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CHECKED: JGL
REVISION NUMBER: 0
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
LAYOUT PLAN
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04
SHEET NO.
19
OF **71**

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LEGEND

- EXISTING ASPHALT PAVEMENT
- FULL DEPTH ASPHALT PAVEMENT SECTION, SEE DETAIL 1/22
- ASPHALT OVERLAY PAVEMENT SECTION, SEE DETAIL 4/23
- FULL DEPTH ASPHALT SHOULDER PAVEMENT SECTION, SEE DETAIL 1/22
- FULL DEPTH ADDITIVE PAVEMENT SECTION, SEE DETAIL 2/22
- FULL DEPTH ADDITIVE SHOULDER PAVEMENT SECTION, SEE DETAIL 2/22
- GRAVEL ROAD SECTION, SEE DETAIL 3/23
- VALLEY GUTTER, SEE DETAIL 2/45
- 1 FULL DEPTH PAVEMENT SECTION TO EXISTING PAVEMENT SECTION CONNECTION, SEE DETAIL 4/22
- 2 FULL DEPTH PAVEMENT SECTION TO VALLEY GUTTER CONNECTION, SEE DETAIL 2/45
- 3 INSTALL WILDLIFE FENCE, SEE DETAIL 1/69
- 4 CONNECT TO EXISTING FENCE, SEE DETAIL 1/70
- 5 INSTALL 18' SWING GATE, SEE DETAIL 1/71

NOTES

- SEE DEMOLITION PLAN FOR ITEMS TO BE REMOVED.
- FOR PAVEMENT FINISH GRADE INFORMATION, SEE PAVING PLANS, PLAN AND PROFILE SHEETS, AND TAXIWAY OVERLAY TYPICAL SECTION, SHEETS 25-28 AND 31.
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

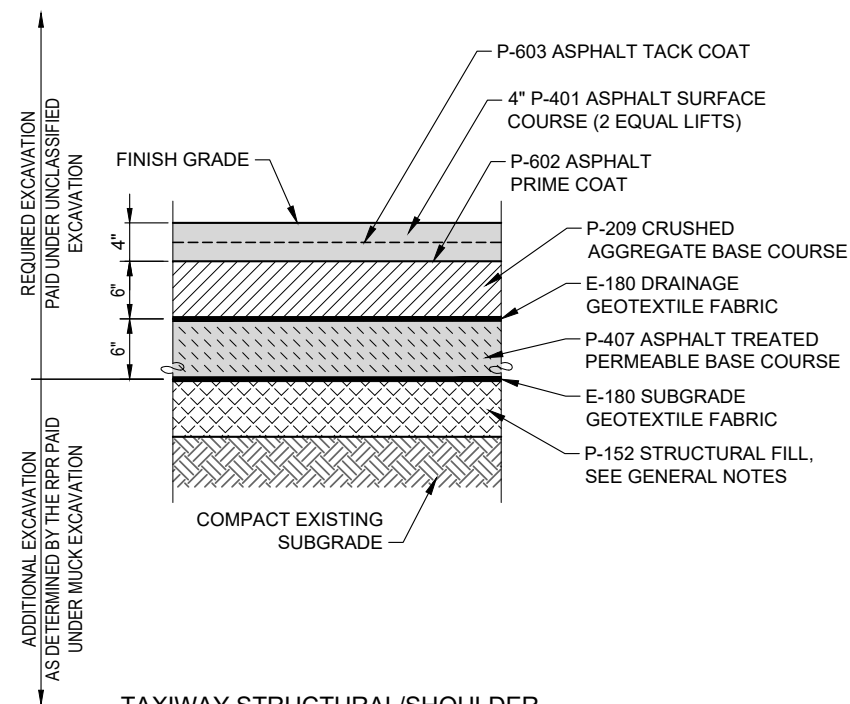
LAYOUT PLAN - ADDITIVE

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
20
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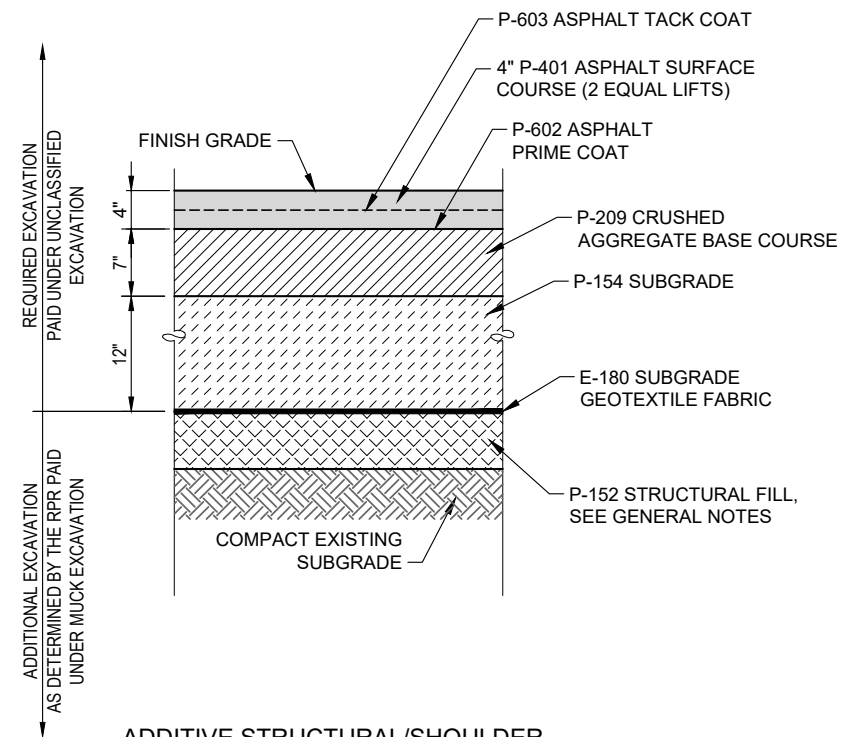
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TAXIWAY STRUCTURAL/SHOULDER
PAVEMENT SECTION

NTS

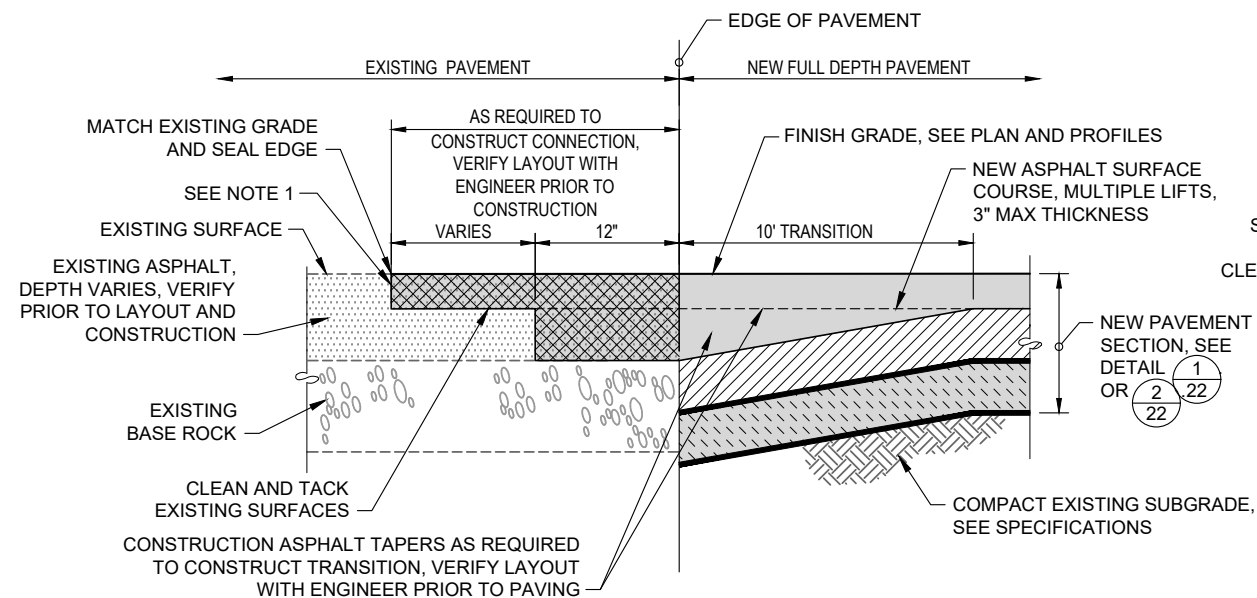
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22



ADDITIVE STRUCTURAL/SHOULDER
TAXIWAY PAVEMENT SECTION

NTS

2
22



EXISTING ASPHALT DEPTH GREATER THAN 4"

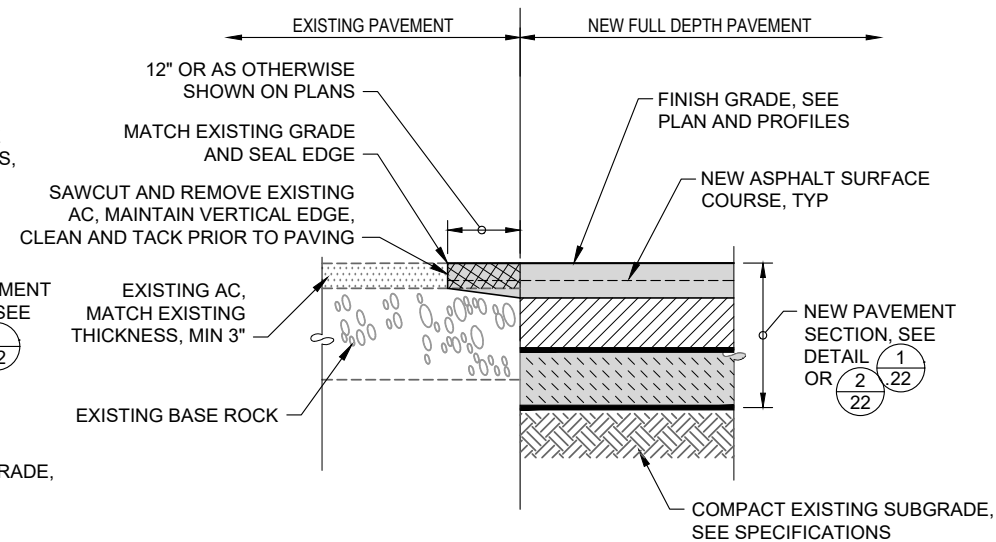
NOTES:

1. SAWCUT AND REMOVE EXISTING ASPHALT, MAINTAIN VERTICAL EDGE, CLEAN AND TACK PRIOR TO PAVING, TYP ALL NOTCHES.

FULL DEPTH PAVEMENT SECTION TO
EXISTING PAVEMENT CONNECTION DETAIL

NTS

3
22



EXISTING ASPHALT DEPTH LESS THAN OR EQUAL TO 4"

GENERAL NOTES:

1. UPON REQUEST OF THE RPR, THE CONTRACTOR SHALL ALLOW FOR VISUAL INSPECTION OF THE SUBGRADE MATERIAL BY THE RPR PRIOR TO COMPACTIVE EFFORT TO DETERMINE SUBGRADE MATERIAL SUITABILITY.
2. ANY EXCAVATED MATERIAL BELOW BOTTOM COURSE OF THE PAVEMENT SECTION SHALL BE PAID FOR UNDER THE MUCK EXCAVATION BID ITEM.

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

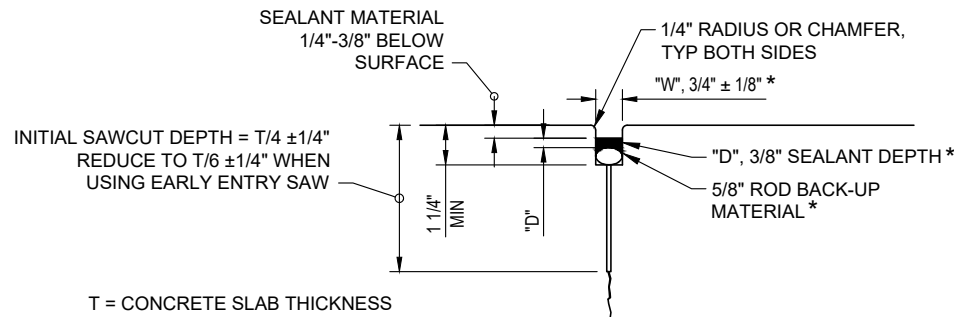
PAVEMENT DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
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* SEE NOTES
CONTRACTION JOINT

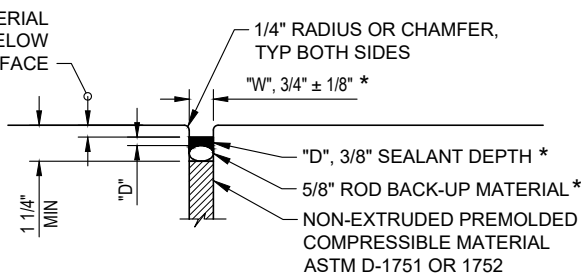
NOTES:

1. CONTRACTOR TO SAWCUT NEW CONCRETE TO PROVIDE RESERVOIR FOR SEALANT MEETING ASTM D-5893. CLEAN AND DRY ALL SURFACES PRIOR TO FILLING, TYP.
2. SEALANT RESERVOIR SHALL BE TO PROVIDE PROPER SHAPE FACTOR. "W" AND "D" DIMENSIONS MAY VARY PER MANUFACTURERS RECOMMENDATIONS TO PROVIDE OPTIMUM PERFORMANCE. ENGINEERS REVIEW AND APPROVAL OF SEALANT RESERVOIR REQUIRED PRIOR TO CONSTRUCTION.
3. ROD BACK-UP MATERIAL MUST BE COMPATIBLE WITH THE TYPE OF LIQUID SEALANT USED AND SIZED TO PROVIDE THE DESIRED SHAPE FACTOR. ROD BACK-UP MATERIAL SIZE MAY BE ADJUSTED TO MAINTAIN MINIMUM DEPTH, VERIFY WITH RPR.
4. ALL JOINTS AND JOINT MATERIALS SHALL BE CONSIDERED INCIDENTAL TO OTHER BID ITEMS.

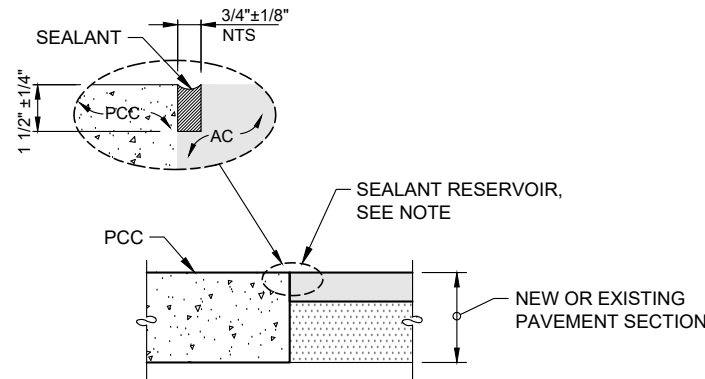
**CONCRETE JOINT SEALANT
INSTALLATION DETAIL**

NTS

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23



* SEE NOTES
ISOLATION/ EXPANSION JOINT



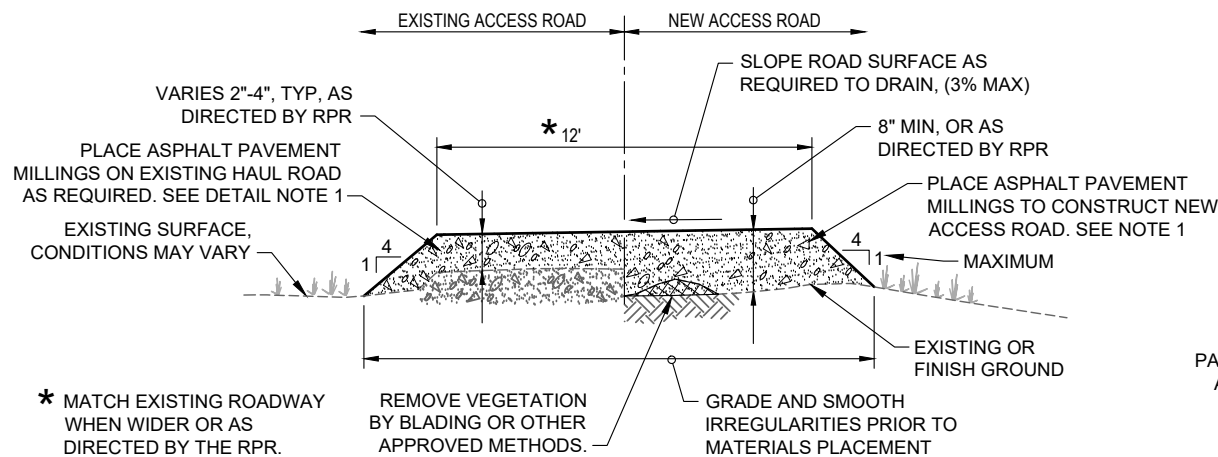
NOTES:

1. CONTRACTOR TO SAWCUT ASPHALT PAVEMENT TO PROVIDE RESERVOIR FOR SEALANT. SEALANT SHALL BE SELF-LEVELING AND AVIATION FUEL RESISTANT. CLEAN AND DRY ALL SURFACES PRIOR TO FILLING, TYPICAL. DETAIL FOR USE WHERE NEW ASPHALT MEETS NEW OR EXISTING CONCRETE.
2. ALL JOINTS AND JOINT MATERIALS SHALL BE CONSIDERED INCIDENTAL TO OTHER BID ITEMS.

LONGITUDINAL JOINT SEAL DETAIL

NTS

2
23



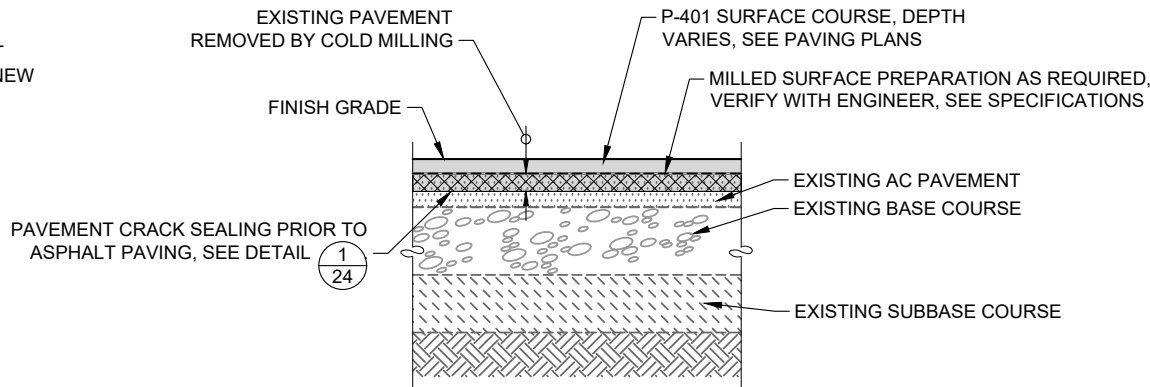
ACCESS ROAD NOTES:

1. GRADE SMOOTH AND COMPACT TO A DENSE UNYIELDING CONDITION AS DIRECTED BY THE RPR.
2. PRIOR TO PLACEMENT OF ANY GRINDINGS ON NEW OR EXISTING ACCESS ROADS, THE CONTRACTOR SHALL VERIFY ALL LOCATIONS WITH THE RPR.
3. UPON COMPLETION OF WORK, CONTRACTOR SHALL REGRADE ACCESS ROAD AND RECOMPACT TO DENSE UNYIELDING CONDITION, VERIFY WITH RPR.
4. ALL WORK AND MATERIALS REQUIRED TO PLACE ASPHALT GRINDINGS SHALL BE CONSIDERED INCIDENTAL TO THE ASPHALT PAVEMENT REMOVAL BID ITEMS.

**CONSTRUCTION ACCESS
ROAD TYPICAL SECTION**

NTS

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23



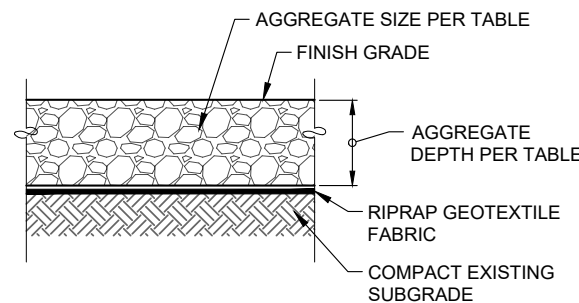
NOTES:

1. MINIMUM LIFT THICKNESSES FOR P-401 SHALL BE 2 INCHES. THINNER LIFTS MAY BE REQUIRED AND SHALL BE CONSIDERED PRE-LEVEL, SEE SPECIFICATIONS.

TAXIWAY OVERLAY PAVEMENT SECTION

NTS

4
23



| BID ITEM | AGGREGATE | DEPTH OF SECTION * |
|-----------------------------|-----------------------|--------------------|
| QUARRY SPALL PROTECTION | 1"-4" QUARRY SPALLS | 6 INCHES |
| CLASS 50 RIPRAP PROTECTION | ODOT CLASS 50 RIPRAP | 12 INCHES |
| CLASS 100 RIPRAP PROTECTION | ODOT CLASS 100 RIPRAP | 18 INCHES |

* UNLESS OTHERWISE SHOWN ON PLANS

EROSION PROTECTION DETAIL

NTS

5
23

1 REPLACE ENTIRE SHEET



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| SCALE: | AS SHOWN |

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

PAVEMENT DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

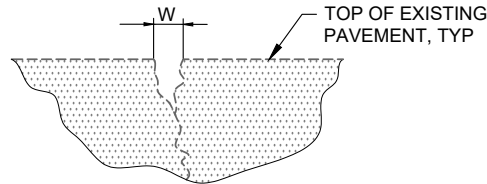
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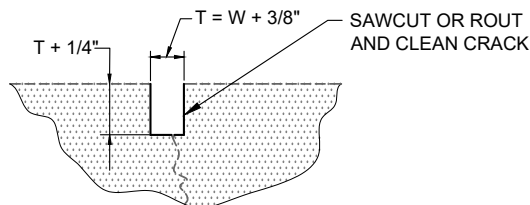
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OF 71

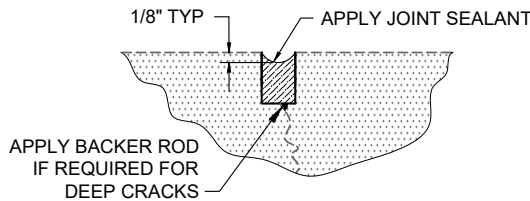
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EXISTING CONDITION



STEP 1



STEP 2

NOTES:

1. CRACKS TO BE SEALED WILL BE DETERMINED BY RPR.
2. FRESHLY LAID SEALANT ABOVE SURFACE LEVEL SHALL BE REMOVED PRIOR TO OVERLAY. EXCESS SEALANT ADJACENT TO CRACK SHALL BE REMOVED BY GRINDING OR BY METHOD DEMONSTRATED TO REMOVE SEALANT WITHOUT SIGNIFICANT DISTRESS TO THE PAVEMENT.

PAVEMENT CRACK SEALING DETAIL
NTS

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24

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

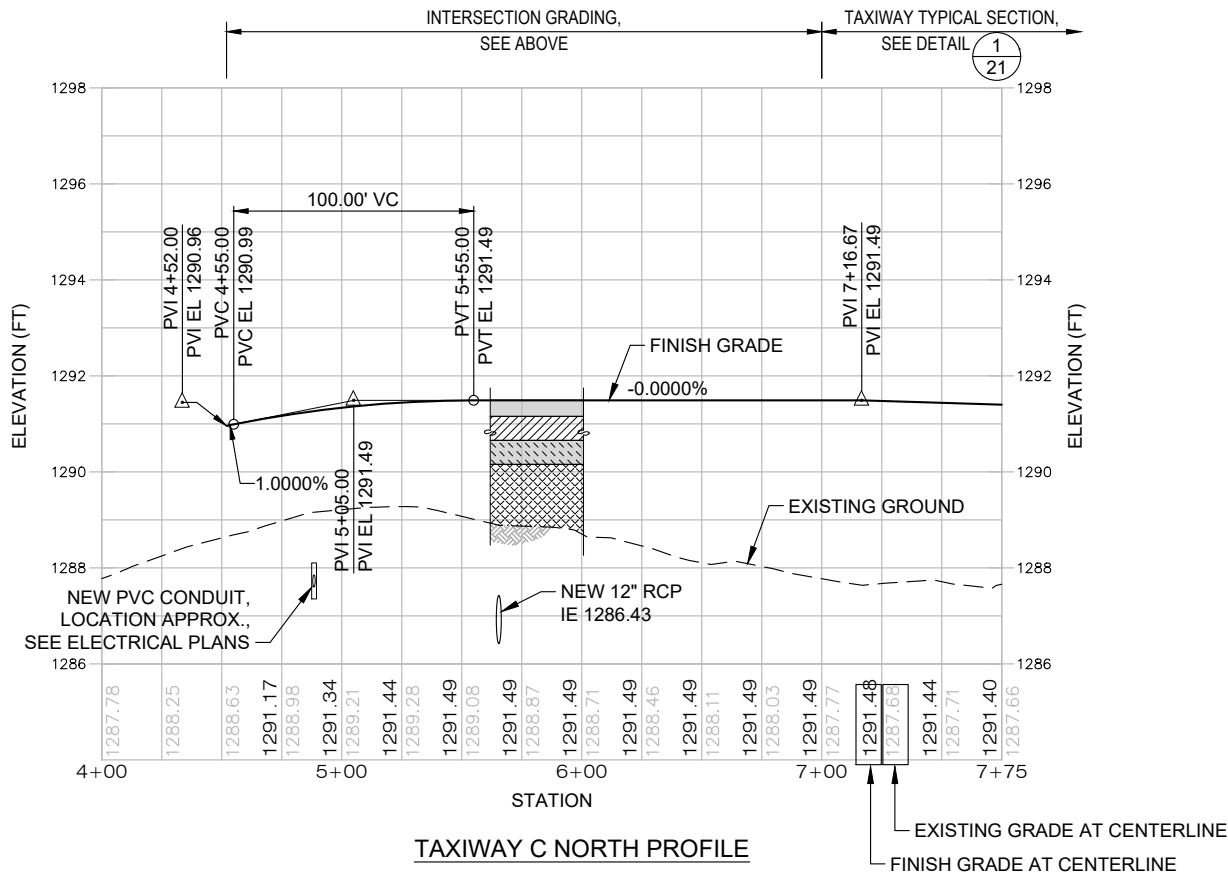
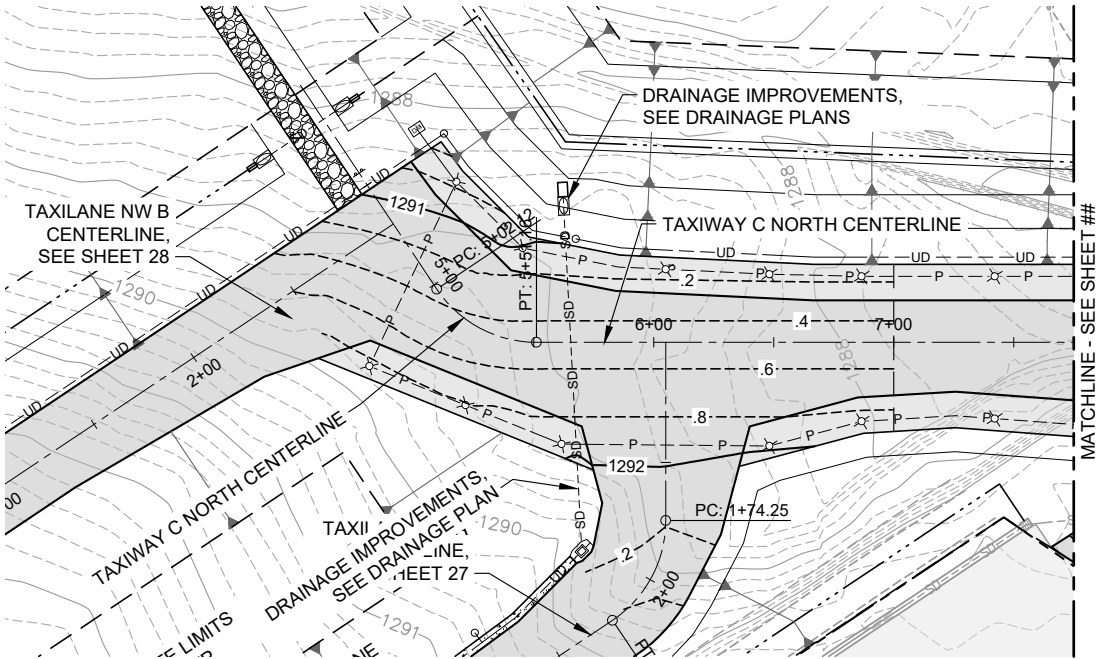
PAVEMENT DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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RVI-2020-W04

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OF 71

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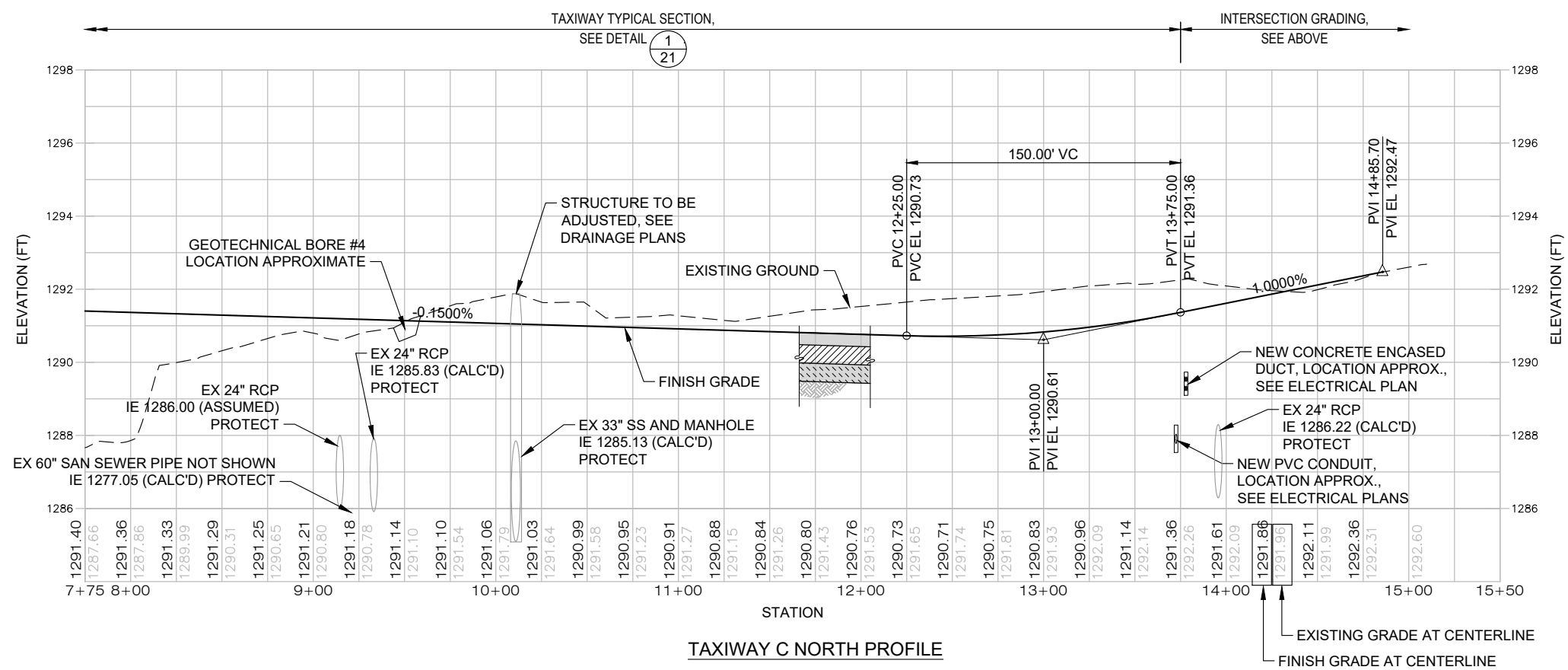
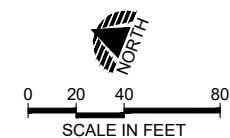


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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
PLAN AND PROFILE - TAXIWAY C NORTH
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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RVI-2020-W04
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

**PLAN AND PROFILE - TAXIWAY C
NORTH**

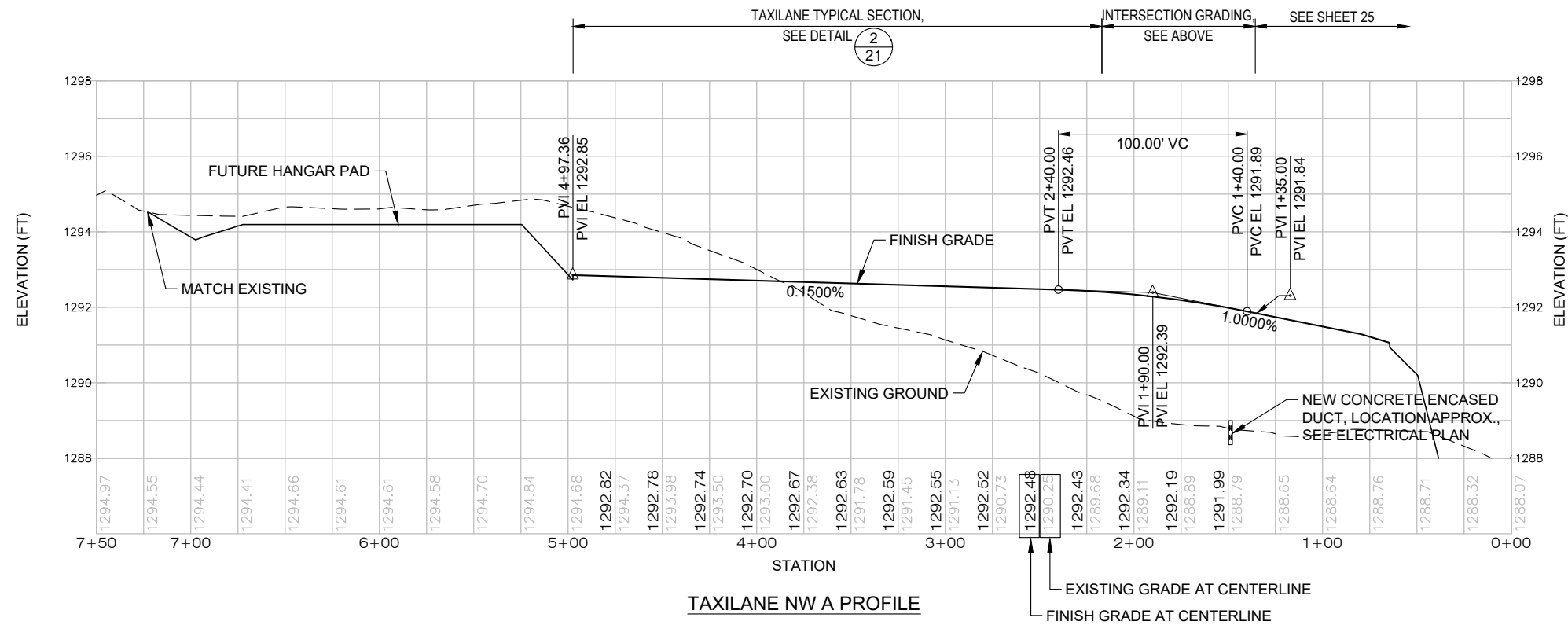
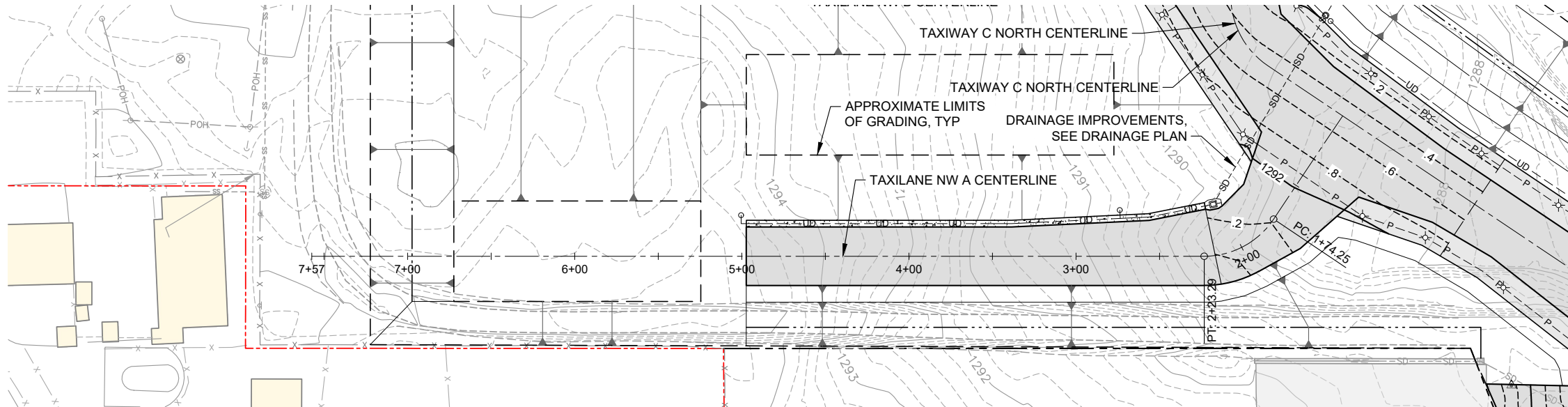
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26

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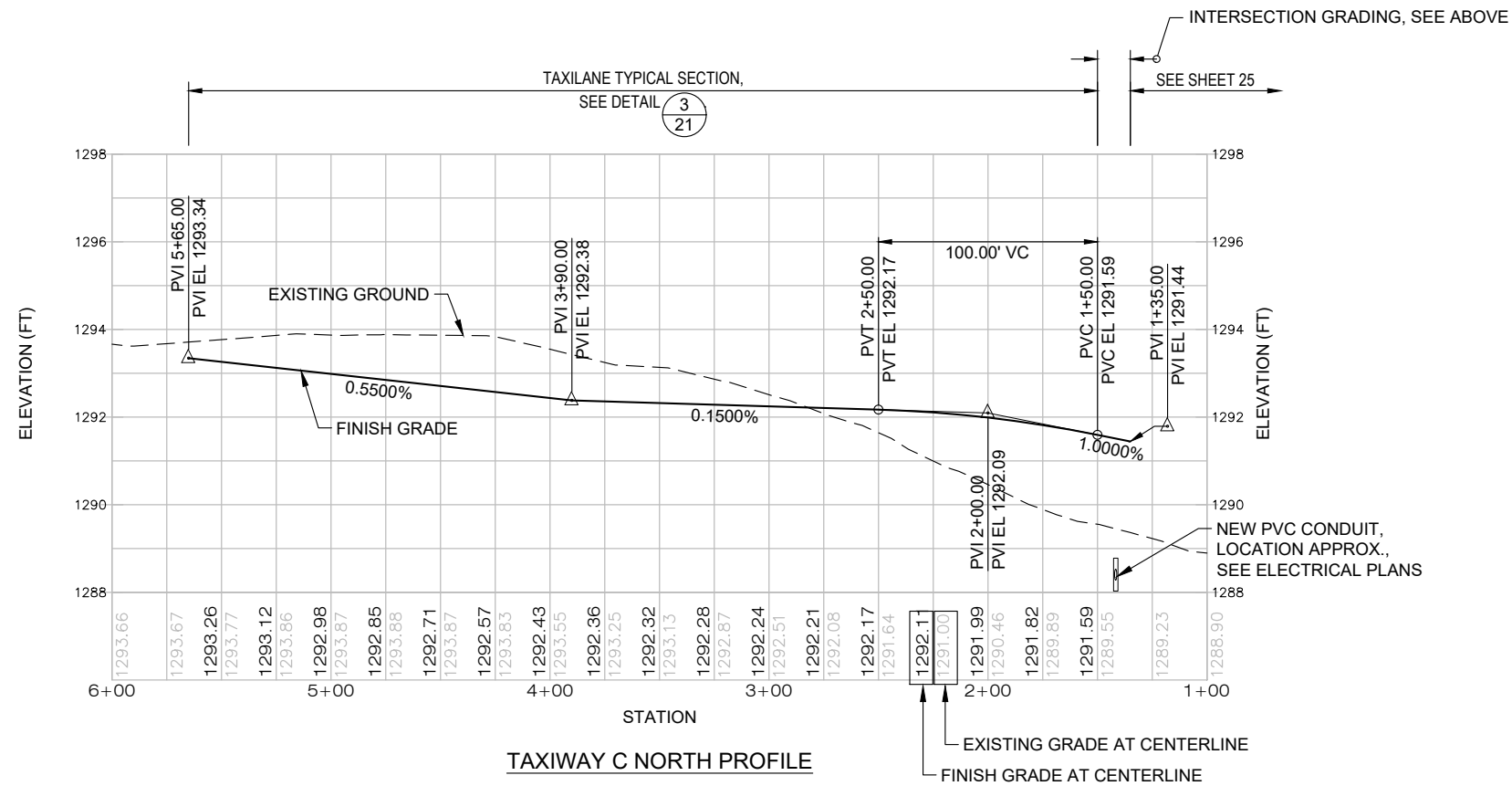
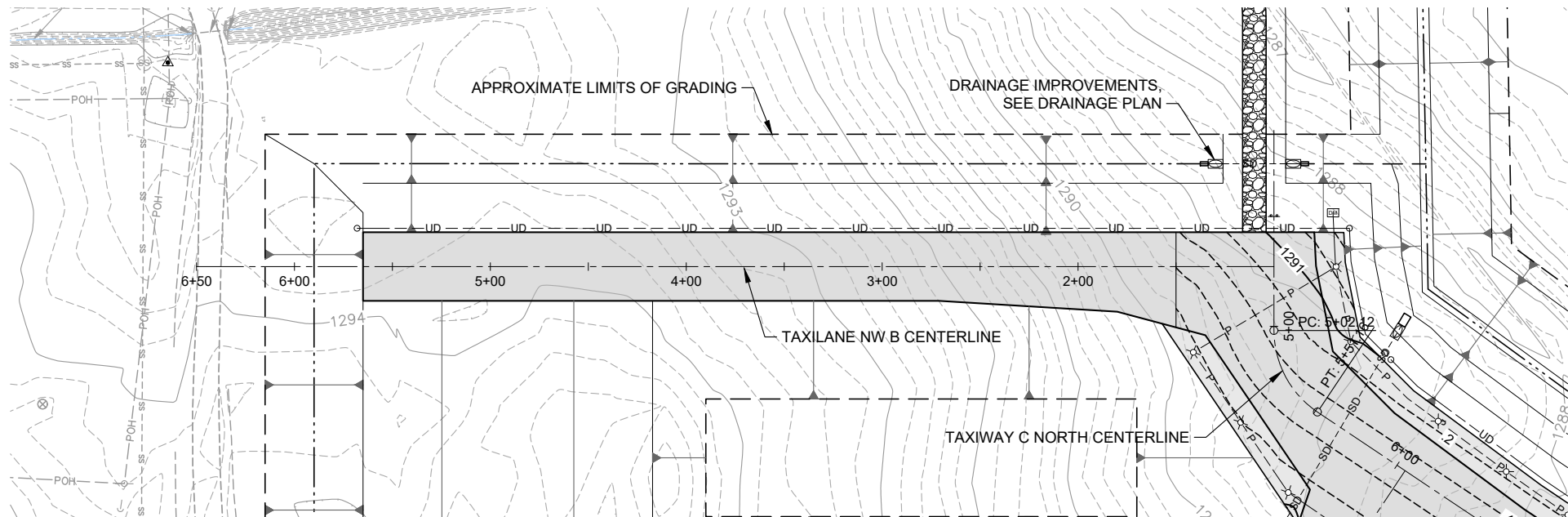
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
PLAN AND PROFILE - TAXILANE A NWA
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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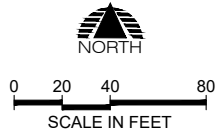
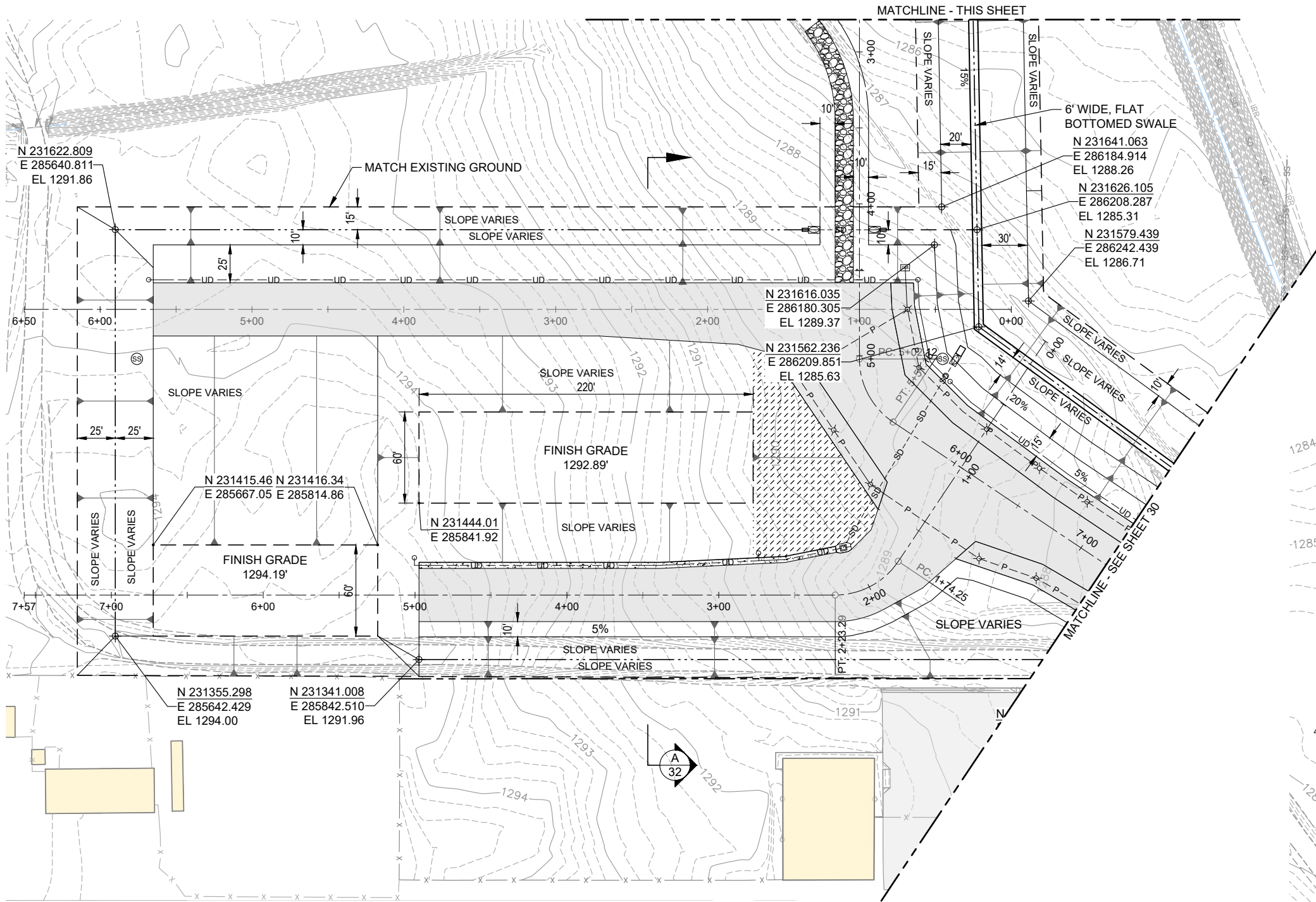
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
PLAN AND PROFILE - TAXILANE B NWA
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
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28
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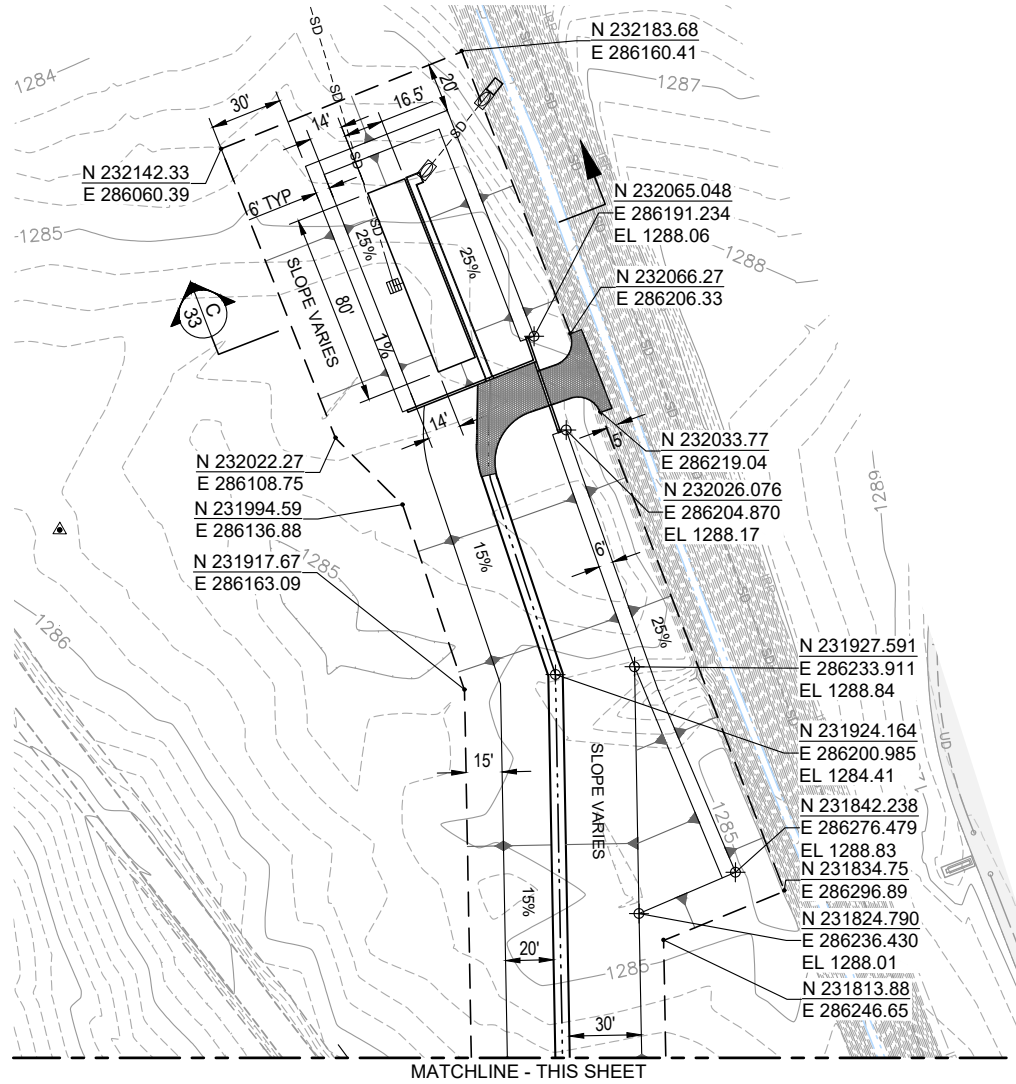


LEGEND

- RELOCATED AIRPORT OPERATIONS ROADWAY
- ASPHALT GRINDINGS, 6" MINIMUM DEPTH
- CLASS 100 RIPRAP, SEE DETAIL 5/22

NOTES:

- CONSTRUCT A 1.5" DROP AT THE EDGE OF PAVEMENT TO PREVENT MIGRATION OF SEDIMENT ONTO PAVED AREAS, TYPICAL.
- AREAS WITHIN LIMITS OF GRADING WITH EXISTING TOPSOIL SHALL BE STRIPPED OF ALL VEGETATION AND TOPSOIL., MINIMUM 6" DEPTH. TOPSOIL SHALL BE STOCKPILED FOR PLACEMENT AFTER ALL EXCAVATION AND EMBANKMENT CONSTRUCTION IS FINISHED. STOCKPILED TOPSOIL SHALL BE UTILIZED ON SITE OR DISPOSED OF OFFSITE, INCIDENTAL, MEASUREMENT AND PAYMENT SHALL BE FOR NEAT LINE QUANTITY OF TOPSOIL IN ITS ORIGINAL CONDITION, SEE SPECIFICATIONS.



PRECISION APPROACH
ENGINEERING
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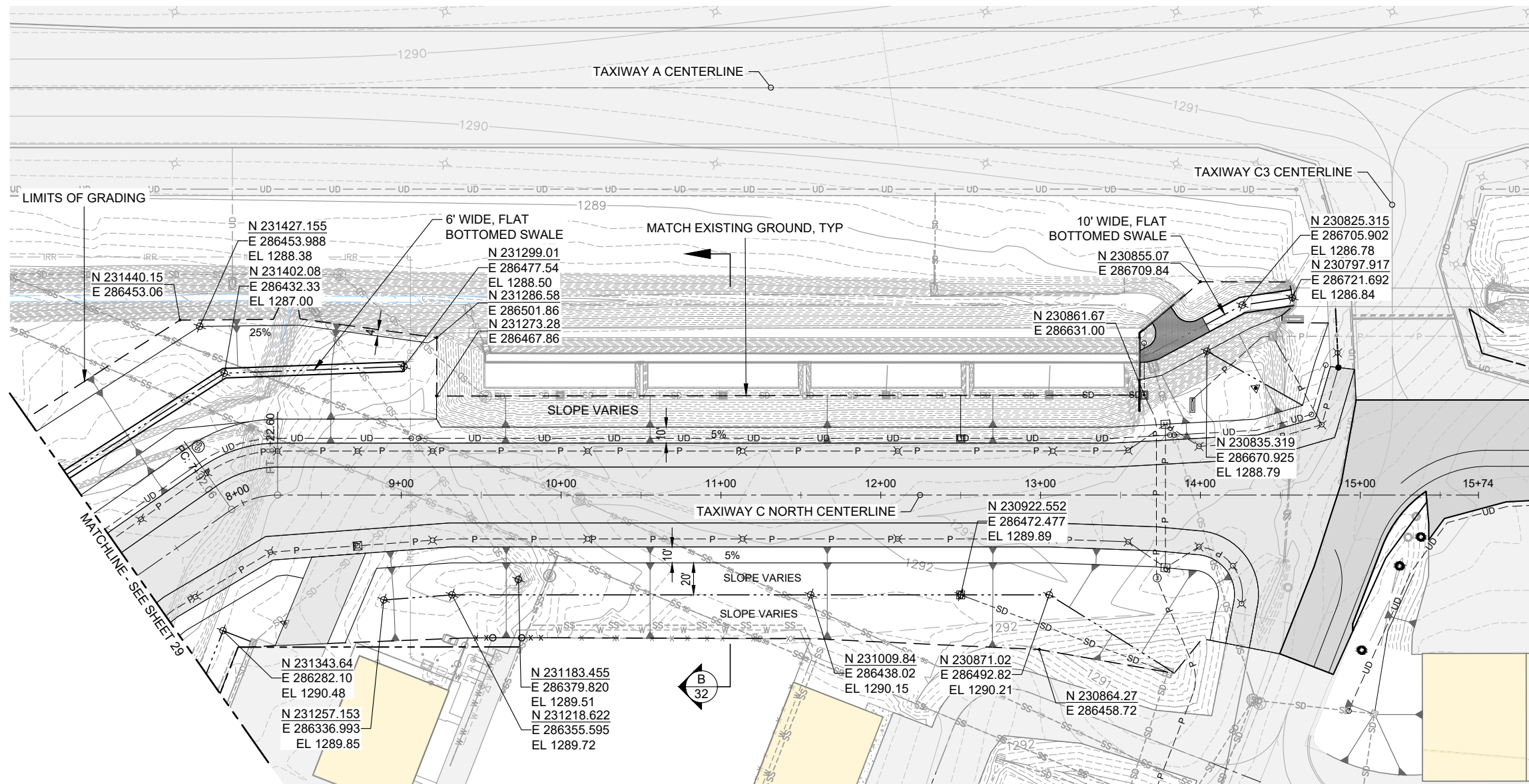
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
GRADING PLAN
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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541-754-0043



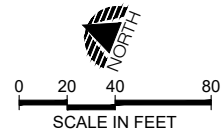
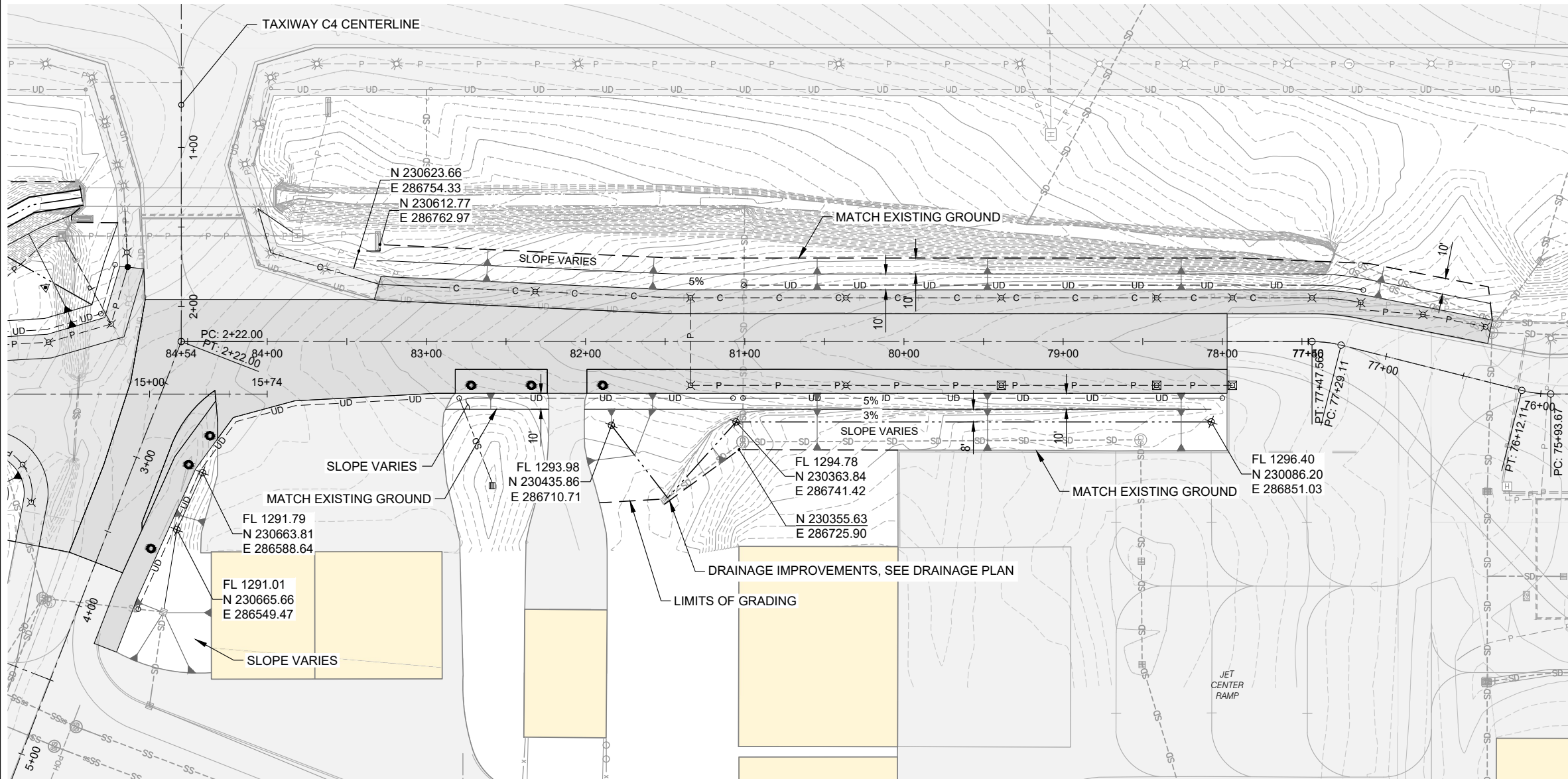
1 ADDENDUM NO. 1 5/24/2024 JGL
REVISIONS: DATE APPD.

DATE: MAY 2024
DESIGN: KJK
DRAWN: JAW
CHECKED: JGL
REVISION NUMBER: 0
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
GRADING PLAN
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04
SHEET NO.
30
OF 71

05/24/24 - 2:11pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_Grading-ADD.dwg



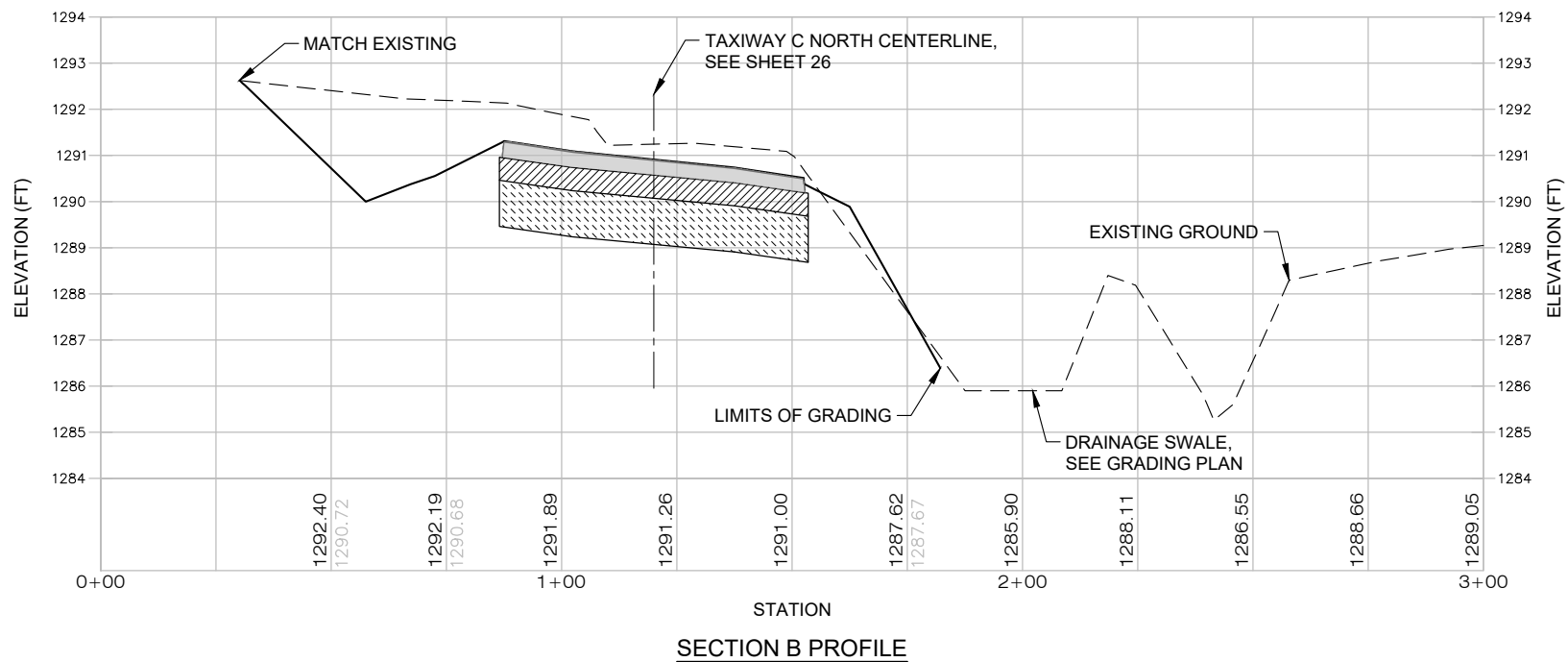
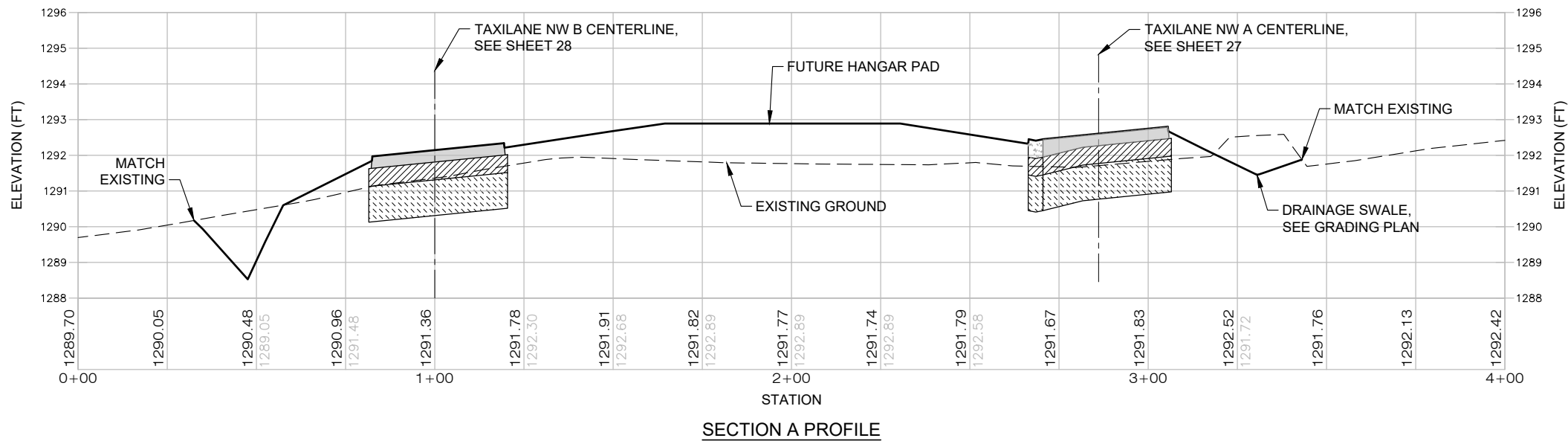
LEGEND

- RELOCATED AIRPORT OPERATIONS ROADWAY
- ASPHALT GRINDINGS, 6" MINIMUM DEPTH

1 REPLACE ENTIRE SHEET

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|--|-----------|---|------------------|-----------|---------|------------|--------|-------|---|---------------------------------|------------------|---|--------|----------|---|--|
| PRECISION APPROACH ENGINEERING 5125 Southwest Hout Street Corvallis, OR 97333 541•754•0043 | | <table><tr><td>DATE:</td><td>MAY 2024</td></tr><tr><td>DESIGN:</td><td>KJK</td></tr><tr><td>DRAWN:</td><td>JAW</td></tr><tr><td>CHECKED:</td><td>JGL</td></tr><tr><td>REVISION NUMBER:</td><td>0</td></tr><tr><td>SCALE:</td><td>AS SHOWN</td></tr></table> | DATE: | MAY 2024 | DESIGN: | KJK | DRAWN: | JAW | CHECKED: | JGL | REVISION NUMBER: | 0 | SCALE: | AS SHOWN | ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES | PROJECT NUMBER: RVI-2020-W04 |
| | | | DATE: | MAY 2024 | | | | | | | | | | | | |
| DESIGN: | KJK | | | | | | | | | | | | | | | |
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| CHECKED: | JGL | | | | | | | | | | | | | | | |
| REVISION NUMBER: | 0 | | | | | | | | | | | | | | | |
| SCALE: | AS SHOWN | | | | | | | | | | | | | | | |
| <table><tr><td>1 ADDENDUM NO. 1</td><td>5/24/2024</td><td>JGL</td></tr><tr><td>REVISIONS:</td><td>DATE</td><td>APPD.</td></tr></table> | | | 1 ADDENDUM NO. 1 | 5/24/2024 | JGL | REVISIONS: | DATE | APPD. | GRADING PLAN - ADDITIVE PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063 | SHEET NO. 31 OF 71 | | | | | | |
| 1 ADDENDUM NO. 1 | 5/24/2024 | JGL | | | | | | | | | | | | | | |
| REVISIONS: | DATE | APPD. | | | | | | | | | | | | | | |

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1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



ADDENDUM NO. 1
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5/24/2024 JGL
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

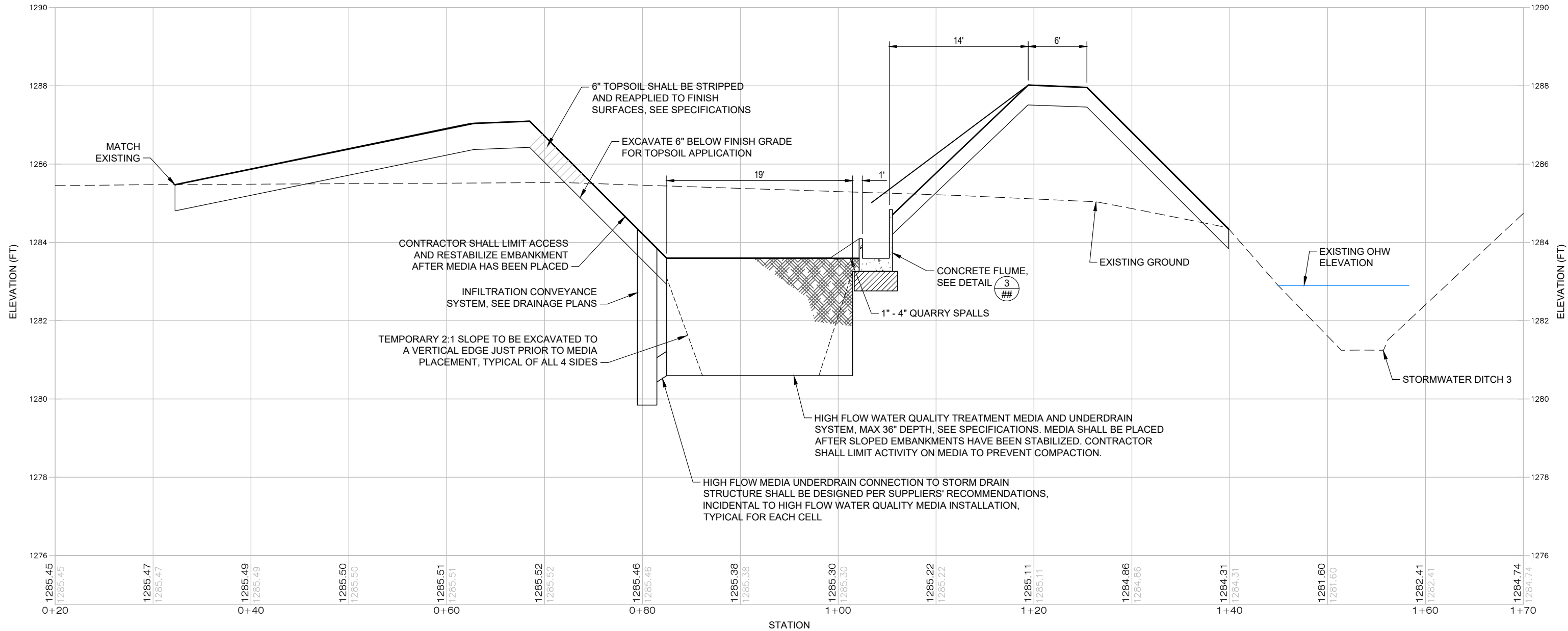
CROSS SECTIONS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
32
OF 71

05/24/24 - 2:12pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_Grading.dwg



1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



ADDENDUM NO. 1
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

CROSS SECTION

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
33
OF 71

05/24/24 - 2:12pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_Drainage.dwg

MATCHLINE - SEE SHEET 36



0 20 40 80
SCALE IN FEET

LEGEND

- 1 INSTALL AUTOMATIC POND INLET SYSTEM, SEE DETAIL ¹/₃₈
- 2 INSTALL NEW CATCH BASIN, SEE DETAIL ²/₃₈
- 3 INSTALL CATCH BASIN WITH CONCRETE DRAINAGE COLLAR, SEE DETAIL ¹/₄₅
- 4 INSTALL CONCRETE VALLEY GUTTER, SEE DETAIL ²/₄₅
- 5 INSTALL CATCH BASIN WITH SOLID LID, SEE DETAIL ²/₃₈
- 6 INSTALL SOLID PIPE AS SPECIFIED, SEE DETAIL ¹/₃₉
- 7 INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, NON-PAVED AREA, SEE DETAIL ¹/₄₀
- 8 INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, PAVED AREA, SEE DETAIL ²/₄₀
- 9 INSTALL UNDERDRAIN CONNECTION WITH CLEANOUT, SEE DETAIL ¹/₄₄
- 10 INSTALL CLEANOUT, SEE DETAIL ²/₄₄
- 11 CONNECT NEW PIPE TO NEW OR EXISTING PIPE, SEE DETAIL ³/₄₄
- 12 CONNECT TO EXISTING STRUCTURE. VERIFY INVERT WITH ENGINEER PRIOR TO EXCAVATING TRENCH. ALL CONNECTIONS SHALL BE WATERTIGHT
- 13 INSTALL CONCRETE FLUME, SEE DETAIL ³/₄₈
- 14 INSTALL CONCRETE SLOPED END, SEE DETAIL ²/₄₆
- 15 STORM DRAIN INLET/OUTLET PROTECTION, SEE DETAIL ⁴/₄₀
- 16 INSTALL LOW FLOW BYPASS BASIN, SEE DETAIL ¹/₄₇
- 17 INSTALL ADJUSTABLE POND INLET SYSTEM, SEE DETAIL ²/₄₁
- 18 INSTALL VALLEY GUTTER CLEANOUT, SEE DETAIL ⁵/₄₄
- 19 POND VALLEY GUTTER EXTENSION, SEE DETAIL ¹/₄₂
- 20 CONCRETE ENERGY DISSIPATION SYSTEM, SEE DETAIL ³/₄₂
- 21 ADJUST EXISTING DRAINAGE STRUCTURE, SEE DETAIL ³/₄₀
- 22 INSTALL 6" PERFORATED UNDERDRAIN PIPE IN EXISTING PAVEMENT, SEE DETAIL ¹/₄₁
- 23 POTENTIAL UTILITY CONFLICT, TYPICAL, POTHOLE TO VERIFY EXISTING UTILITY LOCATION AND ELEVATION, SEE NOTE 1 (NOT ALL LOCATION SHOWN)

NOTES

- IT IS ANTICIPATED EXISTING UTILITIES MAY CONFLICT WITH STORM PIPE CROSSINGS AT SOME LOCATIONS. CONTRACTOR SHALL LOCATE AND USE CARE TO PROTECT EXISTING UTILITIES DURING CONSTRUCTION, ANY CONFLICT FOUND SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEERS ATTENTION. CONTRACTOR SHALL VERIFY COMPATIBILITY WITH INTENDED IMPROVEMENTS AND PROVIDE ANY DEVIATIONS OR RECOMMENDED CHANGES TO ENGINEER FOR EVALUATION PRIOR TO PERFORMING ANY WORK.
- CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE AS NECESSARY TO INTERCEPT SURFACE WATER AND MAINTAIN EXISTING DRAINAGE SYSTEM CONNECTIVITY THROUGHOUT ALL PROJECT PHASES. NO DIRECT PAYMENT.
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- THE CONTRACTOR SHALL MODIFY THE EXISTING SOUTH TREATMENT STORMWATER FLUME BY CORING OR DRILLING 12, 3-INCH DIAMETER WEEP HOLES THROUGHOUT THE FLUME. WEEP HOLES SHALL BE PLACED EQUIDISTANT IN THE FLUME AND SHALL BE EVENLY DISTRIBUTED THROUGH THE FOUR STORMWATER TREATMENT CELLS. WEEP HOLES SHALL BE CONSTRUCTED FLUSH WITH THE BOTTOM OF THE FLUME. ALL WORK AND MATERIALS SHALL BE CONSIDERED INCIDENTAL TO THE SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM. SIMILAR TO NEW CONCRETE FLUME DETAIL.
- THE CONTRACTOR SHALL TEMPORARY BLOCK, PUMP OR OTHERWISE DIRECT STORMWATER FLOWS AROUND THE AREAS TO BE GRADED WHILE UNDER CONSTRUCTION, COORDINATE WITH RPR. NO DIRECT PAYMENT.
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1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



1 ADDENDUM NO. 1 5/24/2024 JGL
REVISIONS: DATE APPD.

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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE PLAN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

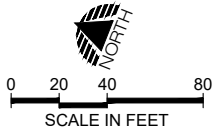
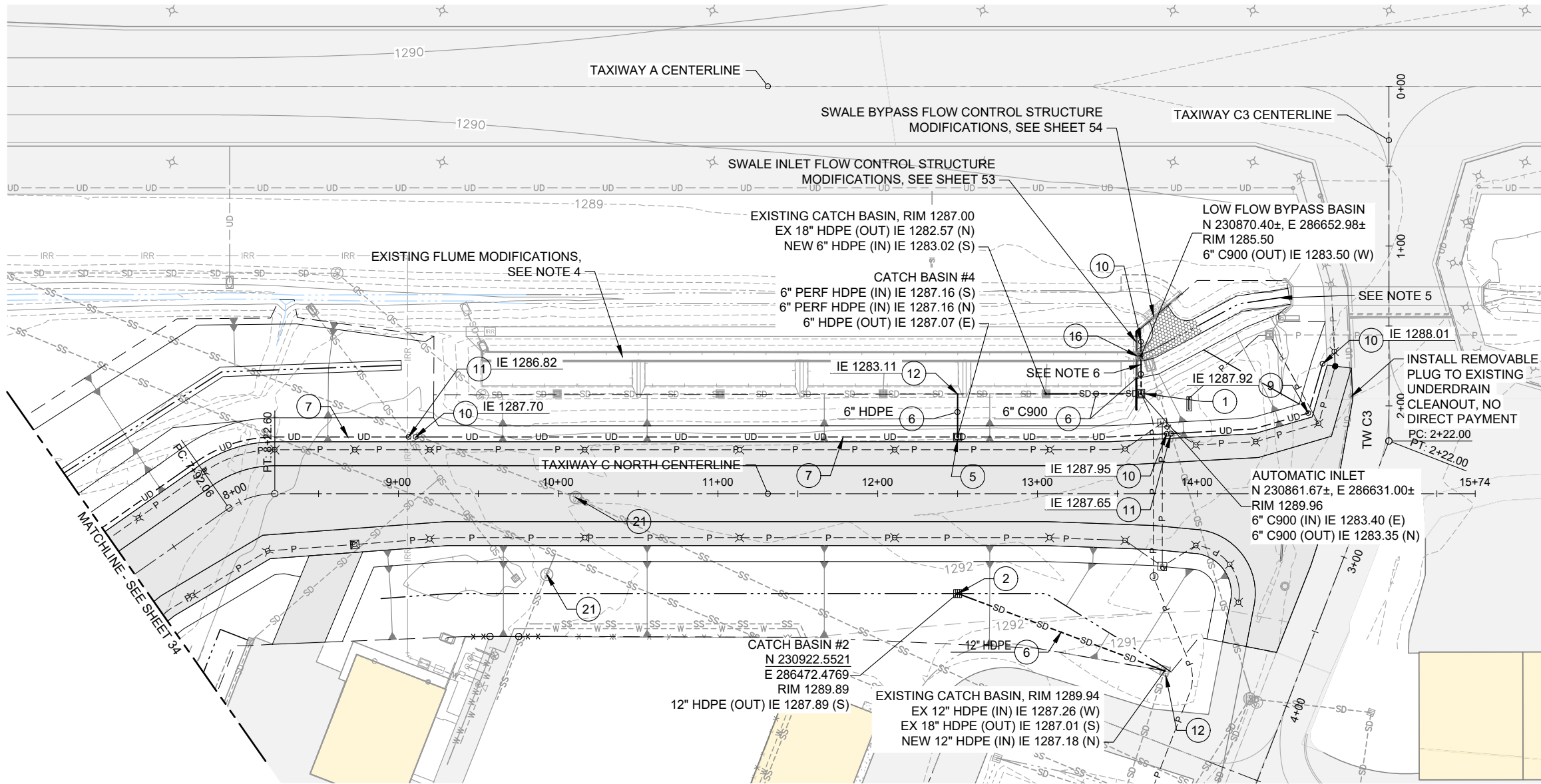
PROJECT NUMBER:
RVI-2020-W04

SHEET NO.

34

OF 71

05/24/24 - 2:12pm - BFortenberry - P:\R\RV\2020-W04 NW Devel\0400CAD\DWG\Sheets\RV\2020-W04_Drainage.dwg



LEGEND

- 1 INSTALL AUTOMATIC POND INLET SYSTEM, SEE DETAIL 1/38
- 2 INSTALL NEW CATCH BASIN, SEE DETAIL 2/38
- 3 INSTALL CATCH BASIN WITH CONCRETE DRAINAGE COLLAR, SEE DETAIL 1/45
- 4 INSTALL CONCRETE VALLEY GUTTER, SEE DETAIL 2/45
- 5 INSTALL CATCH BASIN WITH SOLID LID, SEE DETAIL 2/38
- 6 INSTALL SOLID PIPE AS SPECIFIED, SEE DETAIL 1/39
- 7 INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, NON-PAVED AREA, SEE DETAIL 1/40
- 8 INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, PAVED AREA, SEE DETAIL 2/40
- 9 INSTALL UNDERDRAIN CONNECTION WITH CLEANOUT, SEE DETAIL 1/44
- 10 INSTALL CLEANOUT, SEE DETAIL 2/44
- 11 CONNECT NEW PIPE TO NEW OR EXISTING PIPE, SEE DETAIL 3/44
- 12 CONNECT TO EXISTING STRUCTURE. VERIFY INVERT WITH ENGINEER PRIOR TO EXCAVATING TRENCH. ALL CONNECTIONS SHALL BE WATERTIGHT
- 13 INSTALL CONCRETE FLUME, SEE DETAIL 3/48
- 14 INSTALL CONCRETE SLOPED END, SEE DETAIL 2/46
- 15 STORM DRAIN INLET/OUTLET PROTECTION, SEE DETAIL 4/40
- 16 INSTALL LOW FLOW BYPASS BASIN, SEE DETAIL 1/47
- 17 INSTALL ADJUSTABLE POND INLET SYSTEM, SEE DETAIL 2/41
- 18 INSTALL VALLEY GUTTER CLEANOUT, SEE DETAIL 5/44
- 19 POND VALLEY GUTTER EXTENSION, SEE DETAIL 1/42
- 20 CONCRETE ENERGY DISSIPATION SYSTEM, SEE DETAIL 3/42
- 21 ADJUST EXISTING DRAINAGE STRUCTURE, SEE DETAIL 3/40
- 22 INSTALL 6" PERFORATED UNDERDRAIN PIPE IN EXISTING PAVEMENT, SEE DETAIL 1/41
- 23 POTENTIAL UTILITY CONFLICT, TYPICAL, POTHOLE TO VERIFY EXISTING UTILITY LOCATION AND ELEVATION, SEE NOTE 1 (NOT ALL LOCATION SHOWN)

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1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



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| 1 ADDENDUM NO. 1 | 5/24/2024 | JGL |
| REVISIONS: | DATE | APPD. |

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| DATE: | MAY 2024 |
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| REVISION NUMBER: | 0 |
| SCALE: | AS SHOWN |

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

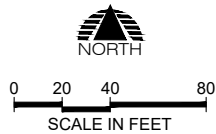
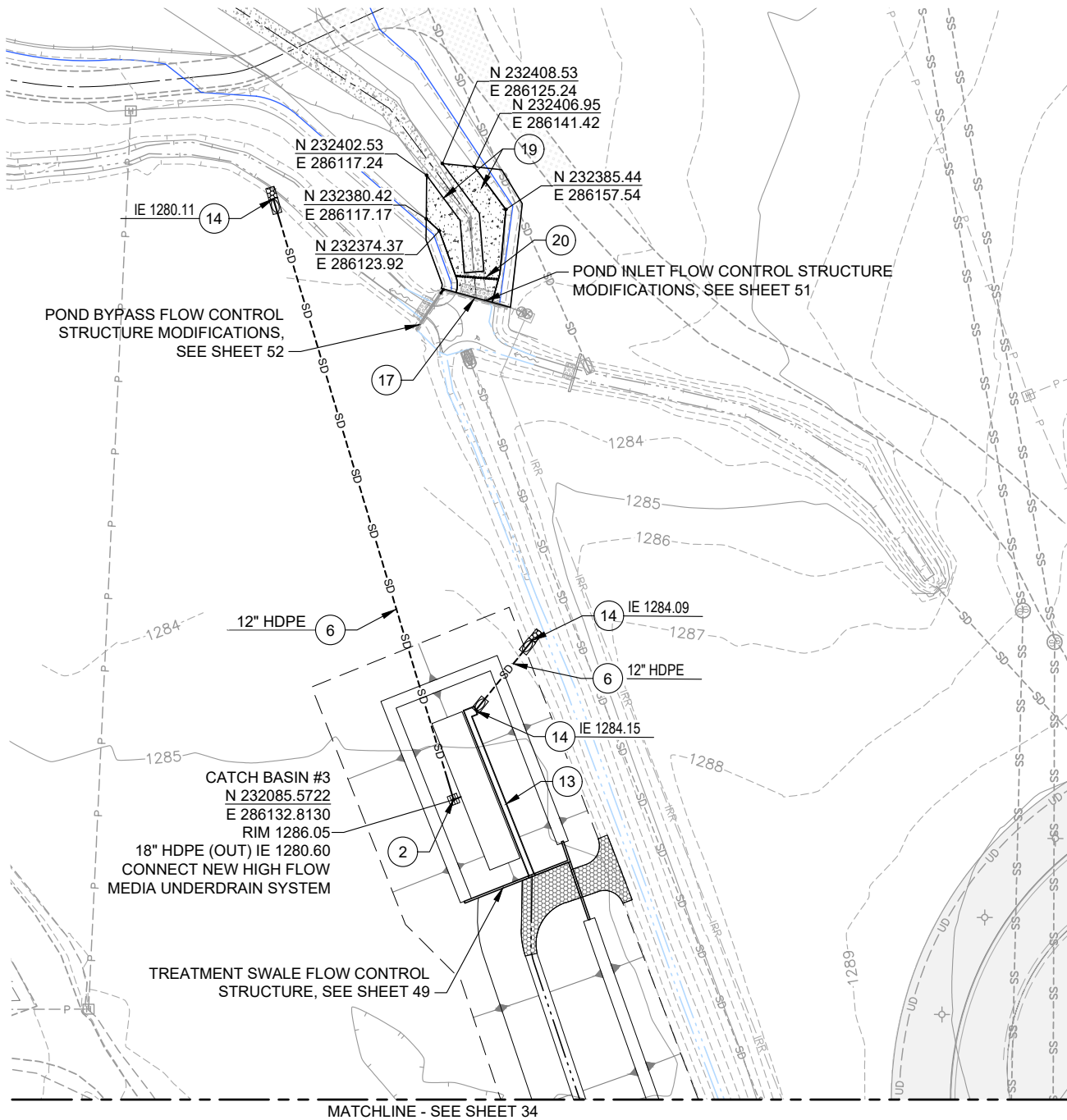
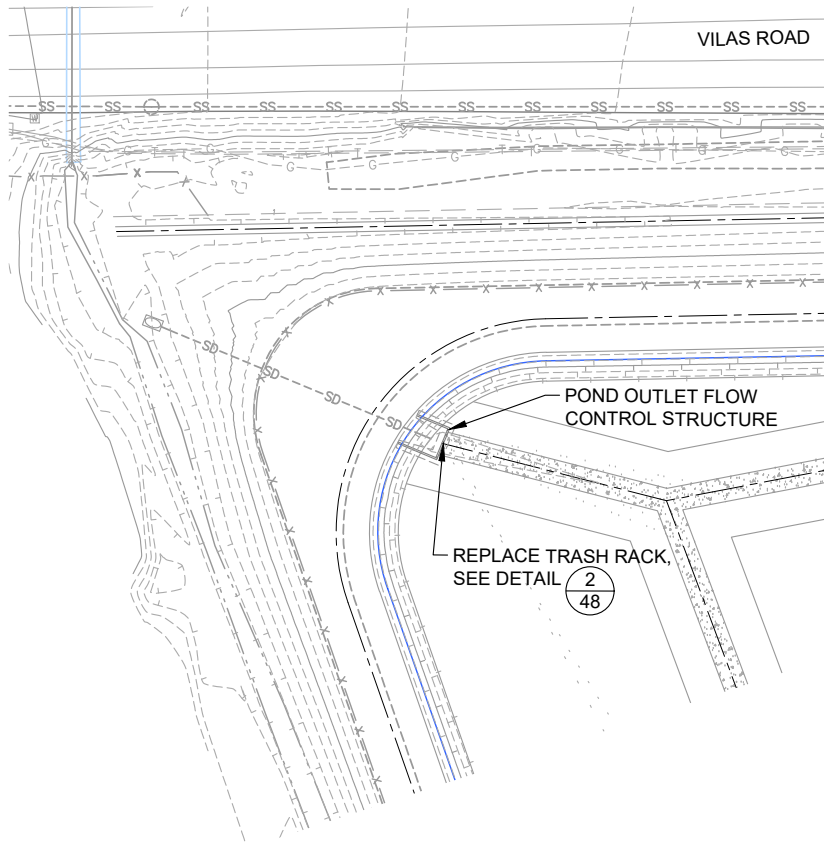
DRAINAGE PLAN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RV-2020-W04

SHEET NO.
35
OF 71

05/24/24 - 2:12pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_Drainage.dwg



LEGEND

- 1 INSTALL AUTOMATIC POND INLET SYSTEM, SEE DETAIL 1/38
- 2 INSTALL NEW CATCH BASIN, SEE DETAIL 2/38
- 3 INSTALL CATCH BASIN WITH CONCRETE DRAINAGE COLLAR, SEE DETAIL 1/45
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1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



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| 1 | ADDENDUM NO. 1 | 5/24/2024 | JGL |
| REVISIONS: | | DATE | APPD. |

DATE: MAY 2024
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

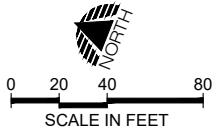
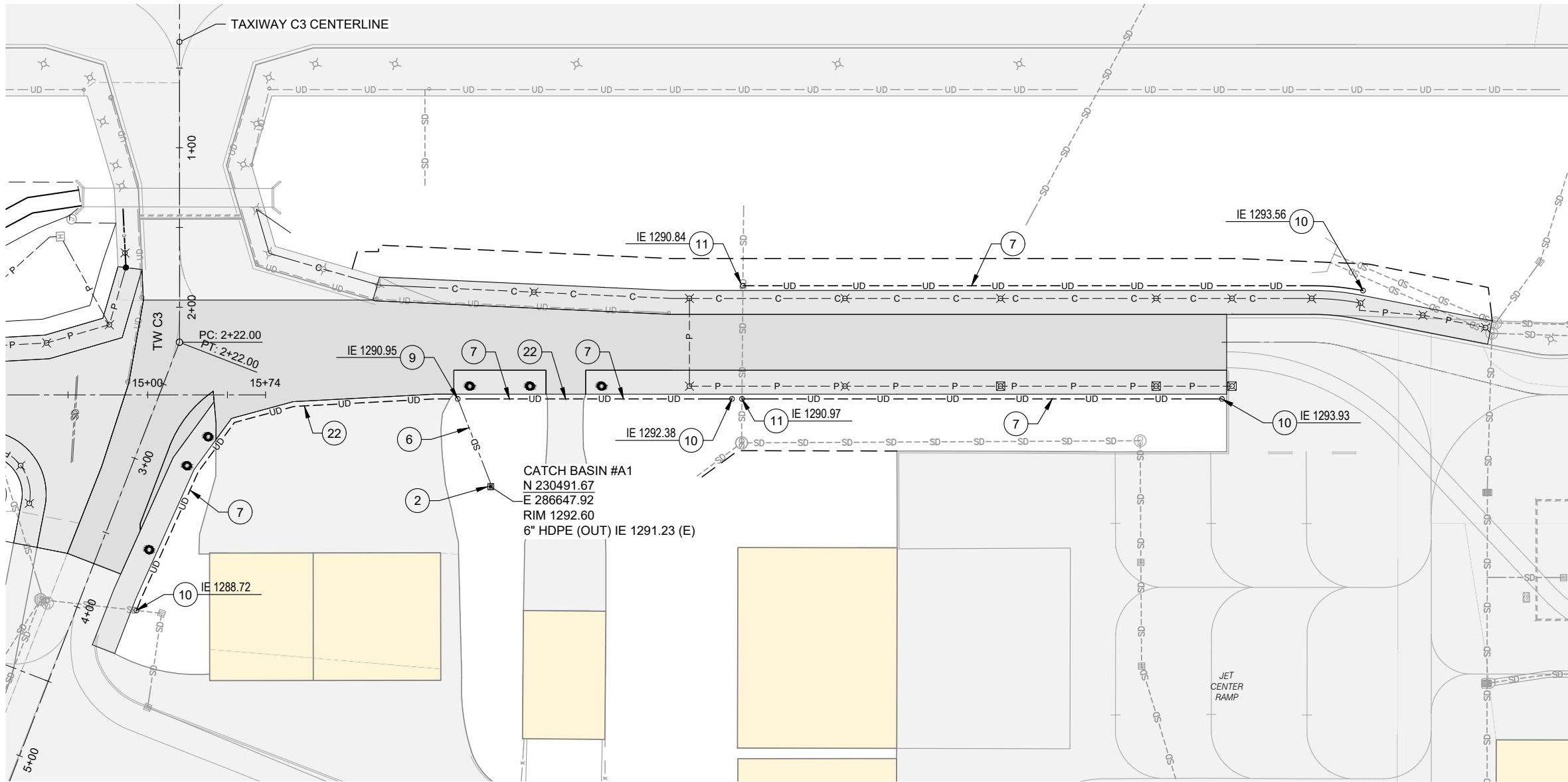
DRAINAGE PLAN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
36
OF 71

05/24/24 - 2:13pm - BFortenberry - P:\RVI-2020-W04 NW Deve\0400CAD\DWG\Sheets\RVI-2020-W04_Drainage-ADD.dwg



LEGEND

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- 2 INSTALL NEW CATCH BASIN, SEE DETAIL 2/38
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- IT IS ANTICIPATED EXISTING UTILITIES MAY CONFLICT WITH STORM PIPE CROSSINGS AT SOME LOCATIONS. CONTRACTOR SHALL LOCATE AND USE CARE TO PROTECT EXISTING UTILITIES DURING CONSTRUCTION, ANY CONFLICT FOUND SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEERS ATTENTION. CONTRACTOR SHALL VERIFY COMPATIBILITY WITH INTENDED IMPROVEMENTS AND PROVIDE ANY DEVIATIONS OR RECOMMENDED CHANGES TO ENGINEER FOR EVALUATION PRIOR TO PERFORMING ANY WORK.
- CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE AS NECESSARY TO INTERCEPT SURFACE WATER AND MAINTAIN EXISTING DRAINAGE SYSTEM CONNECTIVITY THROUGHOUT ALL PROJECT PHASES. NO DIRECT PAYMENT.
- PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL VERIFY THE LOCATION AND INVERTS OF ALL EXISTING DRAINAGE INFRASTRUCTURE. ANY DISCREPANCIES BETWEEN THE PLANS AND FIELD CONDITIONS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE RPR.
- THE CONTRACTOR SHALL MODIFY THE EXISTING SOUTH TREATMENT STORMWATER FLUME BY CORING OR DRILLING 12, 3-INCH DIAMETER WEEP HOLES THROUGHOUT THE FLUME. WEEP HOLES SHALL BE PLACED EQUIDISTANT IN THE FLUME AND SHALL BE EVENLY DISTRIBUTED THROUGH THE FOUR STORMWATER TREATMENT CELLS. WEEP HOLES SHALL BE CONSTRUCTED FLUSH WITH THE BOTTOM OF THE FLUME. ALL WORK AND MATERIALS SHALL BE CONSIDERED INCIDENTAL TO THE SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM. SIMILAR TO NEW CONCRETE FLUME DETAIL.
- THE CONTRACTOR SHALL TEMPORARY BLOCK, PUMP OR OTHERWISE DIRECT STORMWATER FLOWS AROUND THE AREAS TO BE GRADED WHILE UNDER CONSTRUCTION, COORDINATE WITH RPR. NO DIRECT PAYMENT.
- ALL WORK AND MATERIALS TO CONSTRUCT THE SOUTH TREATMENT LOW FLOW BYPASS SYSTEM, EXCEPT FOR THE UPSTREAM CLEANOUT AND CONNECTION TO EXISTING STRUCTURE, INCLUDING THE STRUCTURE, AGGREGATE FILL, C900 PIPE, TRENCHING, EXCAVATING AND BACKFILLING THE PIPE SHALL BE PAID UNDER THE SOUTH TREATMENT LOW FLOW BYPASS BID ITEM.

REPLACE ENTIRE SHEET

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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE PLAN - ADDITIVE

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

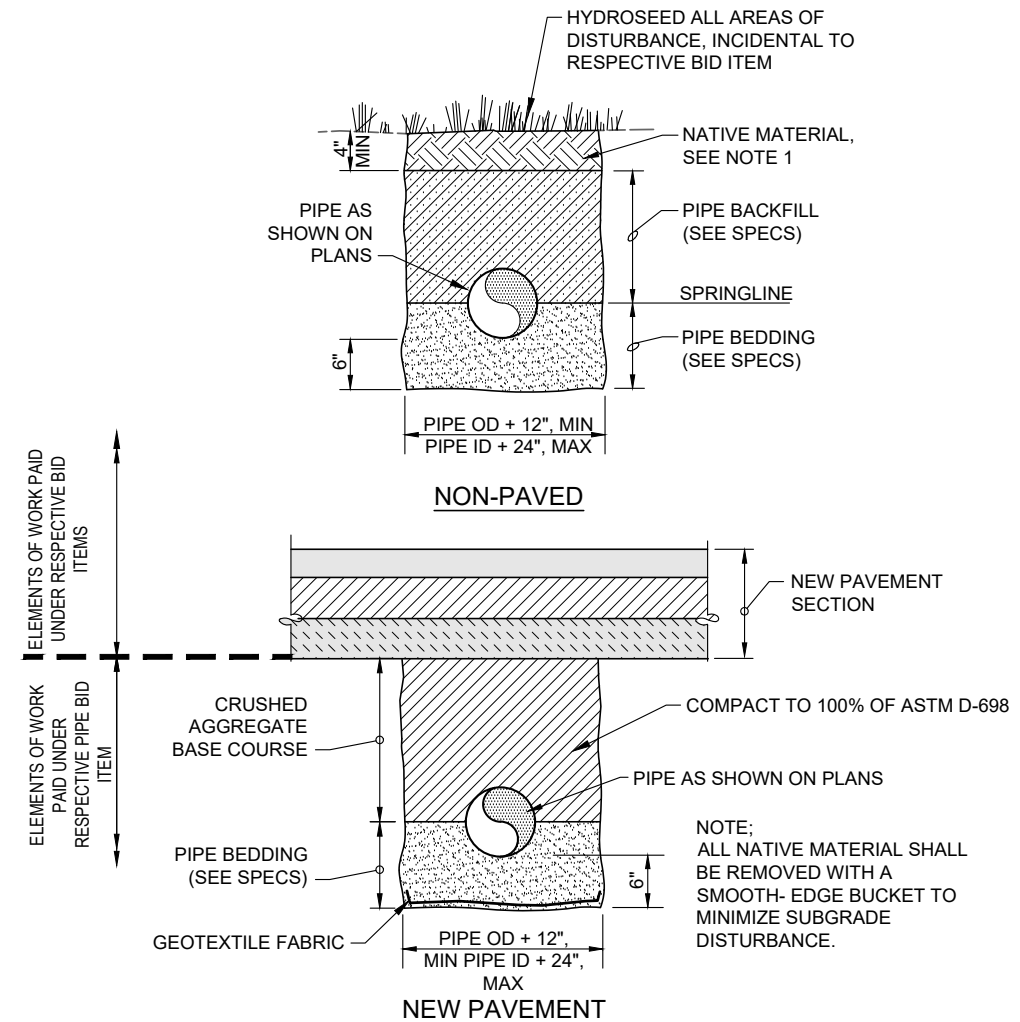
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SOLID STORM DRAIN PIPE DETAIL
NTS

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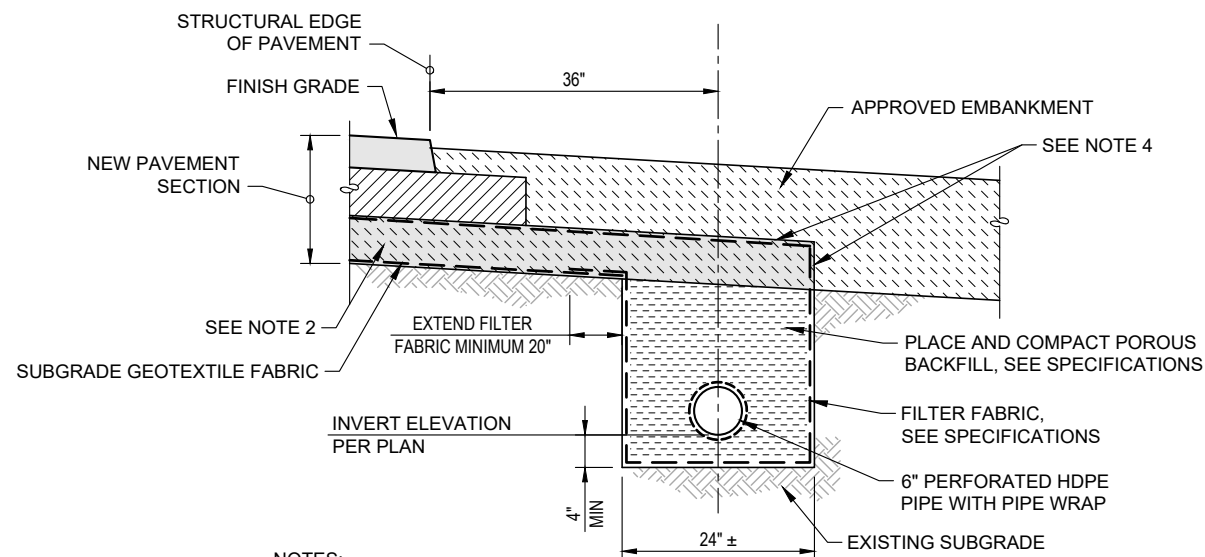
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
DRAINAGE DETAILS
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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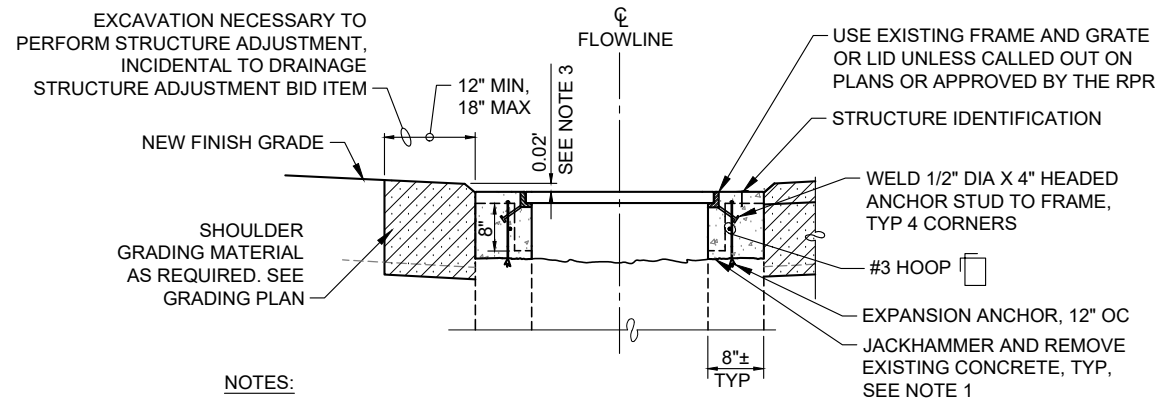
NOTES:

1. FILTER FABRIC TRENCH WRAP, PIPE WRAP, EXCAVATION, AND POROUS BACKFILL TO CONSTRUCTED THE UNDERDRAIN BELOW THE P-407 ASPHALT TREATED PERMEABLE BASE COURSE OR P-154 SUBBASE COURSE SHALL BE PAID UNDER THE 6-INCH PERFORATED UNDERDRAIN PIPE, NON-PAVED AREA BID ITEM.
2. EXTEND P-407 ASPHALT TREATED PERMEABLE BASE COURSE OR P-154 SUBBASE COURSE TO THE EDGE OF THE UNDERDRAIN TRENCH. SUBJECT TO APPROVAL OF THE RPR.
3. PERFORATED PIPE SHALL HAVE A SMOOTH AND UNINTERRUPTED FLOW LINE. PERFORATIONS SHALL ONLY BE INSTALLED ABOVE PIPE SPRING LINE.
4. WHEN PLACING P-407 ASPHALT TREATED PERMEABLE BASE COURSE, EXTEND DRAINAGE GEOTEXTILE FABRIC TO THE LIMITS OF THE EXTERIOR EDGE OF TRENCH. WHEN PLACING P-154 SUBBASE COURSE, EXTEND FILTER FABRIC FROM THE EXTERIOR EDGE OF THE TRENCH TO 20 INCHES BEYOND THE INTERIOR EDGE OF TRENCH.

UNDERDRAIN
PIPE, NON-PAVED AREA DETAIL

NTS

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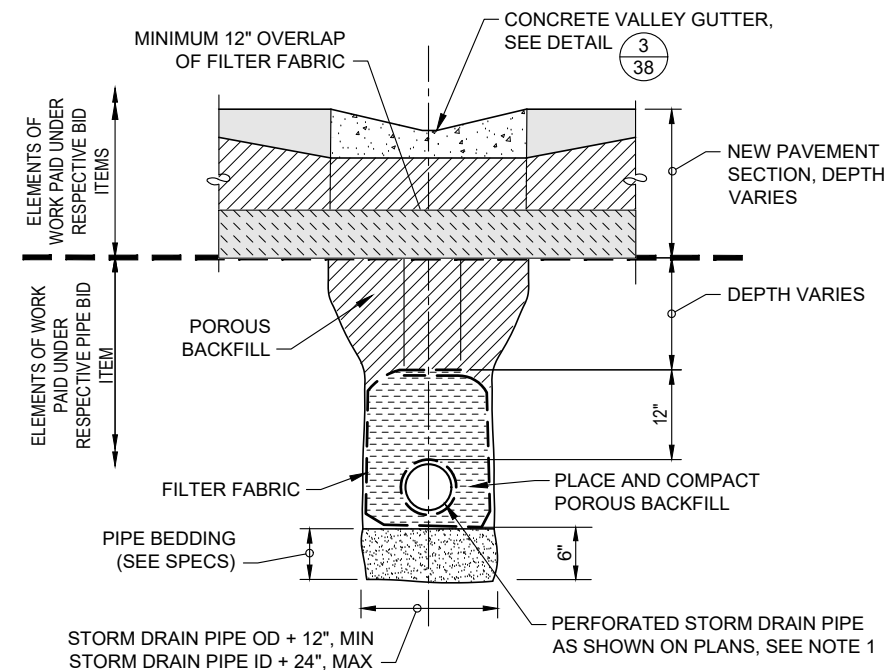
NOTES:

1. REMOVE TOP PORTION OF EXISTING CONCRETE AND ADJUST STRUCTURE WITH NEW CONCRETE TO REQUIRED ELEVATION. PROVIDE MINIMUM OF 8" OF NEW CONCRETE WHETHER RAISING OR LOWERING THE STRUCTURE.
2. FIELD CONDITIONS MAY VARY. VERIFY ALL EXISTING STRUCTURE DIMENSIONS PRIOR TO BEGINNING WORK. OBTAIN ENGINEER'S APPROVAL IF MODIFICATIONS ARE NECESSARY.
3. SOLID LID STRUCTURES TO BE INSTALLED FLUSH WITH FINISH GRADES.
4. ALL NEWLY INSTALLED COMPONENTS OF THE ADJUSTED STRUCTURE SHALL BE 50 KIP LOAD RATED.

DRAINAGE STRUCTURE
ADJUSTMENT DETAIL

NTS

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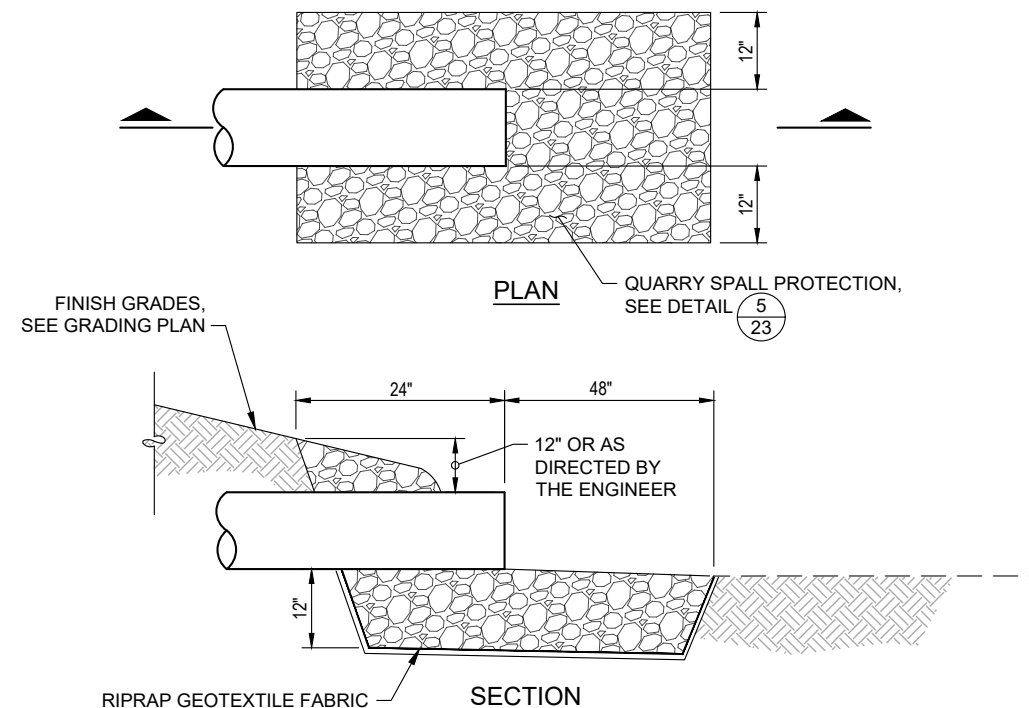
NOTES:

1. PERFORATED PIPE SHALL HAVE A SMOOTH AND UNINTERRUPTED FLOW LINE. PERFORATIONS SHALL ONLY BE INSTALLED ABOVE PIPE SPRING LINE

UNDERDRAIN PIPE
PAVED AREA DETAIL

NTS

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STORM DRAIN
INLET/OUTLET PROTECTION DETAIL

NTS

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

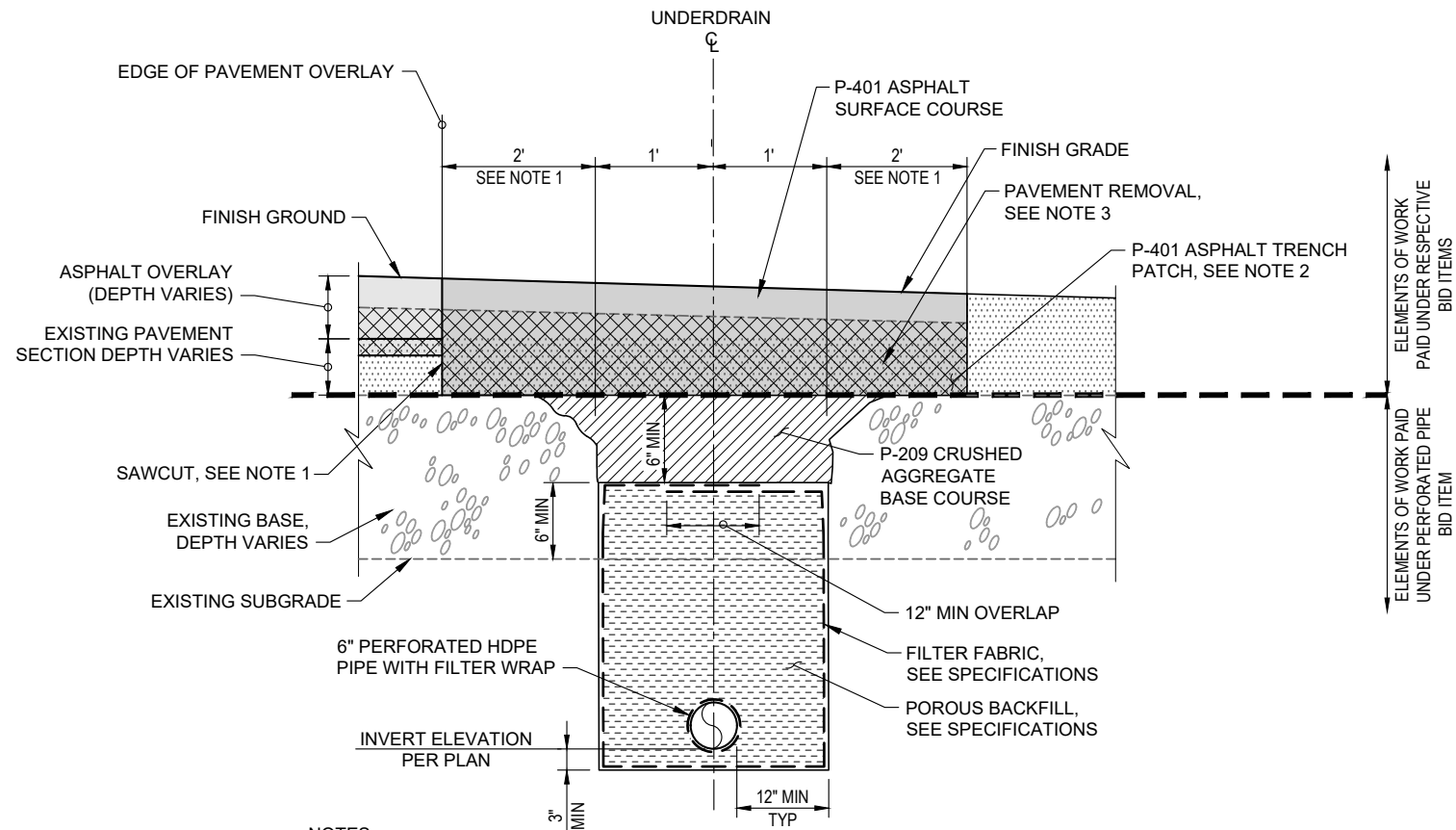
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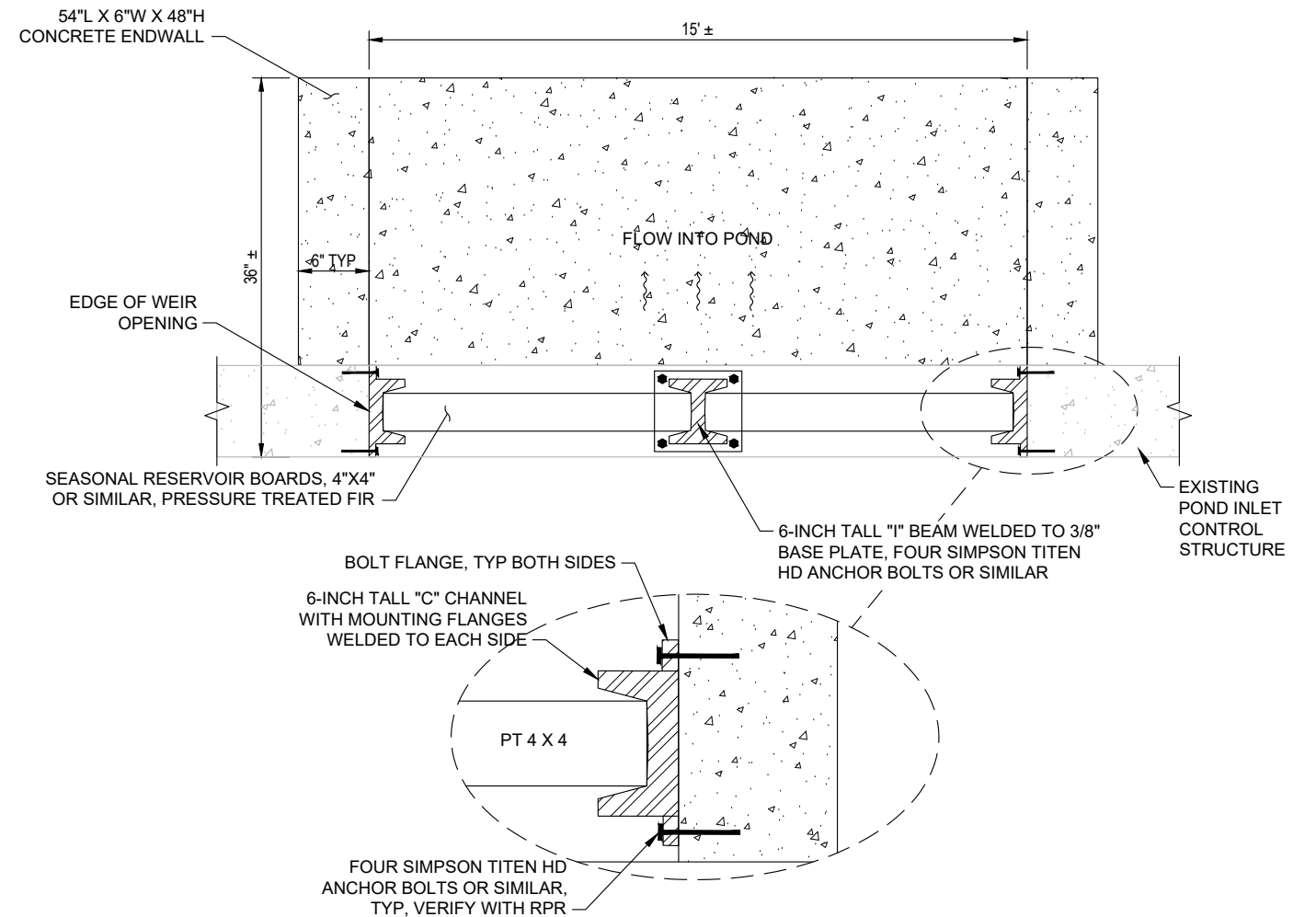
NOTES:

1. CONTRACTOR SHALL TAKE CARE TO PREVENT UNDERMINING OF EXISTING PAVEMENT. IF UNDERMINING OCCURS, CONTRACTOR SHALL MAKE AN ADDITIONAL SAWCUT TO THE INITIAL SAWCUT EDGE TO REMOVE AND REPLACE ANY DAMAGED PAVEMENT AT NO ADDITIONAL COST TO THE OWNER.
2. ASPHALT TRENCH PATCH SHALL BE PAID UNDER THE ASPHALT SURFACE COURSE BID ITEM, MINIMUM 3 INCHES, MAXIMUM 4 INCHES.
3. PRIOR TO PAVEMENT REMOVAL CONTRACTOR SHALL MARK UNDERDRAIN TRENCH PAVEMENT REMOVAL LIMITS. SAWCUTTING AND FULL DEPTH PAVEMENT REMOVAL SHALL BE PAID UNDER THE ASPHALT PAVEMENT REMOVAL, FULL DEPTH BID ITEM.

UNDERDRAIN PIPE,
EXISTING PAVED AREA DETAIL

NTS

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41



NOTES:

1. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS TO RPR FOR REVIEW PRIOR TO CONSTRUCTION
2. PLACE EXISTING ~8" COBBLES ADJACENT TO STRUCTURE. VERIFY COBBLE SIZE AND LIMITS WITH RPR PRIOR TO CONSTRUCTION.

ADJUSTABLE
POND INLET SYSTEM DETAIL

NTS

2
41

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE DETAILS

PRECISION APPROACH ENGINEERING, INC.
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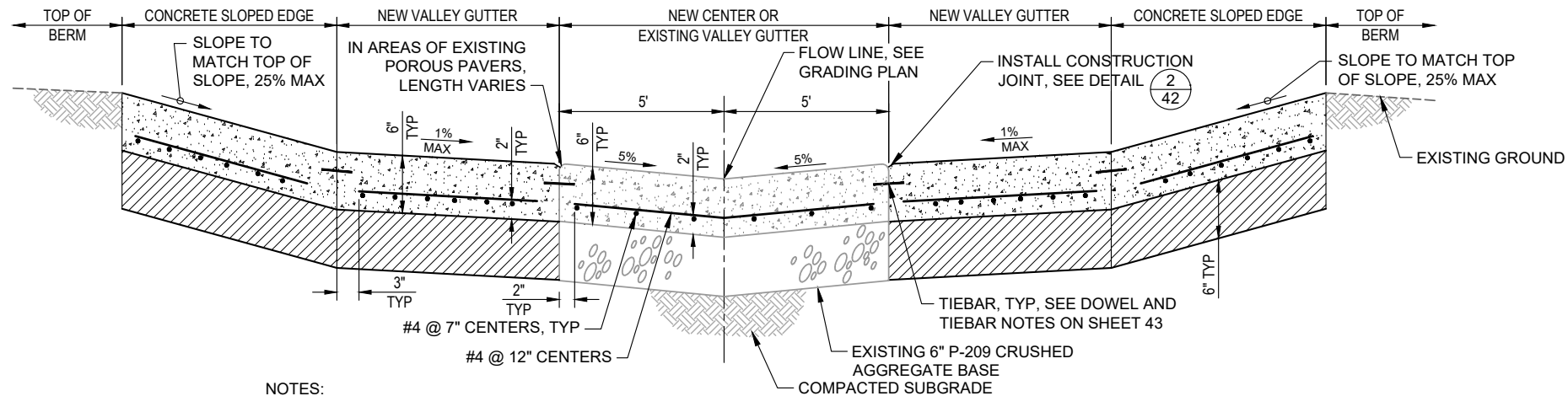
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NOTES:

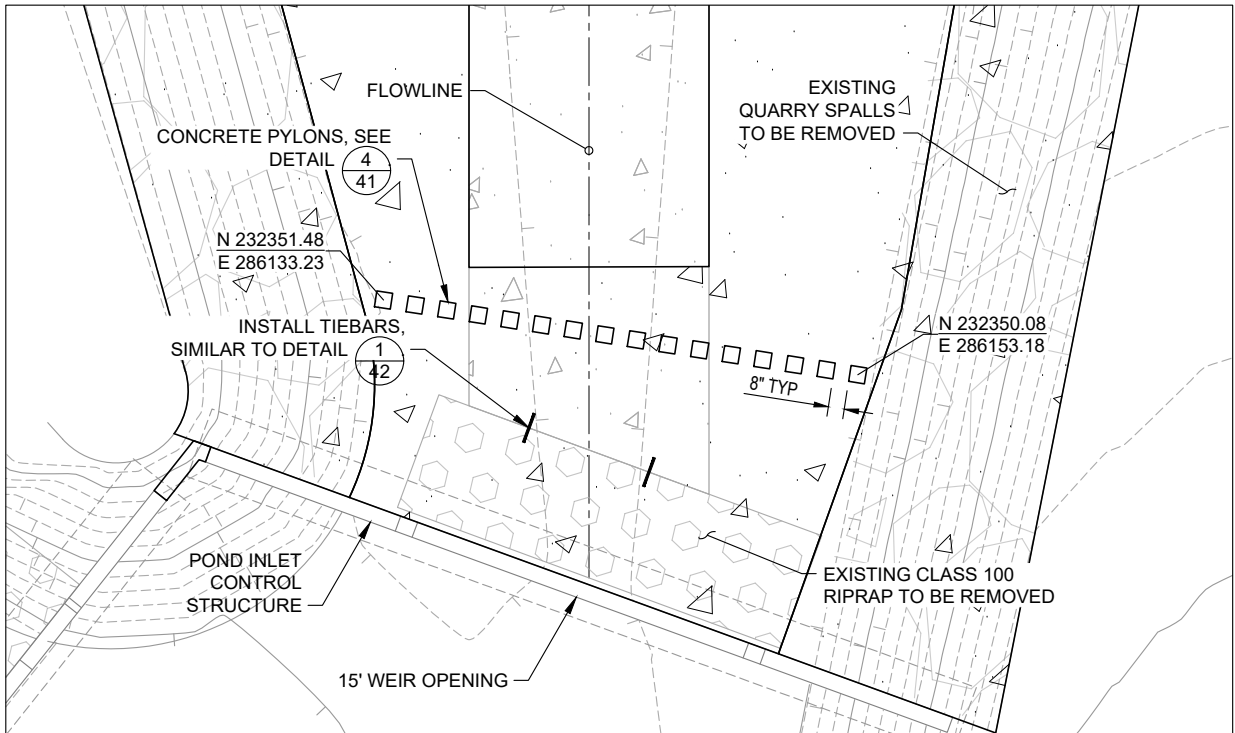
1. TRANSVERSE CONTRACTION JOINTS SHALL BE INSTALLED IN LINE WITH EXISTING JOINTS, VERIFY WITH RPR.
2. ALL WORK AND MATERIALS, EXCLUDING REQUIRED EXCAVATION AND POROUS PAVR REMOVAL, TO CONSTRUCT AND COMPLETE THE POND VALLEY GUTTER EXTENSION AS SHOWN SHALL BE PAID UNDER THE POND VALLEY GUTTER EXTENSION BID ITEM.
3. INSTALL NO. 4, 20" LONG, DEFORMED STEEL TIEBARS 36" O.C. BETWEEN NEW AND EXISTING VALLEY GUTTER, AND BETWEEN THE NEW SHALLOW SLOPED VALLEY GUTTER AND STEEP SLOPED CONCRETE EDGE.
4. THE CONTRACTOR SHALL CONSTRUCT ADDITIONAL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS FOR AREAS OF THE NEW VALLEY GUTTER WHERE THE LENGTH TO WIDTH RATIO EXCEEDS 1.25. THE CONTRACTOR SHALL PROVIDE A JOINT LAYOUT PLAN TO THE RPR FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION.
5. EXISTING VALLEY GUTTER REINFORCEMENT SHOWN FOR INFORMATIONAL PURPOSES ONLY.

CONCRETE POND VALLEY
GUTTER EXTENSION DETAIL

NTS

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CONCRETE ENERGY
DISSIPATION SYSTEM DETAIL

NTS

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE DETAILS

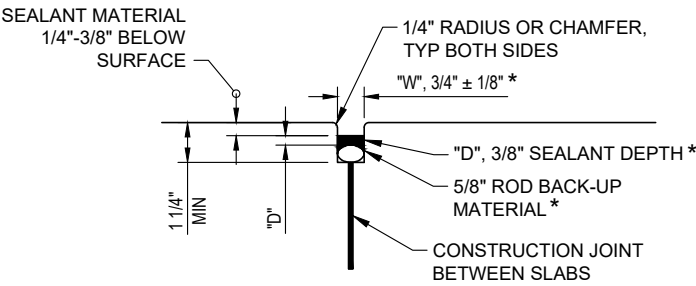
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*SEE NOTES

CONSTRUCTION JOINT

NOTES:

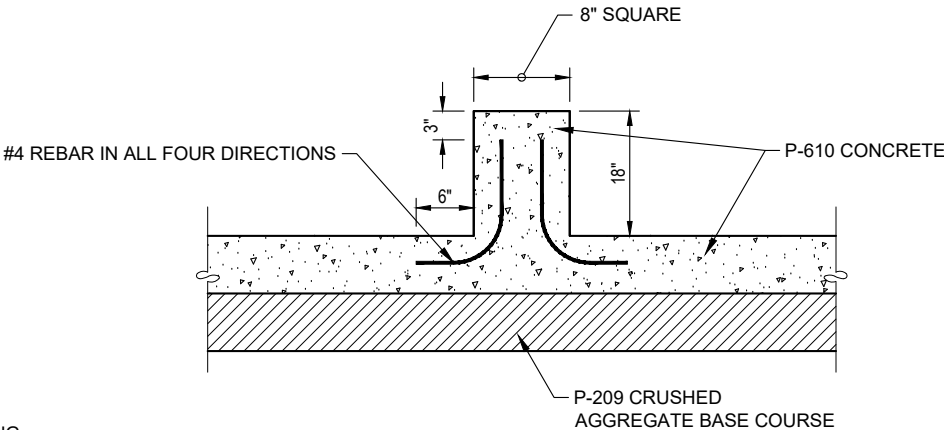
1. THE CONTRACTOR SHALL INSTALL 12" LONG, #4 REBAR DOWELS, 36" O.C. WITH 6" CLEAR SPACE. REBAR DOWELS SHALL BE CENTERED ON THE EXISTING VALLEY GUTTER.

PCC JOINT SEALANT
INSTALLATION DETAIL

NTS

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42



NOTES:

1. CONCRETE PYLONS SHALL BE POURED IN PLACE IN AREAS OF NEW CONCRETE. IN AREAS OF EXISTING CONCRETE THE EXISTING PCC SHALL BE CORED OR OTHERWISE REMOVED TO CONSTRUCT PYLONS.

CONCRETE PYLON DETAIL

NTS

4

42

1 REPLACE ENTIRE SHEET

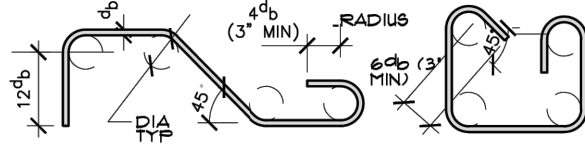
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| f'c | 3000 psi | | 3500 psi | | 4000 psi | |
|--------------|---------------------------|-------------------------|---------------------------|-------------------------|---------------------------|-------------------------|
| Bar Size [1] | Top bar "Ls" [2] (inches) | Other "Ls" [3] (inches) | Top bar "Ls" [2] (inches) | Other "Ls" [3] (inches) | Top bar "Ls" [2] (inches) | Other "Ls" [3] (inches) |
| #3 | 28 | 22 | 26 | 20 | 25 | 20 |
| #4 | 38 | 30 | 35 | 27 | 33 | 26 |
| #5 | 47 | 37 | 43 | 34 | 41 | 32 |
| #6 | 56 | 43 | 52 | 40 | 49 | 38 |
| #7 | 61 | 48 | 57 | 45 | 54 | 43 |
| #8 | 63 | 50 | 59 | 47 | 56 | 45 |

- Top bars are horizontal bars w/ more than 12" of concrete below splice.
- Where different bar sizes are lap spliced, the required lap length is the Ls for the smaller bar and the required development length is the Ls for the bigger bar and clearance based on the larger bar diameter.
- Increase lap splice length by 20% for epoxy-coated bars.

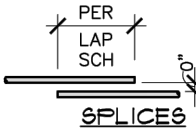
GRADE 60 BEND DIAM.:
6 db FOR #3-#8 BARS
8 db FOR #9-#11 BARS
WHERE db = BAR DIAM.

SPLICES FOR SUCCESSIVE TIES TO BE PLACED AT ALTERNATE CORNERS OF COLUMNS



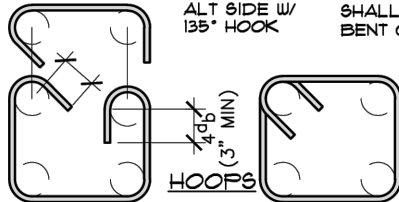
HOOKS & BENDS

ALL BARS SHALL BE TIED TOGETHER



BEAM STIRRUPS

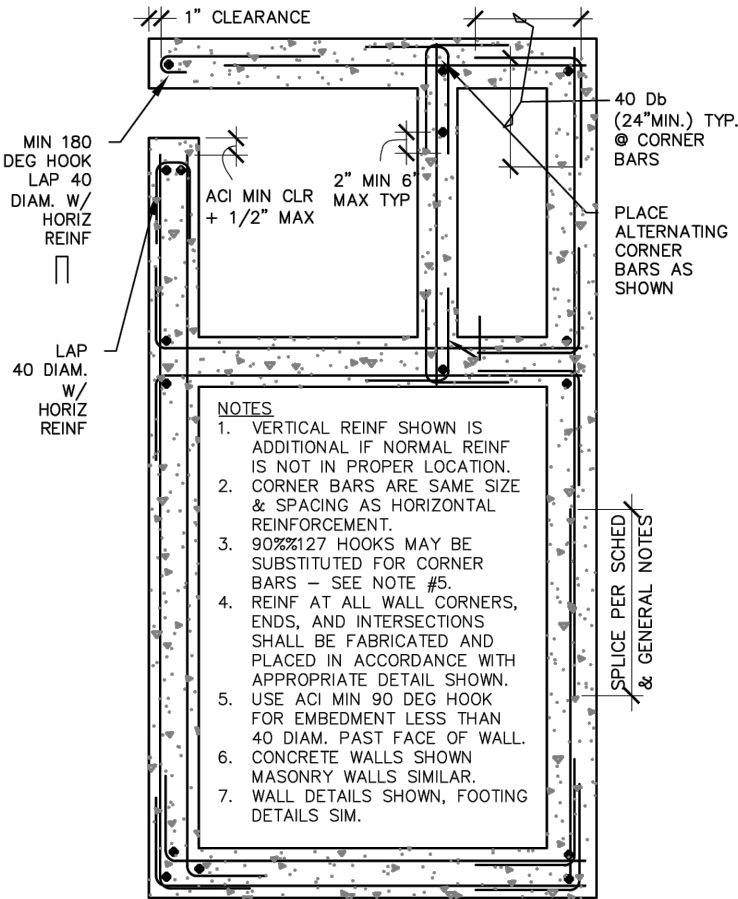
NOTE: ALL REINFORCEMENT SHALL BE BENT COLD



REBAR BEND DETAILS

NTS

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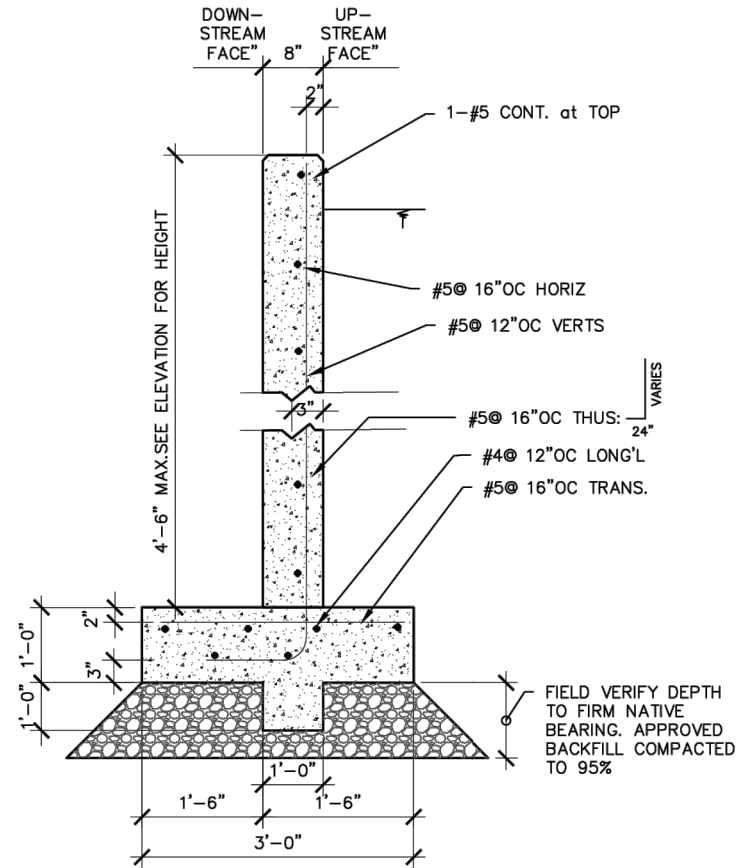
NOTES

- VERTICAL REINF SHOWN IS ADDITIONAL IF NORMAL REINF IS NOT IN PROPER LOCATION.
- CORNER BARS ARE SAME SIZE & SPACING AS HORIZONTAL REINFORCEMENT.
- 90%#127 HOOKS MAY BE SUBSTITUTED FOR CORNER BARS - SEE NOTE #5.
- REINF AT ALL WALL CORNERS, ENDS, AND INTERSECTIONS SHALL BE FABRICATED AND PLACED IN ACCORDANCE WITH APPROPRIATE DETAIL SHOWN.
- USE ACI MIN 90 DEG HOOK FOR EMBEDMENT LESS THAN 40 DIAM. PAST FACE OF WALL.
- CONCRETE WALLS SHOWN MASONRY WALLS SIMILAR.
- WALL DETAILS SHOWN, FOOTING DETAILS SIM.

REBAR CORNER BENDS

NTS

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43



NEW OR MODIFIED CONTROL STRUCTURE

NTS

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43

DOWEL AND TIEBAR GENERAL NOTES:

- ALL DOWELS AND TIEBARS SHALL MEET THE REQUIREMENTS OF SPECIFICATION P-610, CONCRETE FOR MISCELLANEOUS STRUCTURES.
- REBAR DOWELS SHALL BE DRILLED AND EPOXY GROUTED INTO EXISTING CONCRETE. REBAR DOWELS SHALL BE COATED WITH A BOND-BREAKER AS RECOMMENDED BY THE MANUFACTURER.
- WHERE THE LOCATION OF THE REBAR DOWEL CONFLICTS WITH THE LOCATION OF OTHER REBAR, ADJUST REBAR SPACING TO ACCOMMODATE DOWEL. VERIFY ADJUSTED LAYOUT WITH RPR.
- ON ALL CONTROL STRUCTURES TO BE EXPANDED, HEIGHTENED OR OTHERWISE MODIFIED, INSTALL 1-INCH DIAMETER, 18 INCH LONG, 12" O.C. ON ALL HORIZONTAL AND VERTICAL JOINTS BETWEEN NEW AND EXISTING CONCRETE. ALL WORK AND MATERIALS TO INSTALL DOWELS SHALL BE PAID UNDER THE RESPECTIVE CONTROL STRUCTURE MODIFICATION BID ITEMS.

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

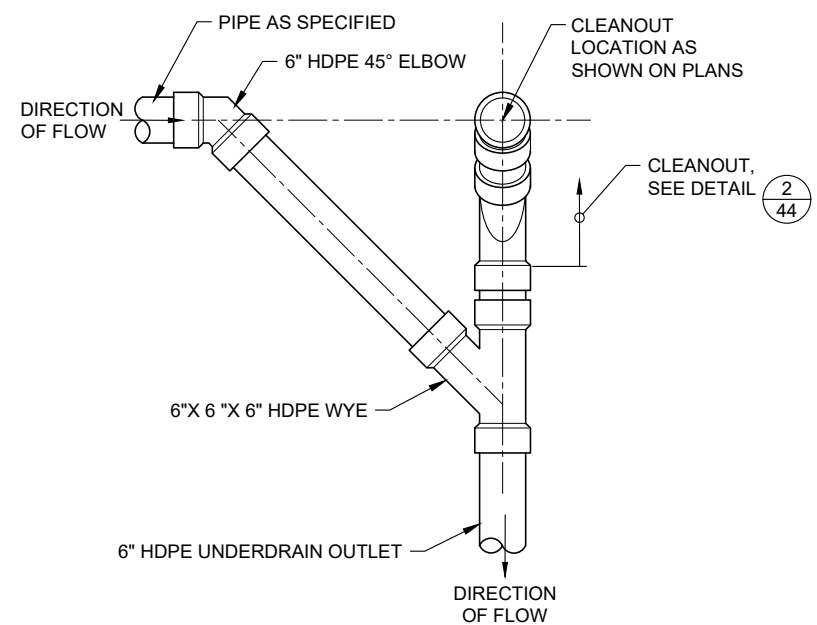
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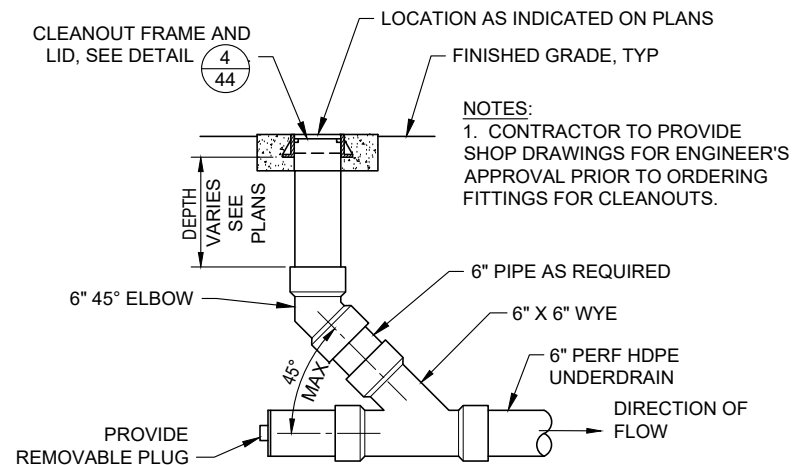
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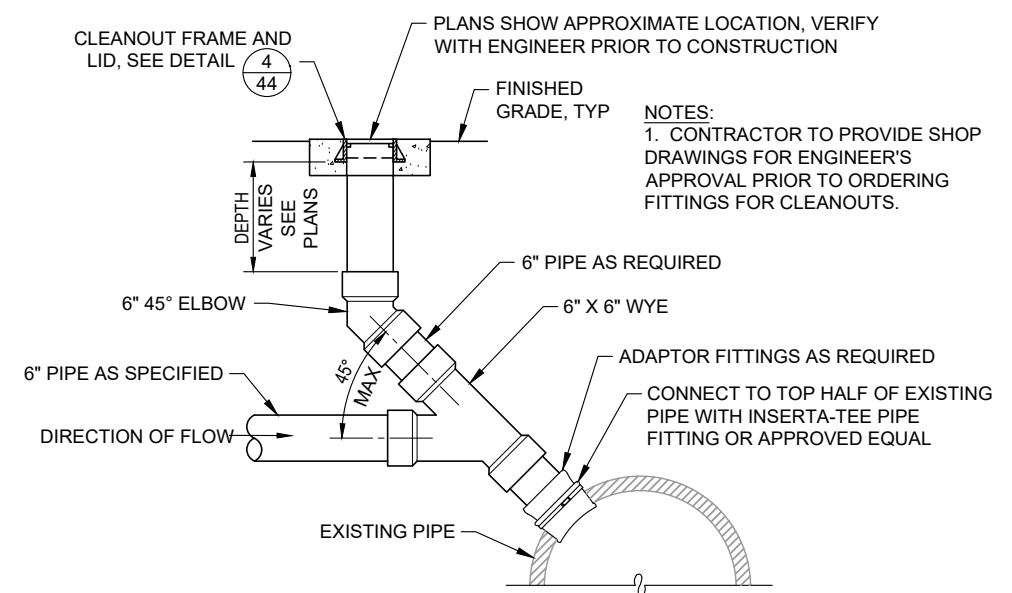
UNDERDRAIN CONNECTION WITH CLEANOUT DETAIL
NTS

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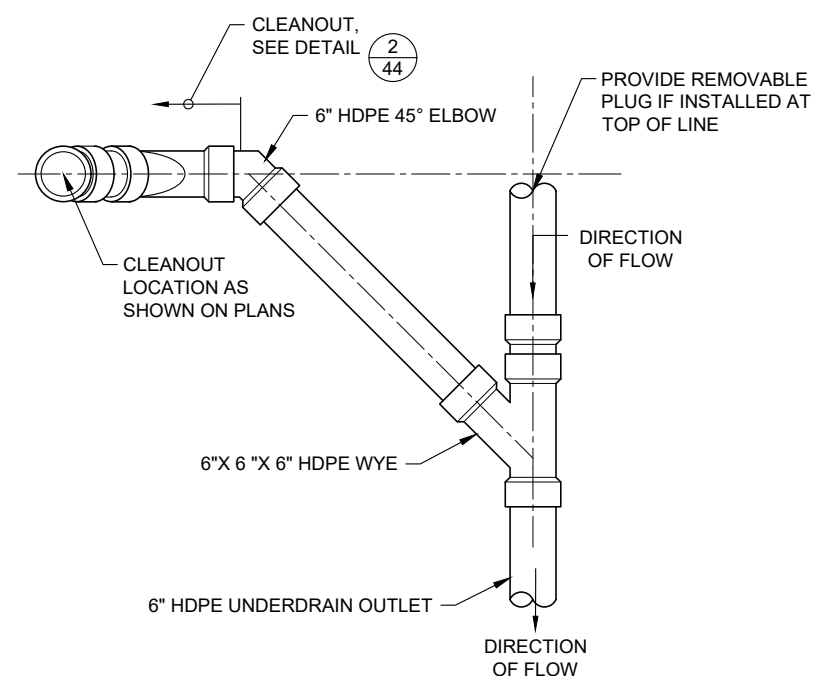
CLEANOUT DETAIL
NTS

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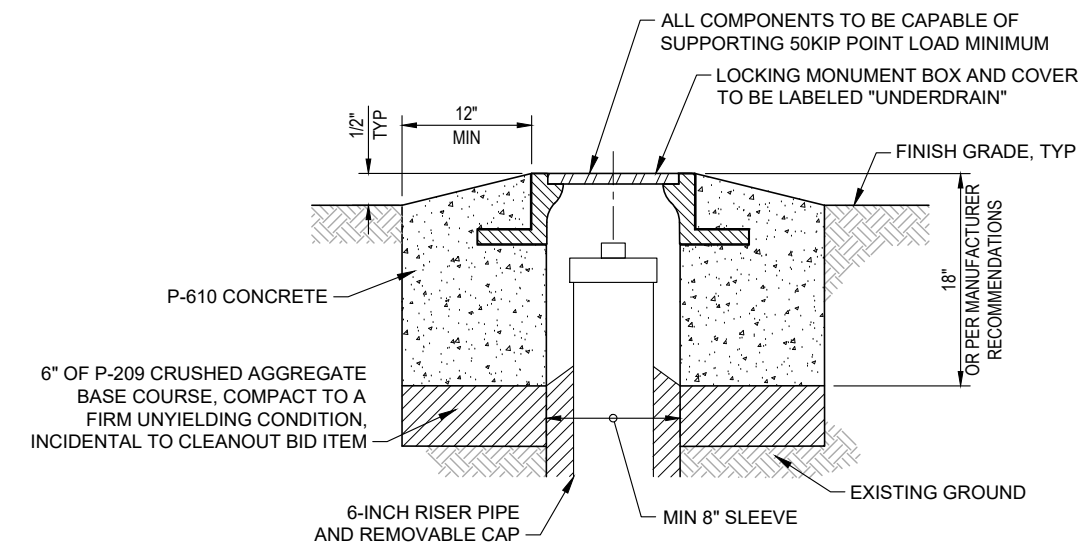
PIPE CONNECTION TO PIPE DETAIL
NTS

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VALLEY GUTTER UNDERDRAIN CLEANOUT DETAIL
NTS

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44



CLEAN-OUT FRAME AND LID DETAIL
NTS

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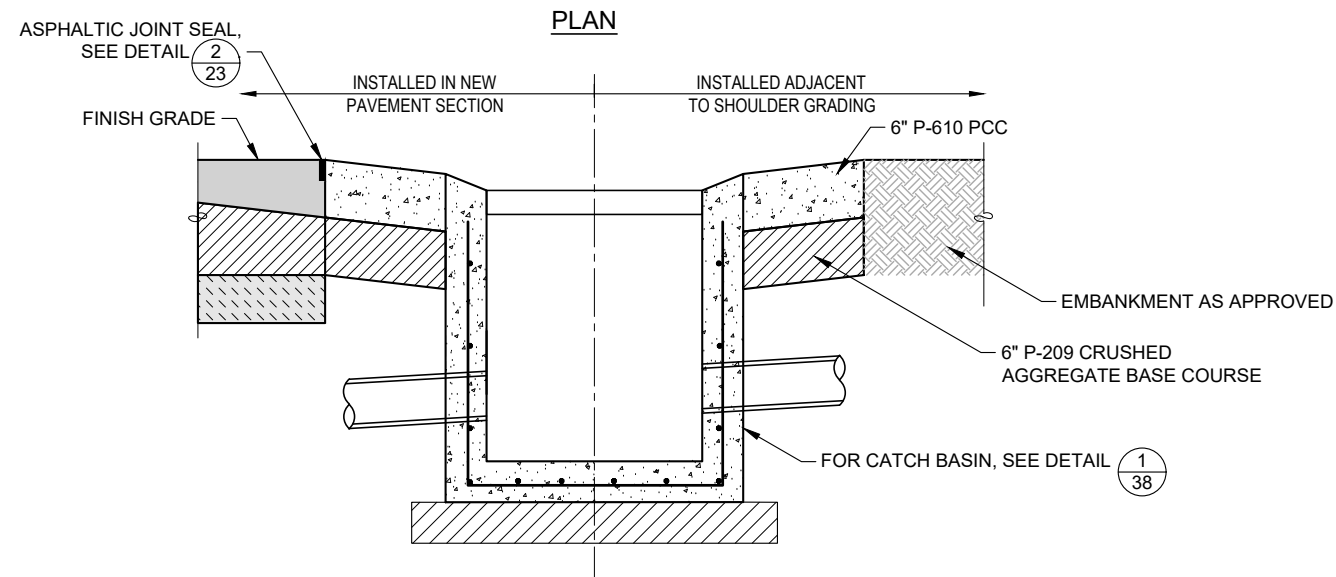
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TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE DETAILS

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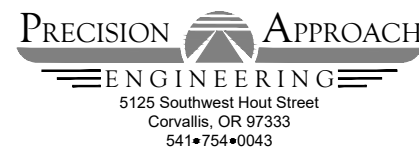


NOTES:

1. MODIFICATIONS TO CONCRETE DRAINAGE COLLAR DIMENSIONS SHOWN MAY BE NECESSARY TO FIT SPECIFIC FIELD CONDITIONS, CONTRACTOR TO PROVIDE SHOP DRAWING FOR ENGINEER APPROVAL PRIOR TO CONSTRUCTION OF CONCRETE DRAINAGE COLLAR.
2. ALL WORK AND MATERIALS INCLUDING FORMING, REBAR, JOINT SEALANT AND BASE ROCK REQUIRED FOR CONSTRUCTION OF THE CONCRETE DRAINAGE COLLAR SHALL BE PAID UNDER THE CONCRETE DRAINAGE COLLAR BID ITEM.
3. INSTALLATION OF THE CATCH BASIN, EACH CONNECTING PIPES, AND ABUTTING ASPHALT PAVEMENT WILL BE PAID UNDER EACH RESPECTIVE BID ITEM.

NTS

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1 ADDENDUM NO. 1

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| 12/23/2018 | 12/23/2018 |
| 12/24/2018 | 12/24/2018 |
| 12/25/2018 | 12/25/2018 |
| 12/26/2018 | 12/26/2018 |
| 12/27/2018 | 12/27/2018 |
| 12/28/2018 | 12/28/2018 |
| 12/29/2018 | 12/29/2018 |
| 12/30/2018 | 12/30/2018 |
| 12/31/2018 | 12/31/2018 |

SCALE: AS SHOWN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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OF 71

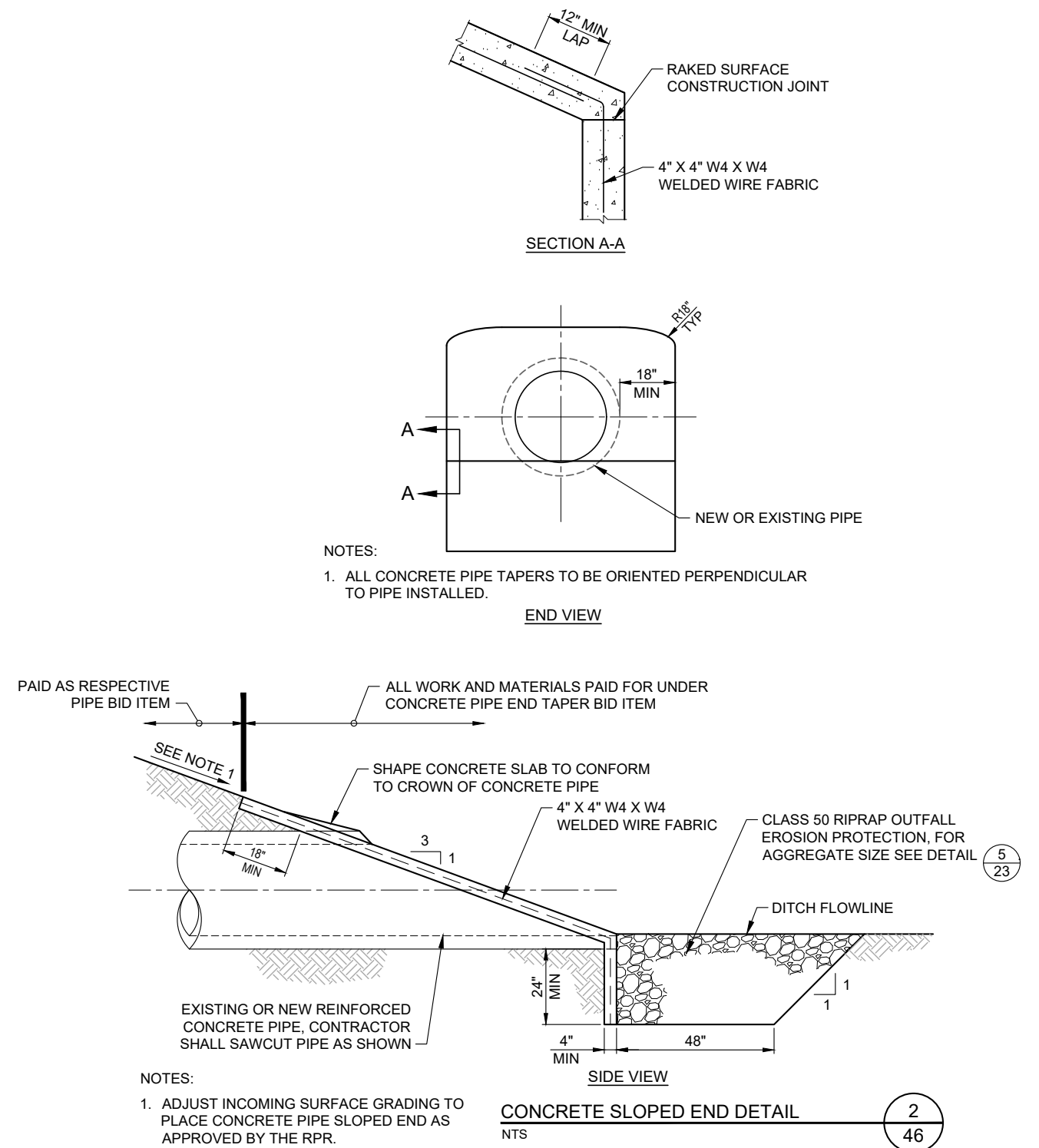
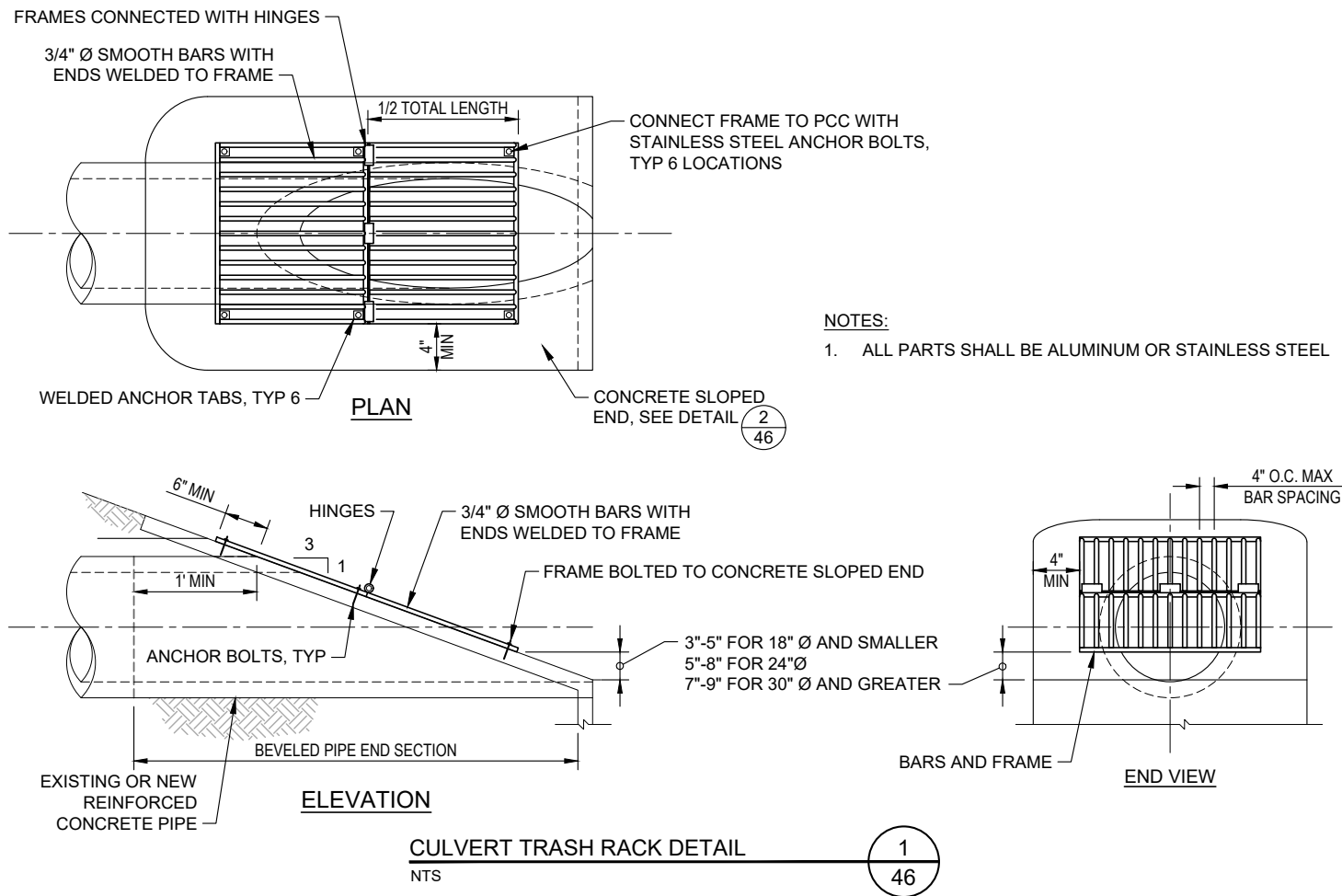


1. THE CONTRACTOR SHALL INSTALL TRANSVERSE EXPANSION JOINTS AT A MAXIMUM SPACING OF 50' UNLESS OTHERWISE APPROVED BY THE ENGINEER. CONTRACTOR SHALL INSTALL TRANSVERSE CONTRACTION JOINTS AT 5' INTERVALS UNLESS OTHERWISE APPROVED BY THE RPR. ALL TRANSVERSE AND EXPANSION JOINTS REQUIRED TO INSTALL THE VALLEY GUTTERS ARE CONSIDERED INCIDENTAL TO THE CONCRETE VALLEY GUTTER BID ITEM.
2. PROVIDE JOINT LAYOUT, TYPE, AND SPACING, TO THE RPR PRIOR TO CONSTRUCTION OF THE CONCRETE VALLEY GUTTERS.
3. P-407 ASPHALT TREATED PERMEABLE BASE COURSE, DRAINAGE GEOTEXTILE FABRIC, AND SUBGRADE GEOTEXTILE FABRIC SHALL BE PAID UNDER THEIR RESPECTIVE BID ITEMS.

NTS

$$\frac{2}{45}$$

05/24/24 - 2:14pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_SDDE.dwg



1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



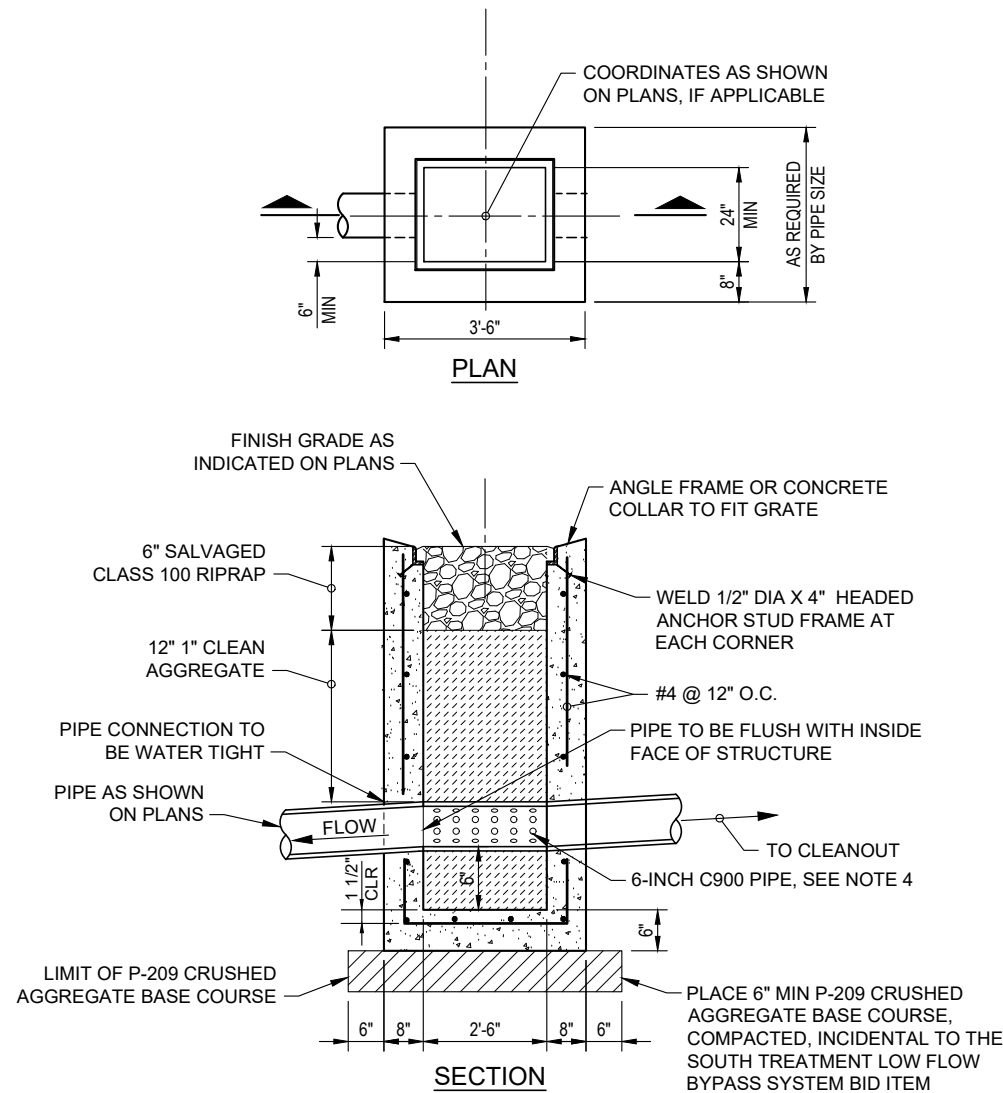
1 ADDENDUM NO. 1 5/24/2024 JGL
REVISIONS: DATE APPD.

DATE: MAY 2024
DESIGN: KJK
DRAWN: JAW
CHECKED: JGL
REVISION NUMBER: 0
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
DRAINAGE DETAILS
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04
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OF 71

05/24/24 - 2:14pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_SDDE.dwg

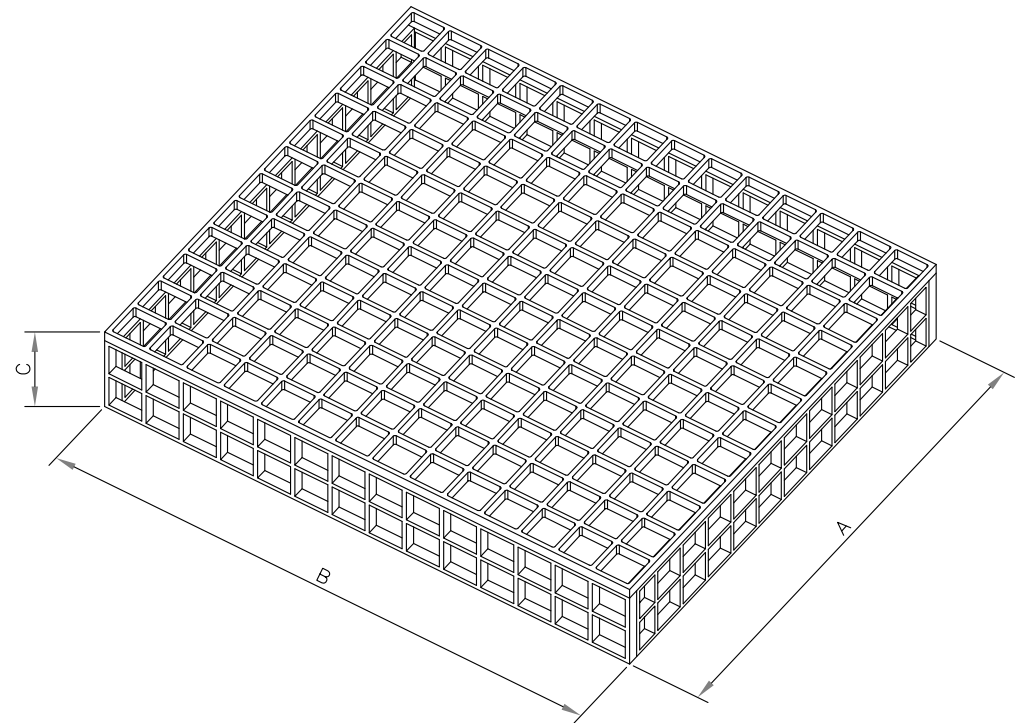


NOTES:

1. DIMENSIONS OF CATCH BASIN MAY CHANGE, WITH ENGINEER'S APPROVAL, IF REQUIRED FOR COMPATIBILITY WITH FRAME AND GRATE OR DUE TO PIPE SIZE.
2. CONTRACTOR TO PROVIDE SHOP DRAWINGS FOR ENGINEERS APPROVAL PRIOR TO ORDERING STRUCTURES.
3. ALL COMPONENTS SHALL BE CAPABLE OF SUPPORTING H20 POINT LOAD MINIMUM.
4. C900 PIPE WITHIN THE STRUCTURE SHALL HAVE 1/2" DRILLED HOLES ON A 2-INCH GRID.
5. THE UPSTREAM CLEANOUT SHALL BE LOCATED 7 FEET AWAY FROM THE EDGE OF THE STRUCTURE AT A MINIMUM.

SOUTH TREATMENT
LOW FLOW BYPASS SYSTEM DETAIL
NTS

1
47



| DIMENSION | MINIMUM DIMENSIONS (INCHES) | |
|-----------|-----------------------------|-----------------------|
| | POND OUTLET STRUCTURE | POND BYPASS STRUCTURE |
| "A" | 16 | 18 |
| "B" | 27 | 72 |
| "C" | 24 | 24 |

NOTES:

1. TRASHRACKS SHALL BE CONSTRUCTED OF CONSTRUCTED USING HIGH DENSITY POLYETHYLENE STRUCTURAL PLASTIC OR GALVANIZED STEEL.
2. FOR MINIMUM DIMENSIONS OF TRASHRACK, SEE TABLE.
3. TRASH RACK SHALL BE ATTACHED TO THE CONCRETE CONTROL STRUCTURES AS RECOMMENDED BY THE MFG.
4. THE CONTRACTOR SHALL PROVIDE TRASHRACKS WITH DIMENSIONS SIMILAR TO THOSE SHOWN ABOVE OR AS OTHERWISE APPROVED BY THE RPR.
5. TRASHRACK GAPS SHALL BE 2" MINIMUM SIZE OR AS OTHERWISE RECOMMENDED BY THE MFG AND APPROVED BY THE RPR.

CONTROL STRUCTURE
TRASHRACK DETAIL
NTS

2
47

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5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



1 ADDENDUM NO. 1 5/24/2024 JGL
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

DRAINAGE DETAILS

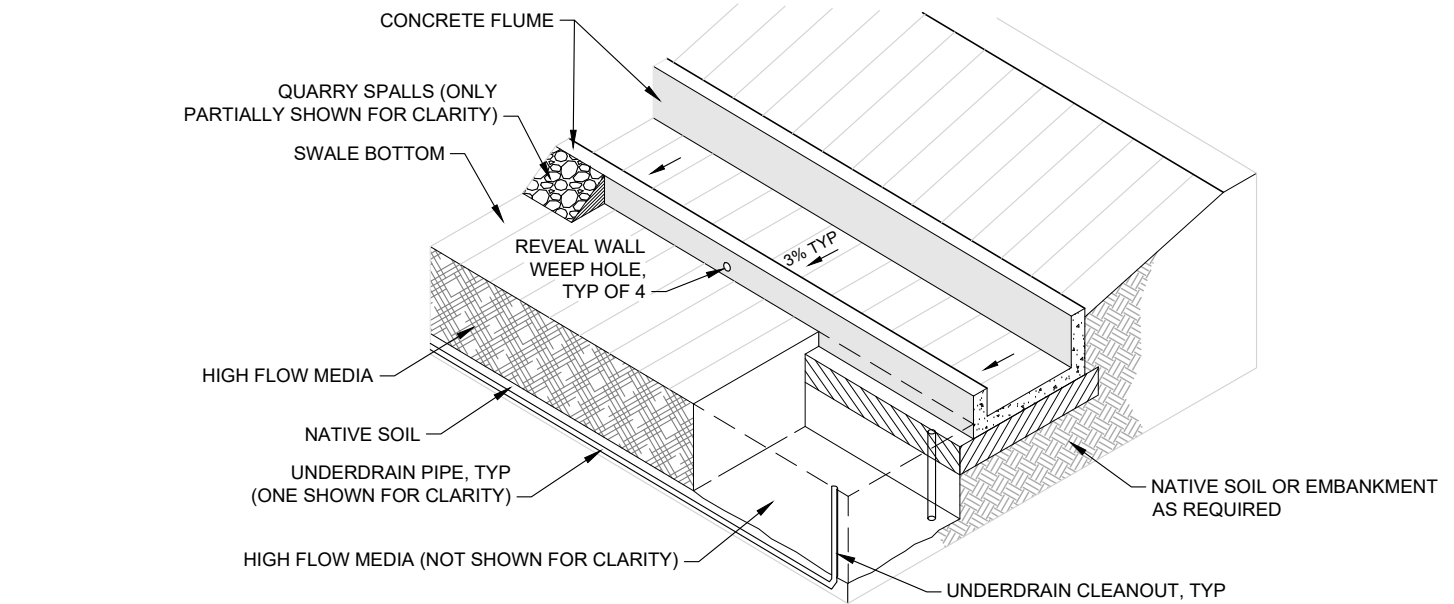
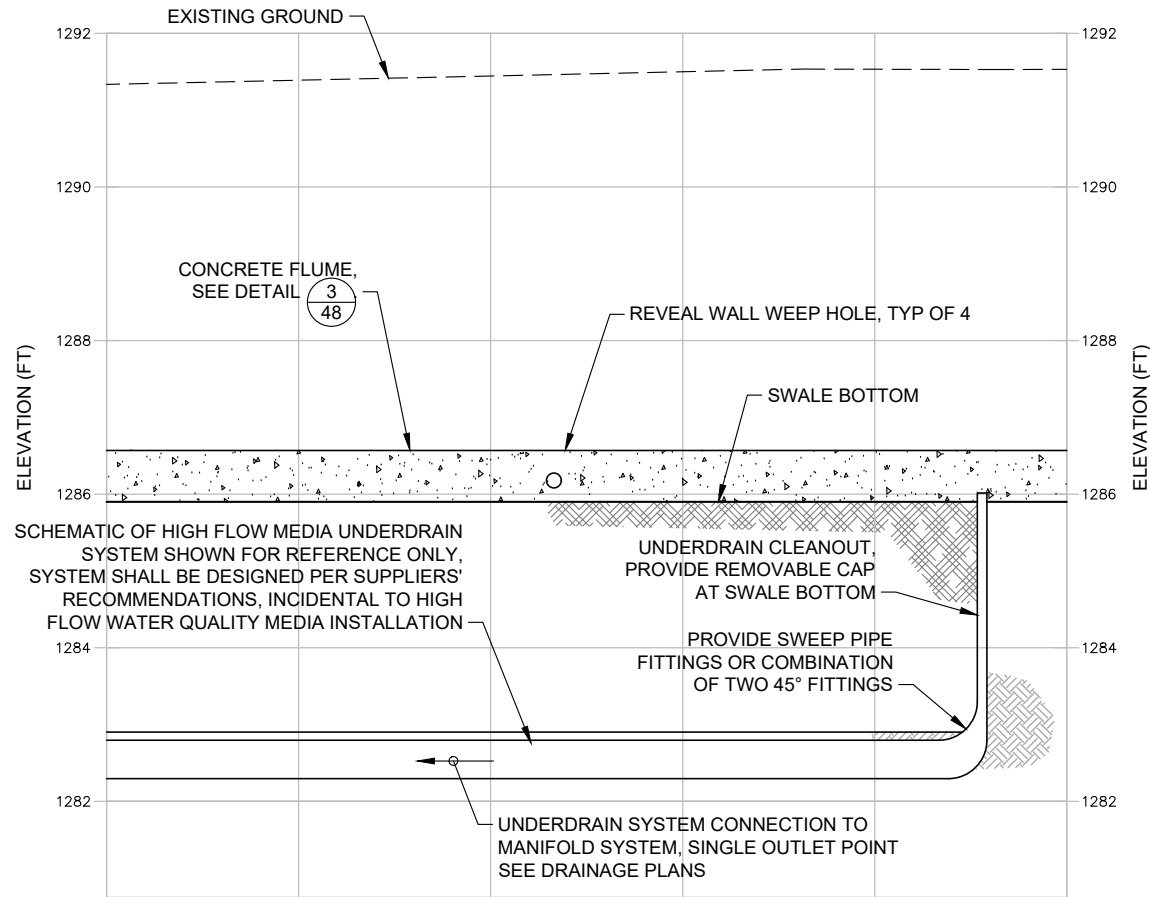
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PROJECT NUMBER:
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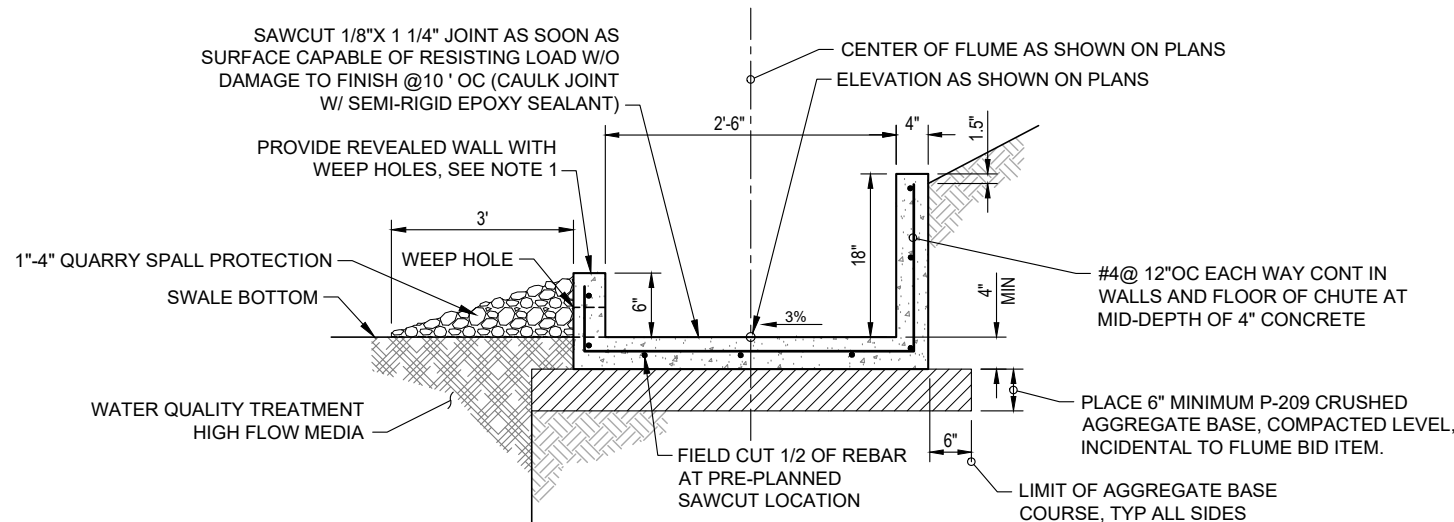
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SWALE AND SEPARATION BERM OBLIQUE VIEW
NTS



- NOTES:
1. LOW SIDE REVEAL SHALL HAVE FOUR 3-INCH DIAMETER WEEP HOLES TO PREVENT PERPETUAL STANDING WATER IN THE FLUME. WEEP HOLES SHALL BE PVC PIPE OR DRILLED OR CORED THROUGH THE FLUME. WEEP HOLES SHALL BE SPACED EQUIDISTANT IN THE FLUME. WEEP HOLES SHALL BE CONSTRUCTED FLUSH WITH THE BOTTOM OF THE FLUME AND THE TOP OF THE HIGH FLOW MEDIA SYSTEM.
 2. TRANSVERSE CONTRACTION JOINTS SHALL BE INSTALLED EVERY 10' WITH AN EXPANSION JOINT AT LEAST EVERY 50'.
 3. CONSTRUCT AN IMMEDIATE 2-INCH DROP BETWEEN THE FLUME AND THE CONTROL STRUCTURE. THE FIRST 10 FEET OF CONCRETE FLUME SHALL BE SLOPED AT 1% AWAY FROM THE CONTROL STRUCTURE TO INCREASE STORMWATER VELOCITY AWAY FROM STRUCTURE.
 4. THE CONCRETE FLUME SHALL BE CONSTRUCTED WITH A 3% TRANSVERSE SLOPE TO PREVENT PERPETUAL STANDING WATER IN THE FLUME.

CONCRETE FLUME DETAIL
NTS

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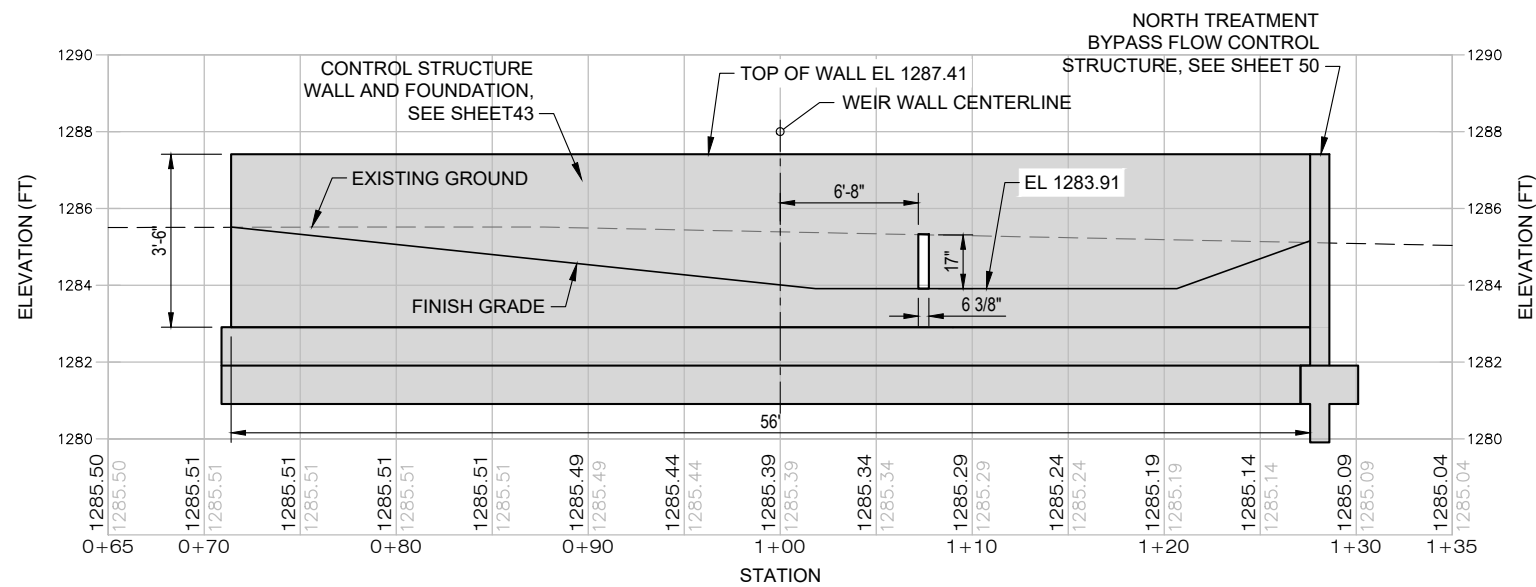
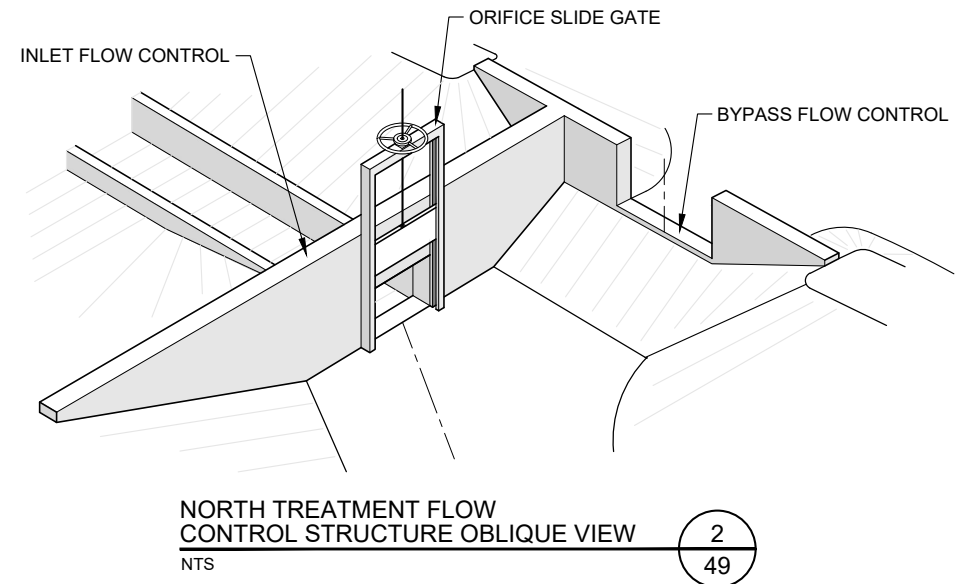
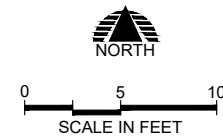
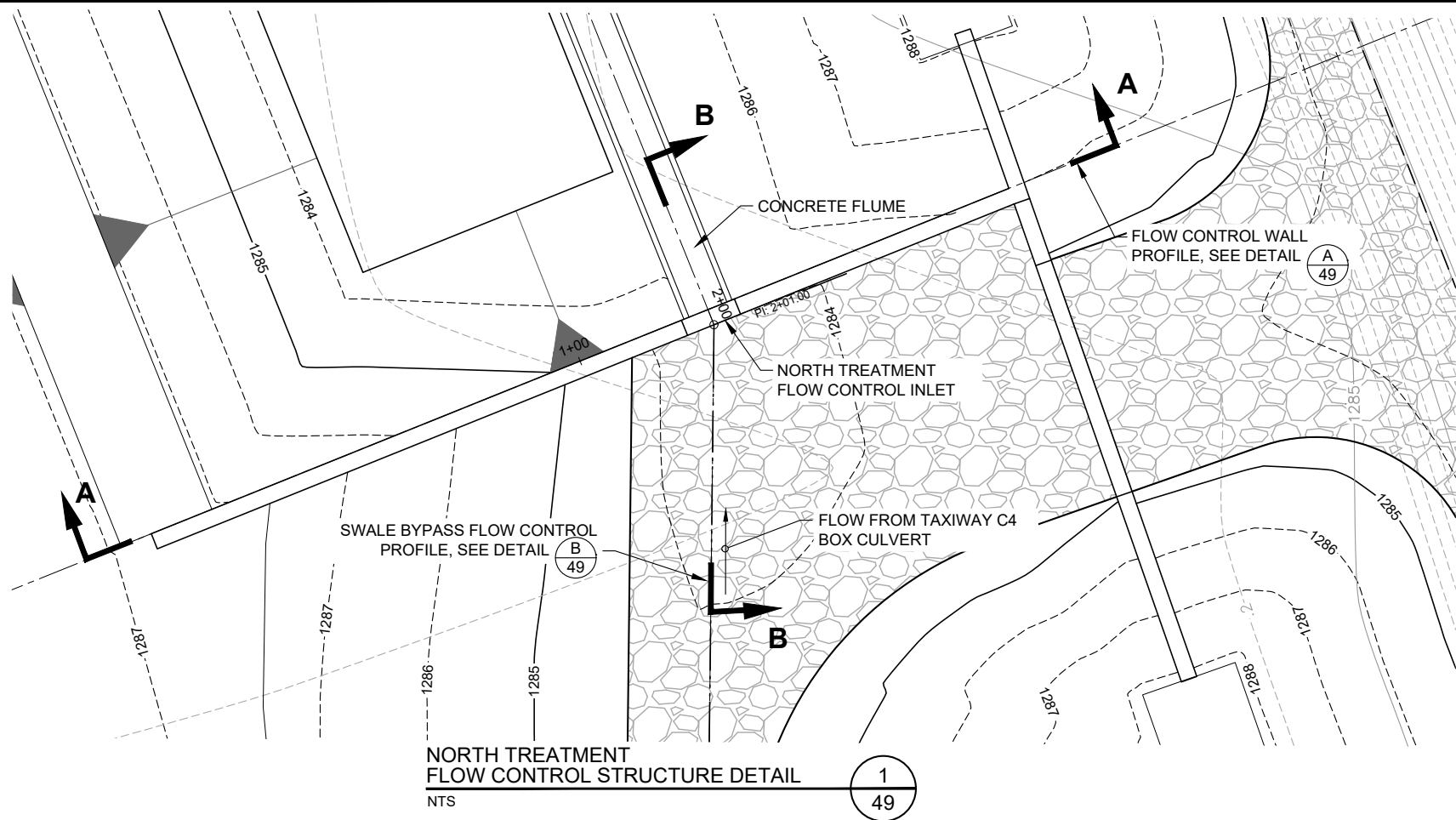
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
DRAINAGE DETAILS
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04
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OF 71

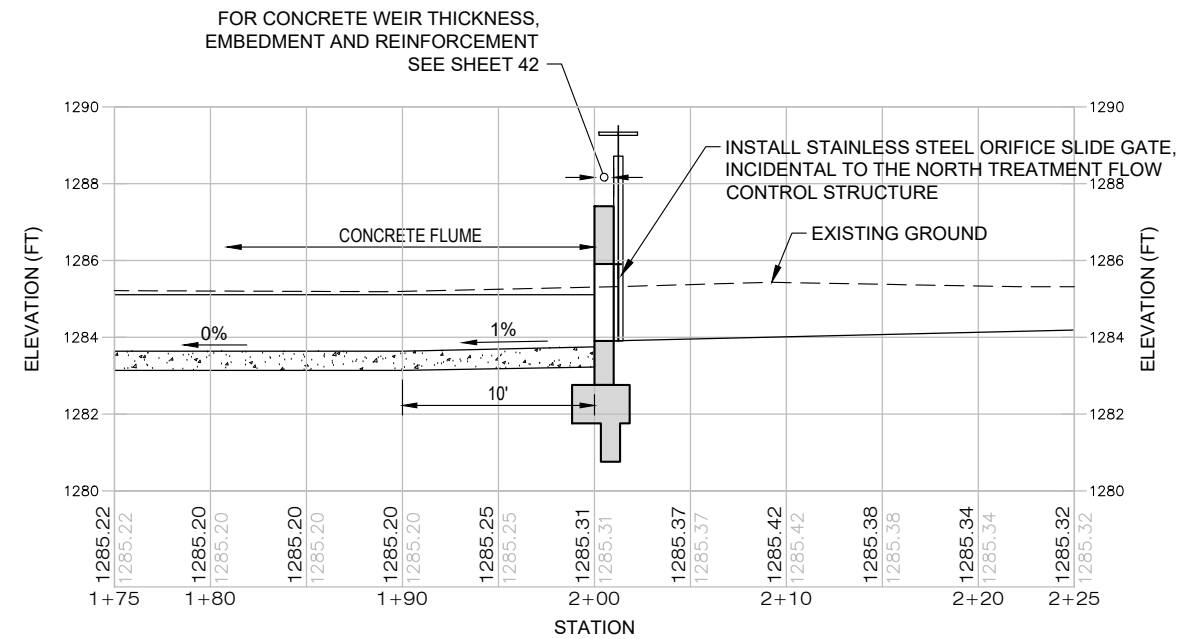
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NORTH TREATMENT FLOW CONTROL
STRUCTURE WALL PROFILE

NTS

A
49



NORTH TREATMENT FLOW CONTROL
STRUCTURE FLOWLINE PROFILE

NTS

B
49

1 REPLACE ENTIRE SHEET

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

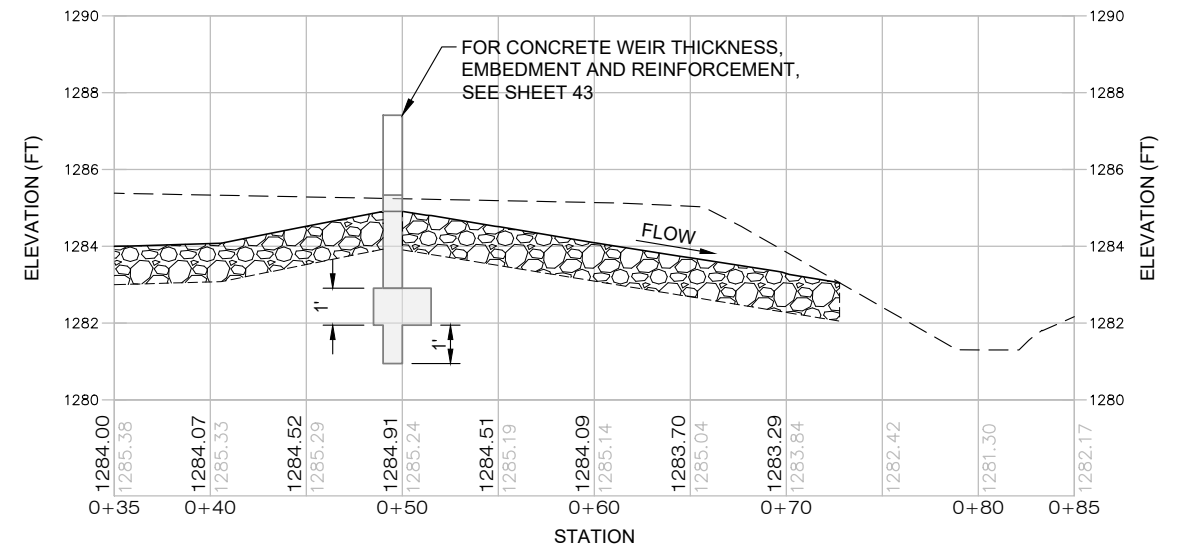
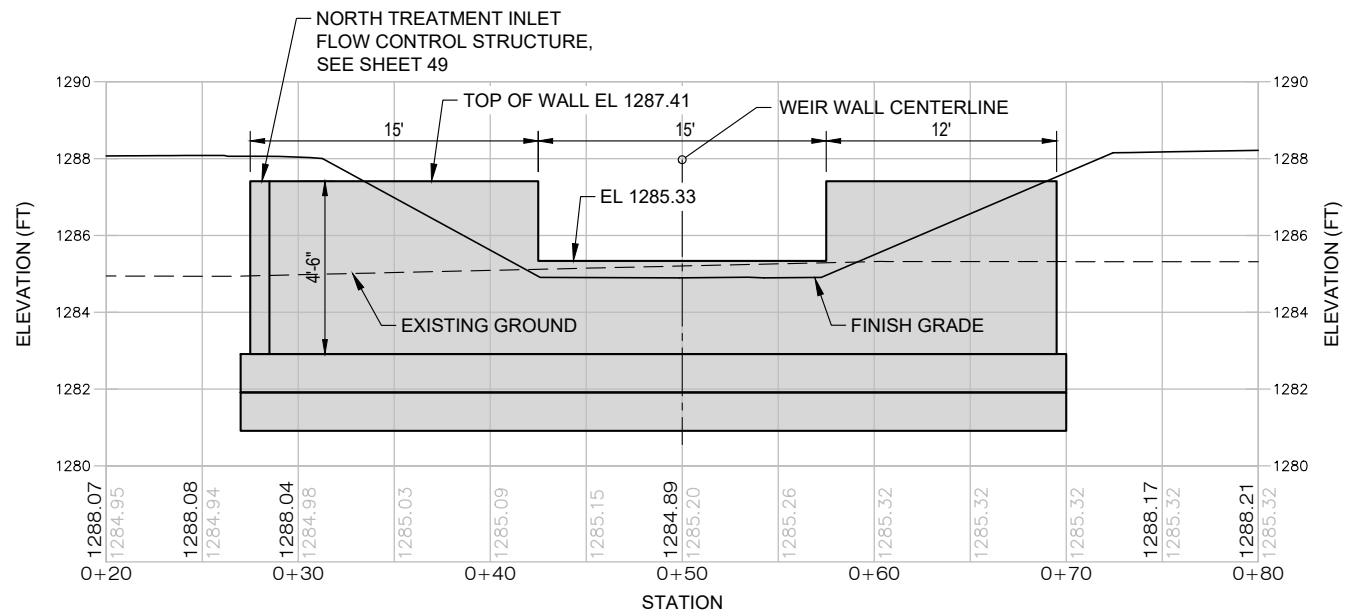
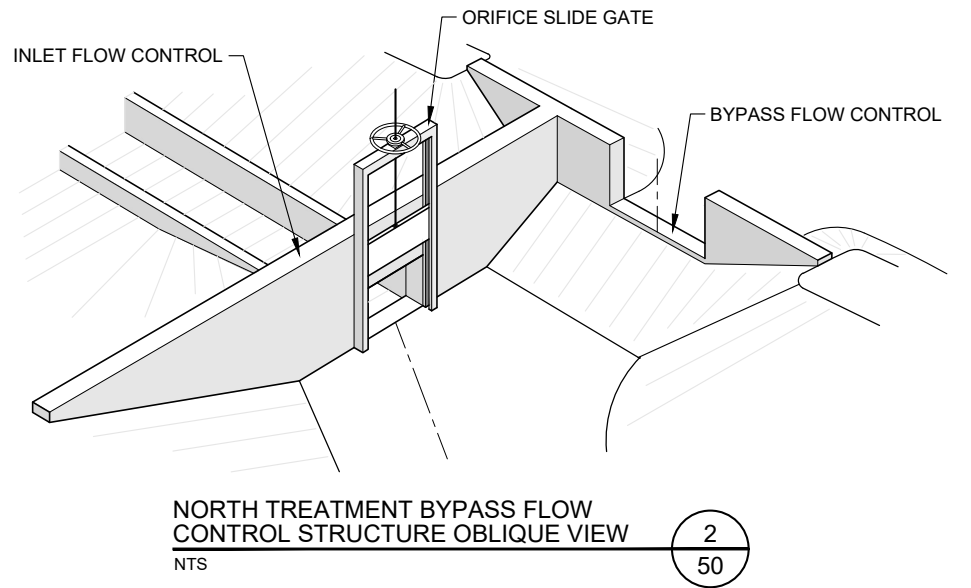
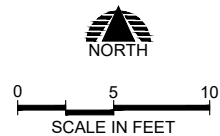
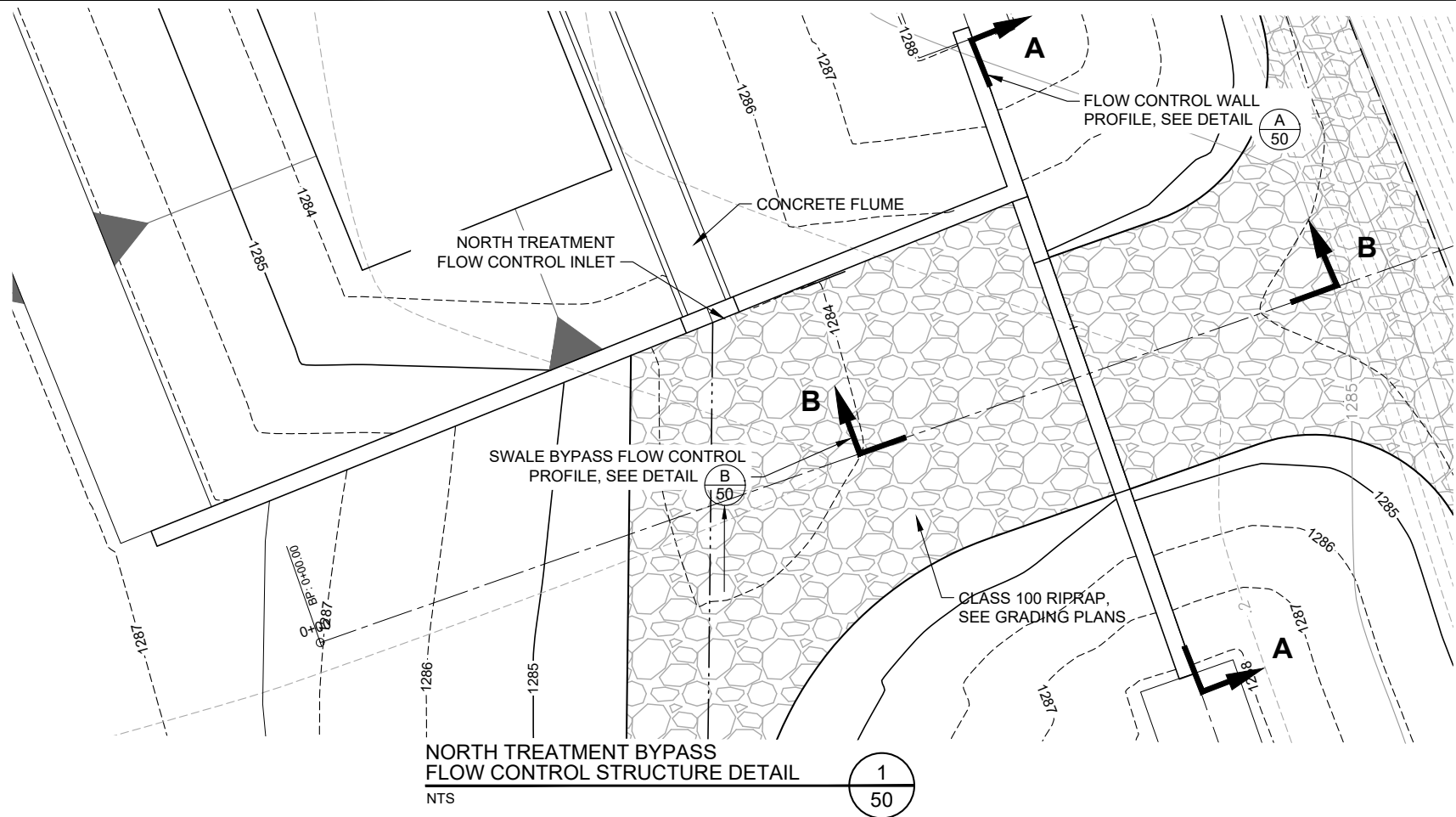
**NORTH TREATMENT INLET
STRUCTURE DETAILS**

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
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OF 71

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1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541-754-0043



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|------------------|---------------|
| DATE: | MAY 2024 |
| DESIGN: | KJK |
| DRAWN: | JAW |
| CHECKED: | JGL |
| REVISION NUMBER: | 0 |
| SCALE: | AS SHOWN |
| ADDENDUM NO. 1 | 5/24/2024 JGL |
| REVISIONS: | DATE APPD. |

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

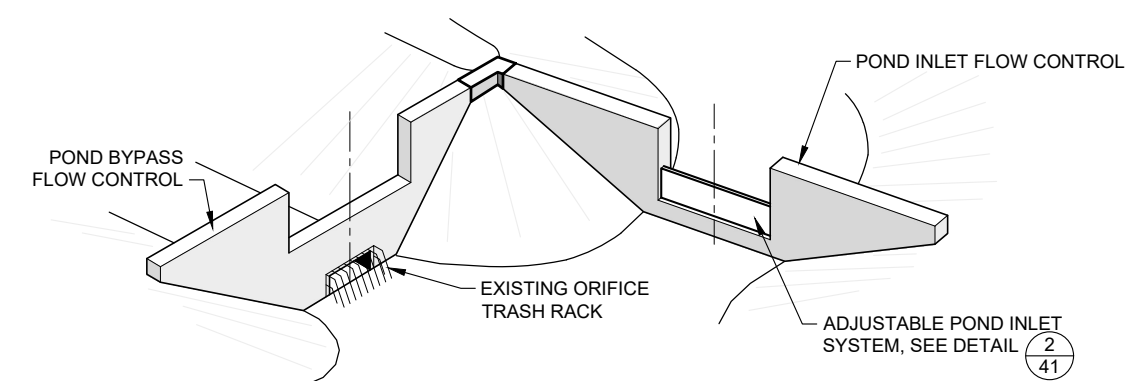
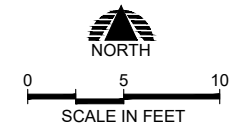
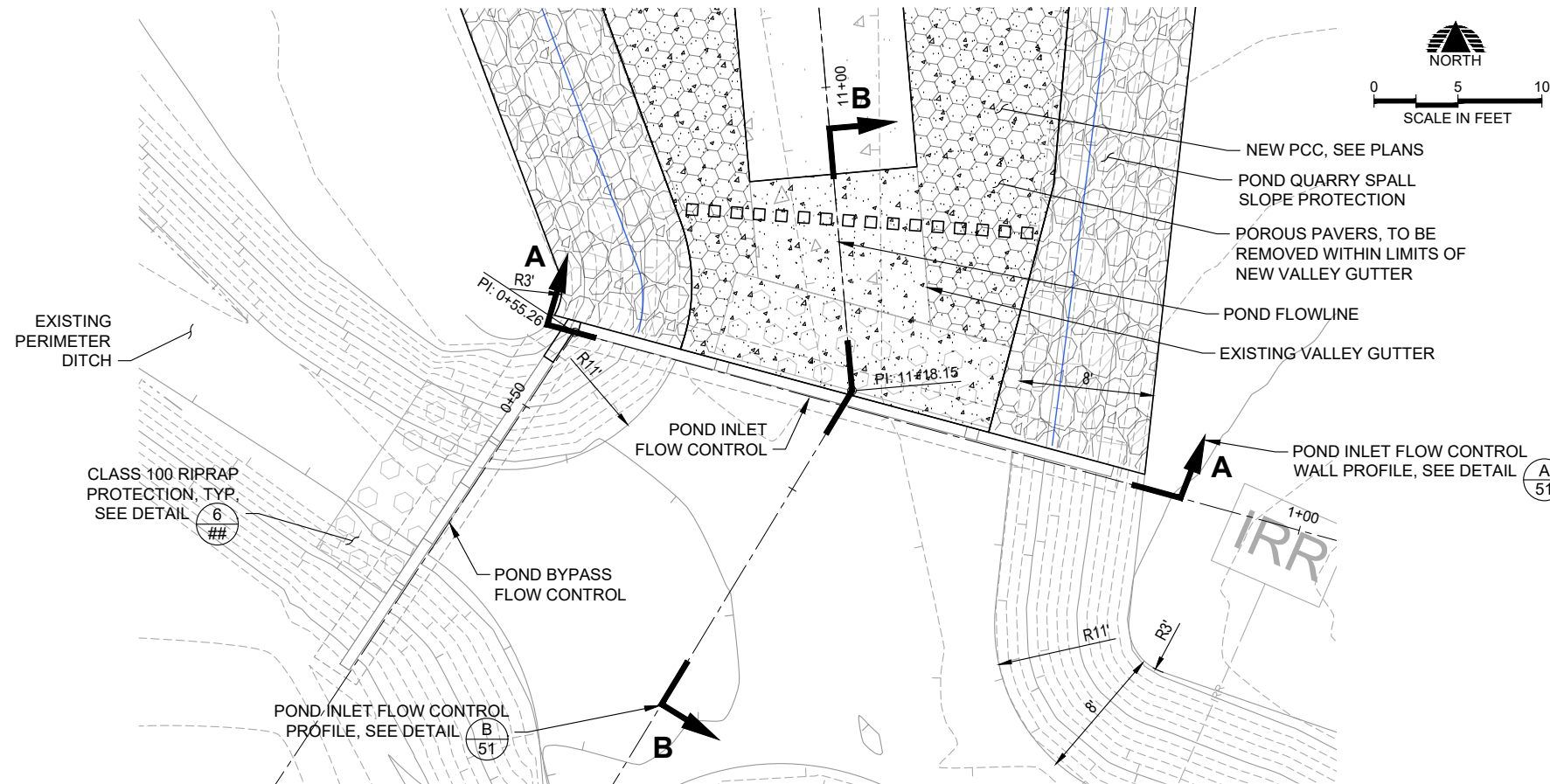
**NORTH TREATMENT BYPASS
STRUCTURE DETAILS**

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
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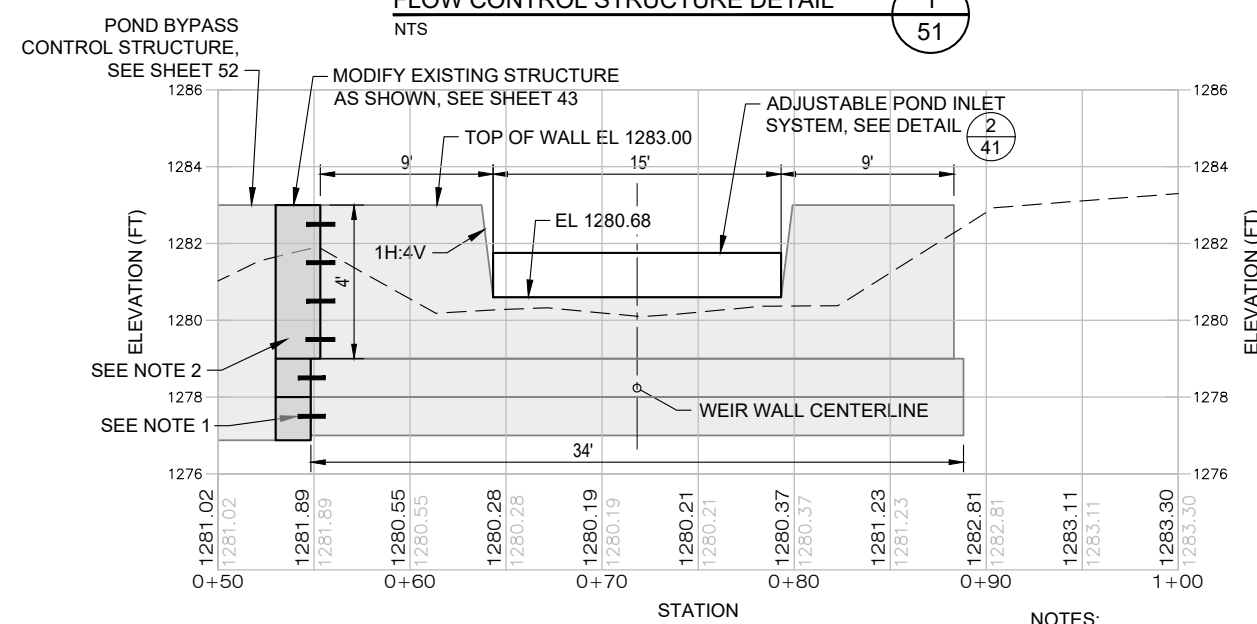
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OF 71

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POND INLET FLOW CONTROL STRUCTURE DETAIL
NTS

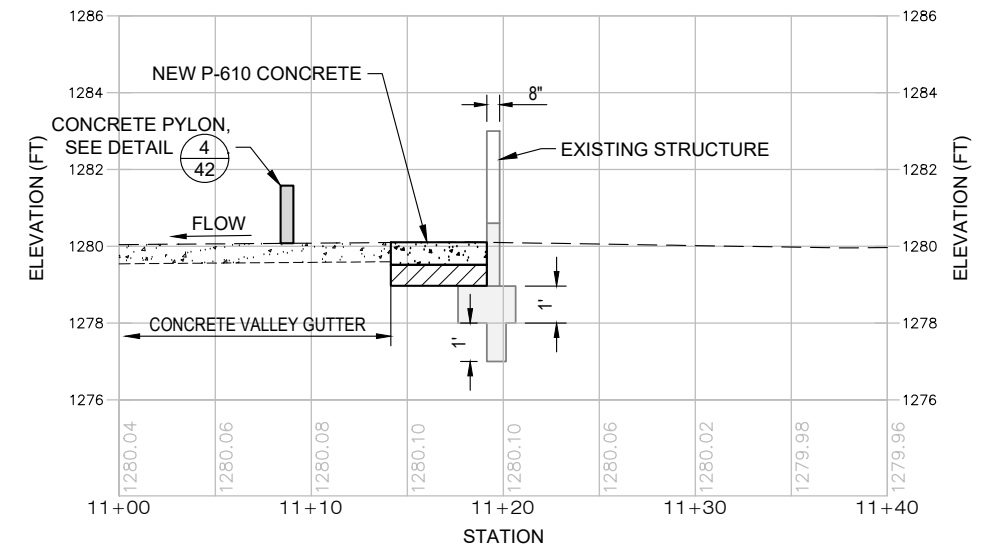
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POND INLET FLOW CONTROL STRUCTURE WALL PROFILE
NTS

A
51

- NOTES:
1. INSTALL STEEL DOWELS BETWEEN NEW AND EXISTING CONCRETE, TYP, CENTER DOWELS ON KEY, SLAB FOUNDATION AND WALL, SEE CONTROL STRUCTURE DOWEL NOTES ON SHEET 43.
 2. ALL WORK AND MATERIALS TO MODIFY THE EXISTING WALL AS SHOWN SHALL BE PAID UNDER THE POND INLET FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



POND INLET FLOW CONTROL STRUCTURE FLOWLINE PROFILE
NTS

B
51

1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541-754-0043

REGISTERED PROFESSIONAL
ENGINEER
77750PE
DIGITALLY SIGNED
JOSHUA G. LEIKER
JUNE 2, 2010
OREGON
EXPIRES 12/31/2024

| | | |
|----------------|-----------|-------|
| ADDENDUM NO. 1 | 5/24/2024 | JGL |
| REVISIONS: | DATE | APPD. |

DATE: MAY 2024
DESIGN: KJK
DRAWN: JAW
CHECKED: JGL
REVISION NUMBER: 0
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

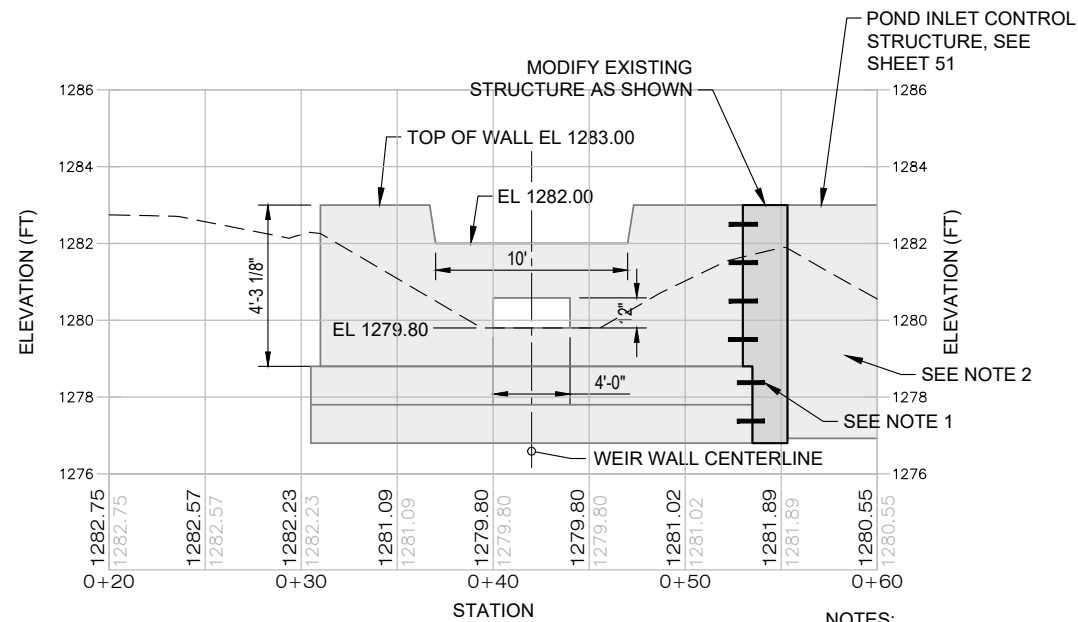
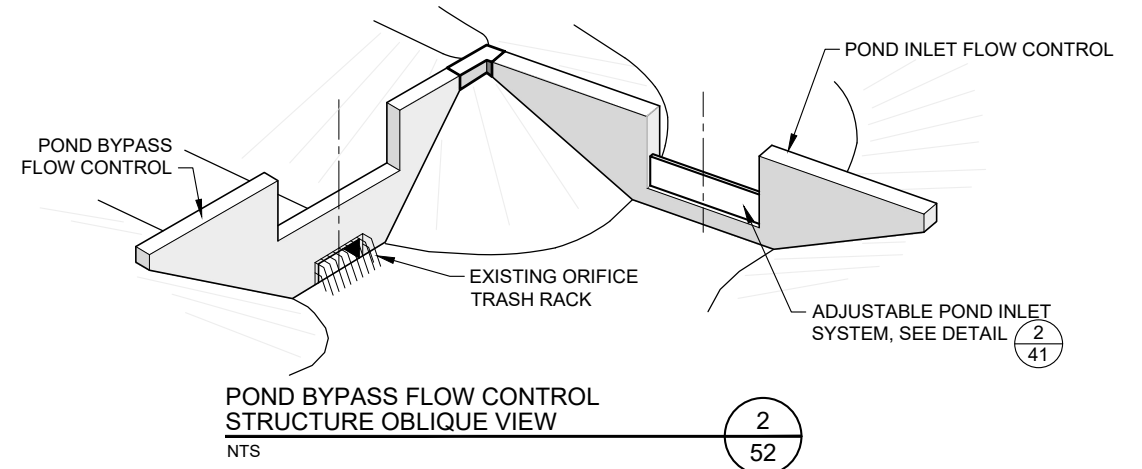
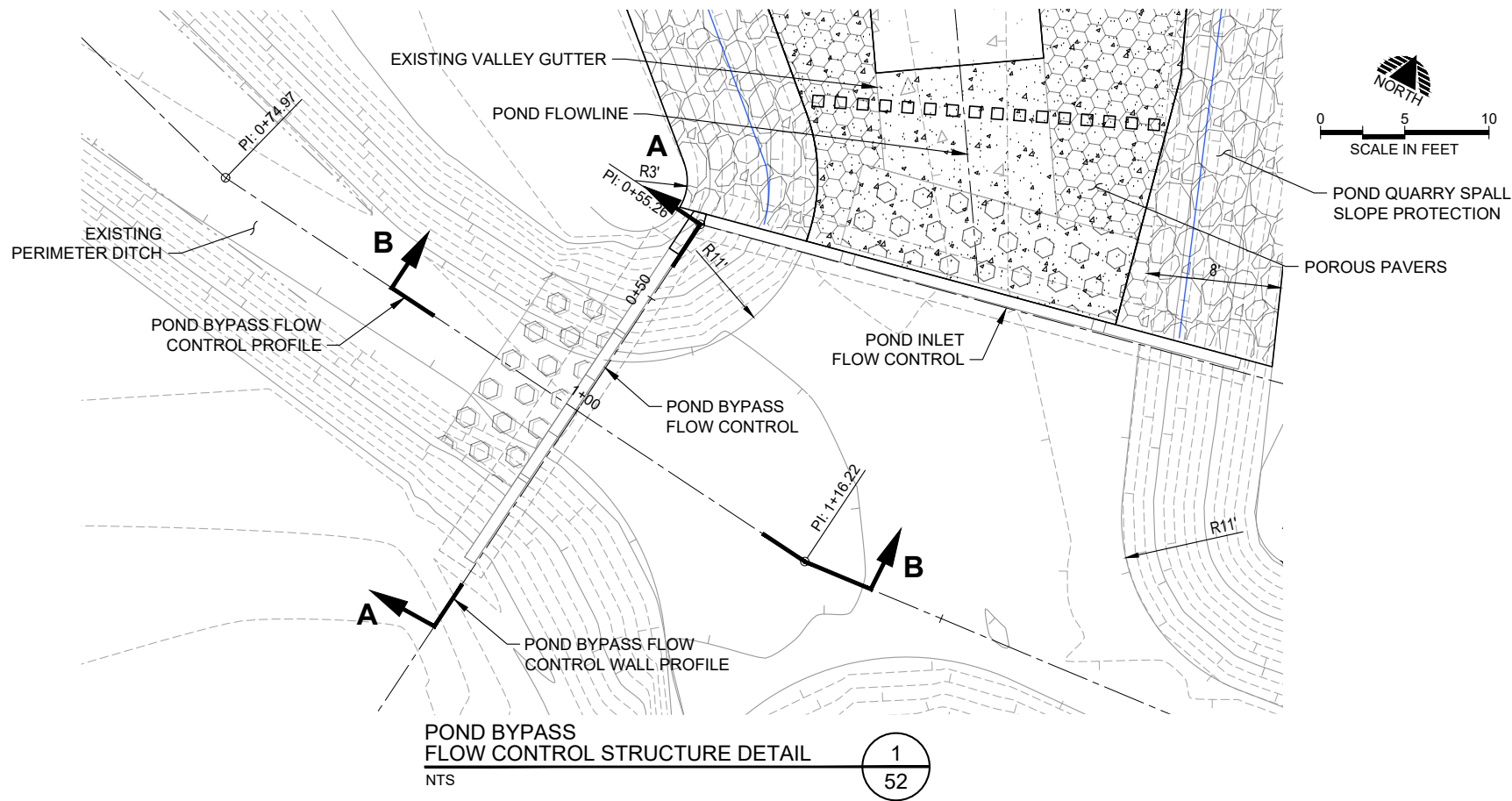
POND INLET STRUCTURE DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

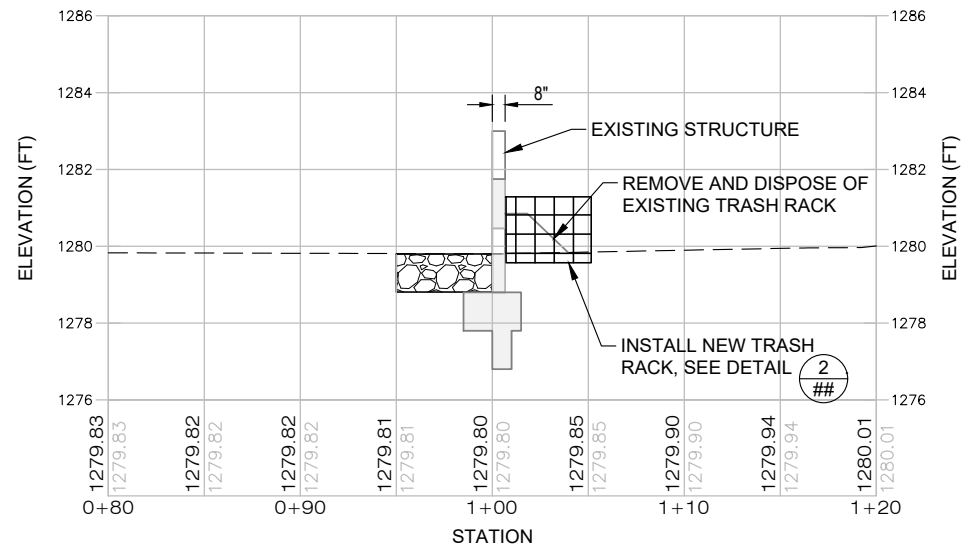
PROJECT NUMBER:
RVI-2020-W04

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OF 71

05/24/24 - 2:15pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_Drainage-Structures.dwg



- NOTES:
1. INSTALL STEEL DOWELS BETWEEN NEW AND EXISTING CONCRETE, TYP, CENTER DOWELS ON KEY, SLAB FOUNDATION AND WALL, SEE CONTROL STRUCTURE DOWEL NOTES ON SHEET 43.
 2. ALL WORK AND MATERIALS TO MODIFY THE EXISTING WALL AS SHOWN SHALL BE PAID UNDER THE POND BYPASS FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



POND BYPASS FLOW CONTROL STRUCTURE FLOWLINE PROFILE

REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541-754-0043



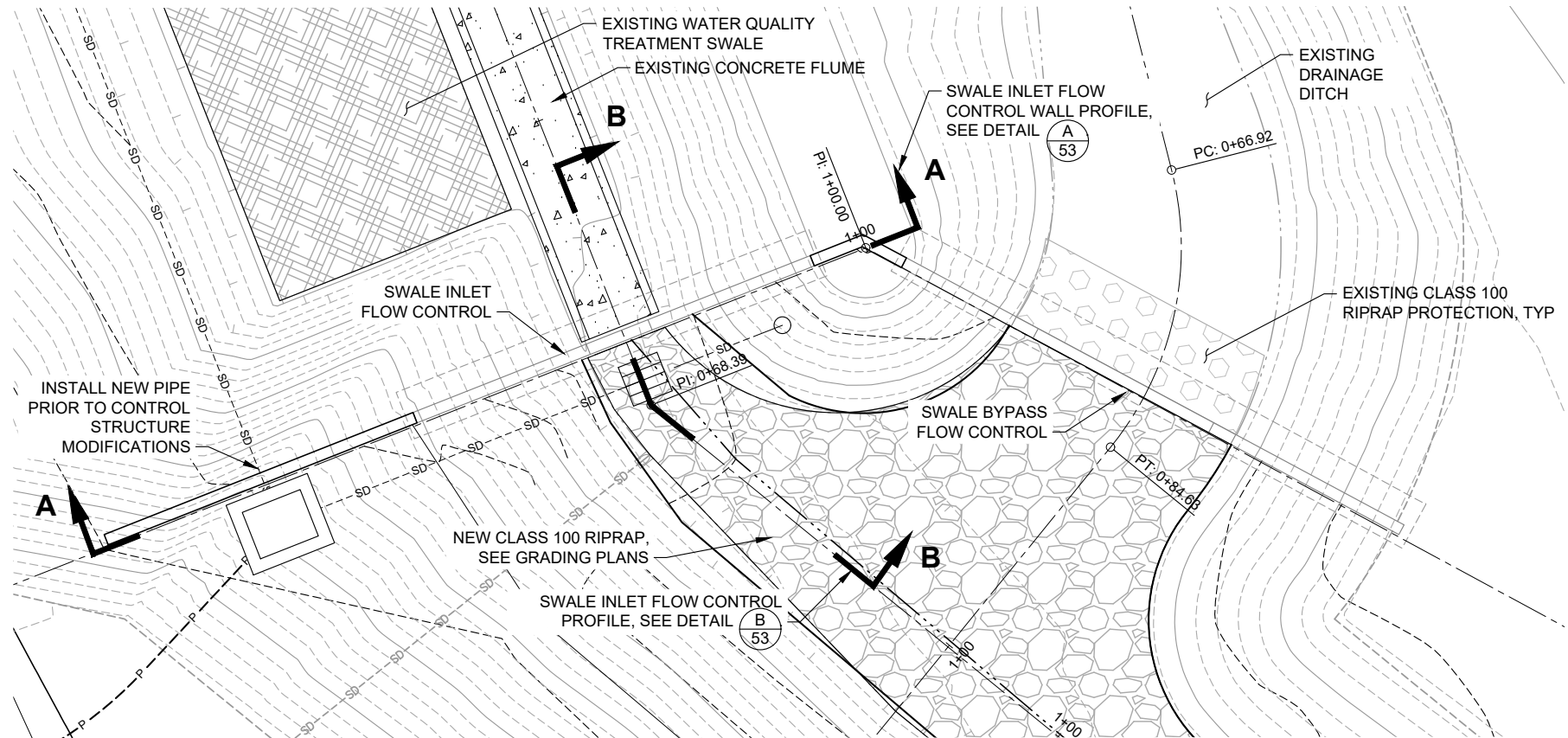
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
**POND BYPASS STRUCTURE
DETAILS**
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

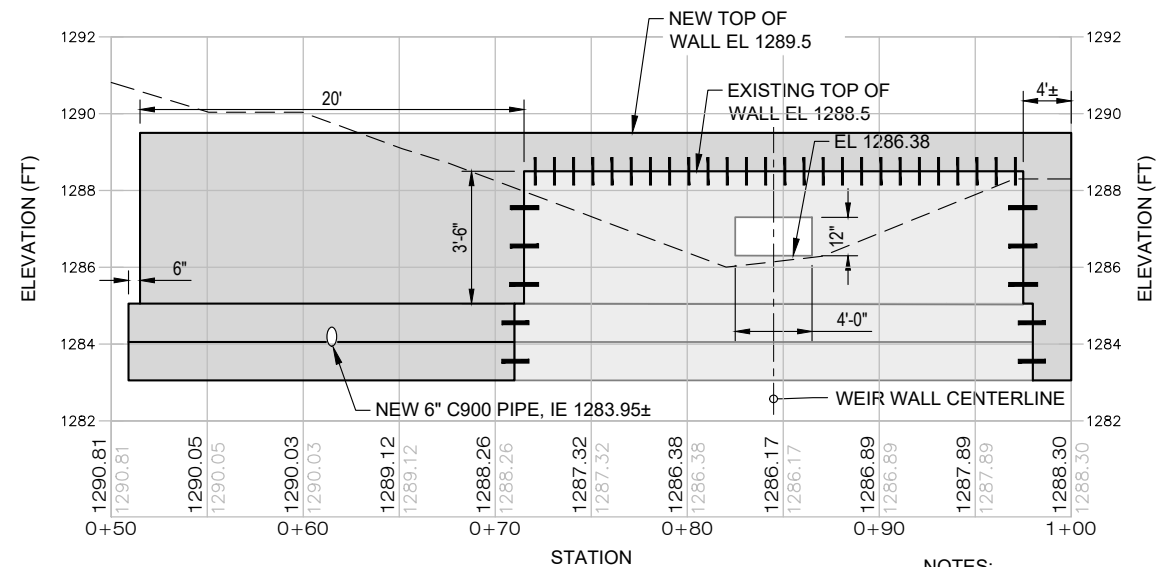
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OF 71

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SOUTH TREATMENT FLOW CONTROL STRUCTURE DETAIL
NTS

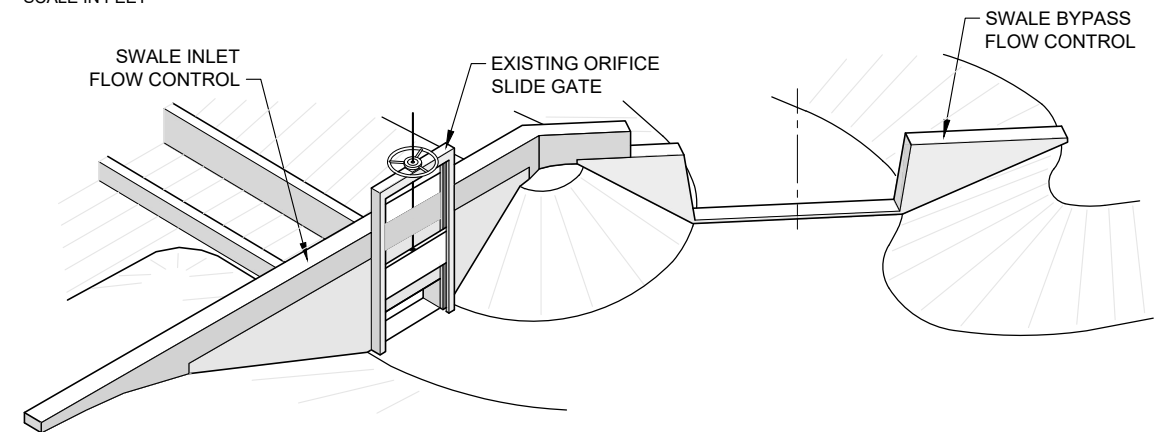
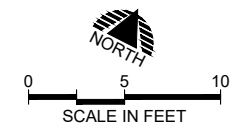
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SOUTH TREATMENT FLOW CONTROL STRUCTURE WALL PROFILE
NTS

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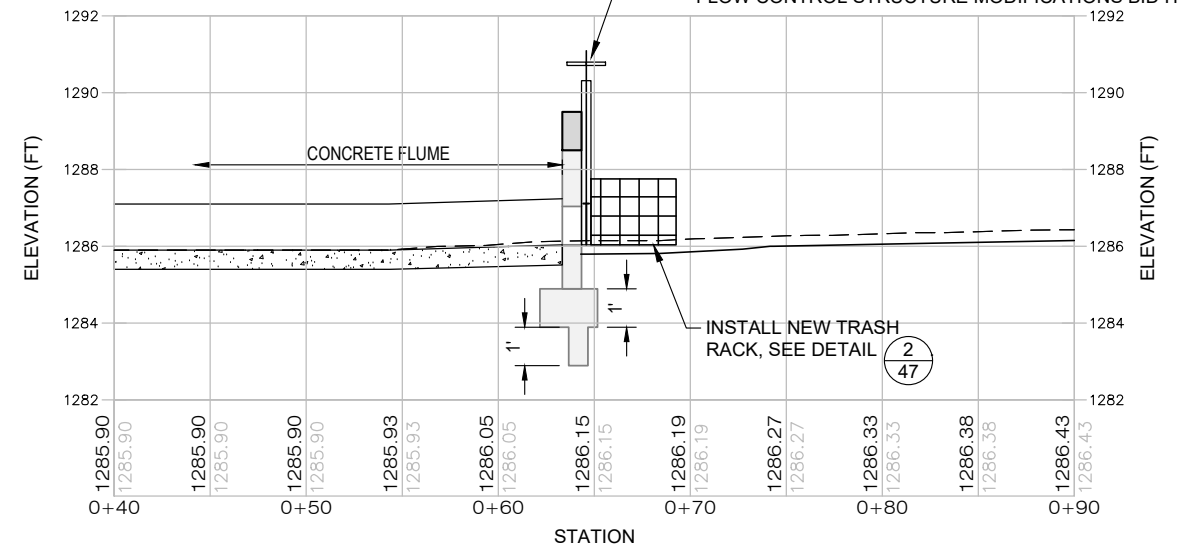
- NOTES:
1. INSTALL STEEL DOWELS BETWEEN NEW AND EXISTING CONCRETE, TYP, CENTER DOWELS ON KEY, SLAB FOUNDATION AND WALL, SEE CONTROL STRUCTURE DOWEL NOTES ON SHEET 43.
 2. ALL WORK AND MATERIALS TO MODIFY THE EXISTING WALL AS SHOWN SHALL BE PAID UNDER THE POND BYPASS FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



SOUTH TREATMENT CONTROL STRUCTURE OBLIQUE VIEW
NTS

2
53

THE CONTRACTOR SHALL REMOVE THE EXISTING GATE DURING CONTROL STRUCTURE MODIFICATIONS AND REATTACH WHEN SUITABLE AFTER CONTROL STRUCTURE MODIFICATIONS ARE COMPLETE. THE CONTRACTOR SHALL PROVIDE SHOP DRAWINGS TO EXTEND THE EXISTING GATE TO REACH NEW WALL HEIGHT AS NEEDED. ALL WORK AND MATERIALS TO EXTEND THE GATE TO BE SUITABLE FOR OPERATION AFTER THE MODIFICATIONS HAVE BEEN COMPLETE SHALL BE PAID UNDER THE SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



SOUTH TREATMENT FLOW CONTROL STRUCTURE FLOWLINE PROFILE
NTS

B
53

1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



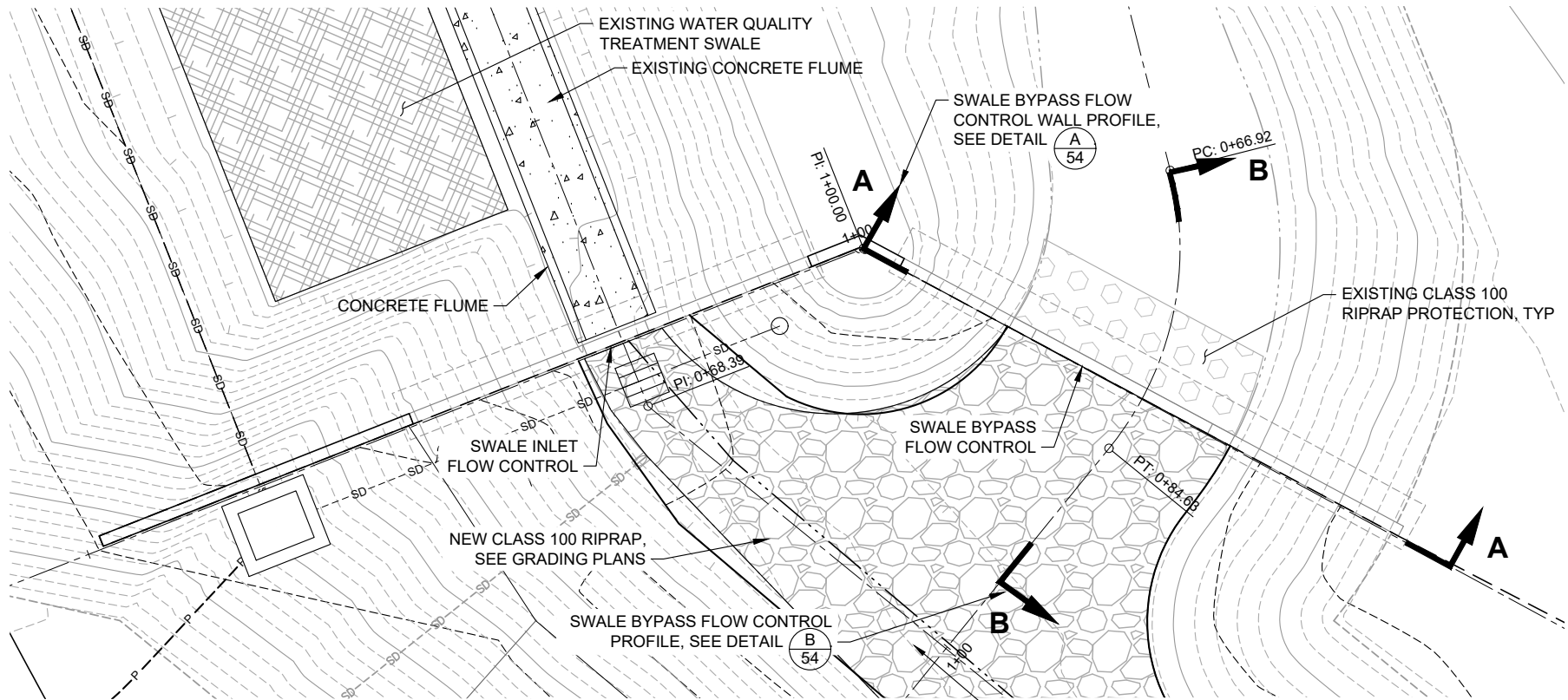
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| REVISIONS: | DATE | APPD. |

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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
SWALE INLET STRUCTURE DETAILS
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

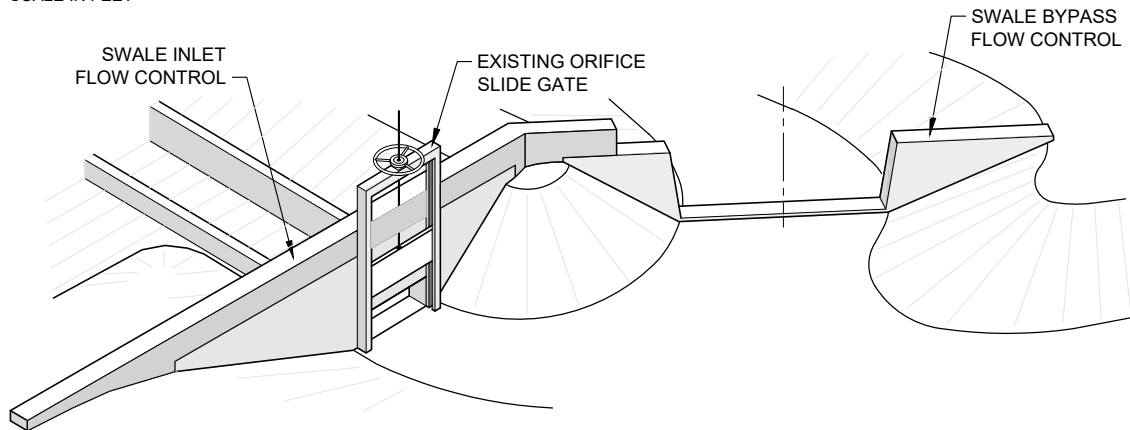
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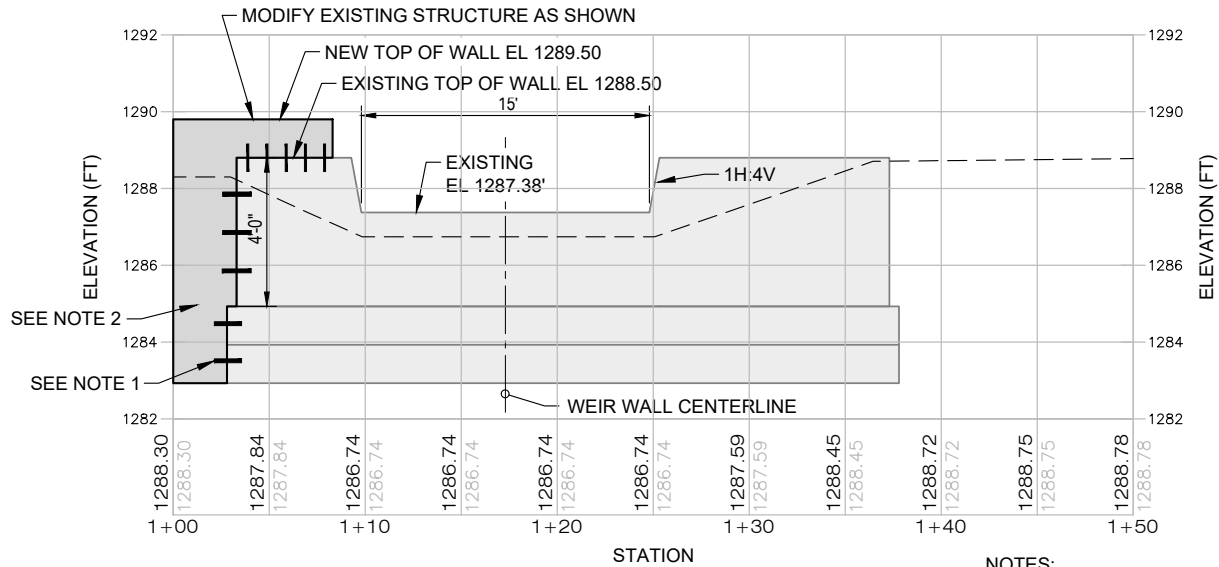
SOUTH TREATMENT BYPASS
FLOW CONTROL STRUCTURE DETAIL
NTS

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54



SOUTH TREATMENT
CONTROL STRUCTURE OBLIQUE VIEW
NTS

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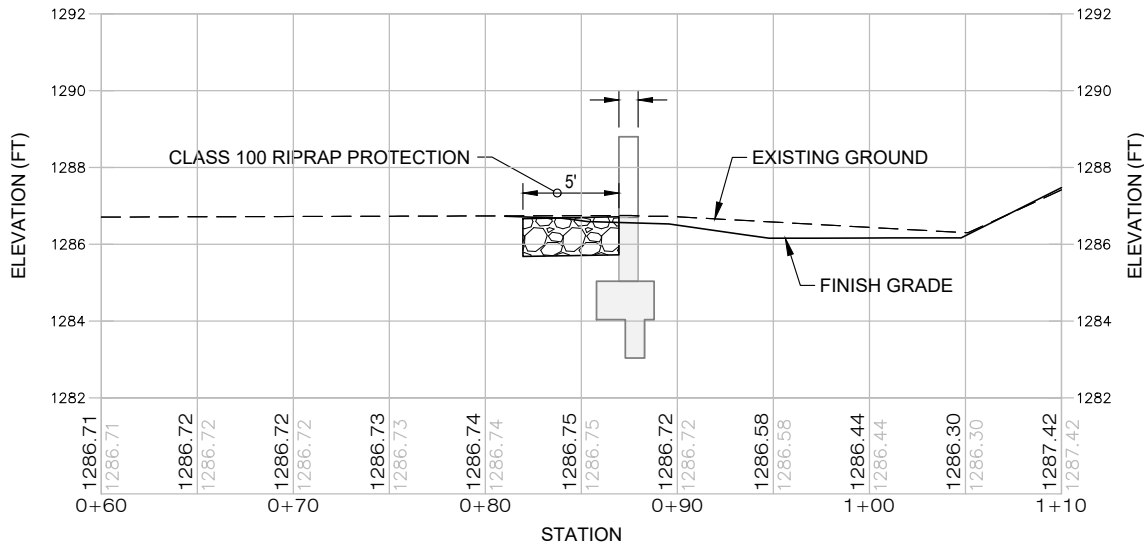


SOUTH TREATMENT BYPASS CONTROL
STRUCTURE WALL PROFILE
NTS

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54

NOTES:

1. INSTALL STEEL DOWELS BETWEEN NEW AND EXISTING CONCRETE, TYP, CENTER DOWELS ON KEY, SLAB FOUNDATION AND WALL, SEE CONTROL STRUCTURE DOWEL NOTES ON SHEET 43.
2. ALL WORK AND MATERIALS TO MODIFY THE EXISTING WALL AS SHOWN SHALL BE PAID UNDER THE POND BYPASS FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



SOUTH TREATMENT BYPASS CONTROL
STRUCTURE FLOWLINE PROFILE
NTS

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54

1 REPLACE ENTIRE SHEET



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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

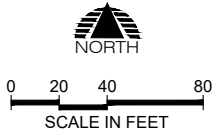
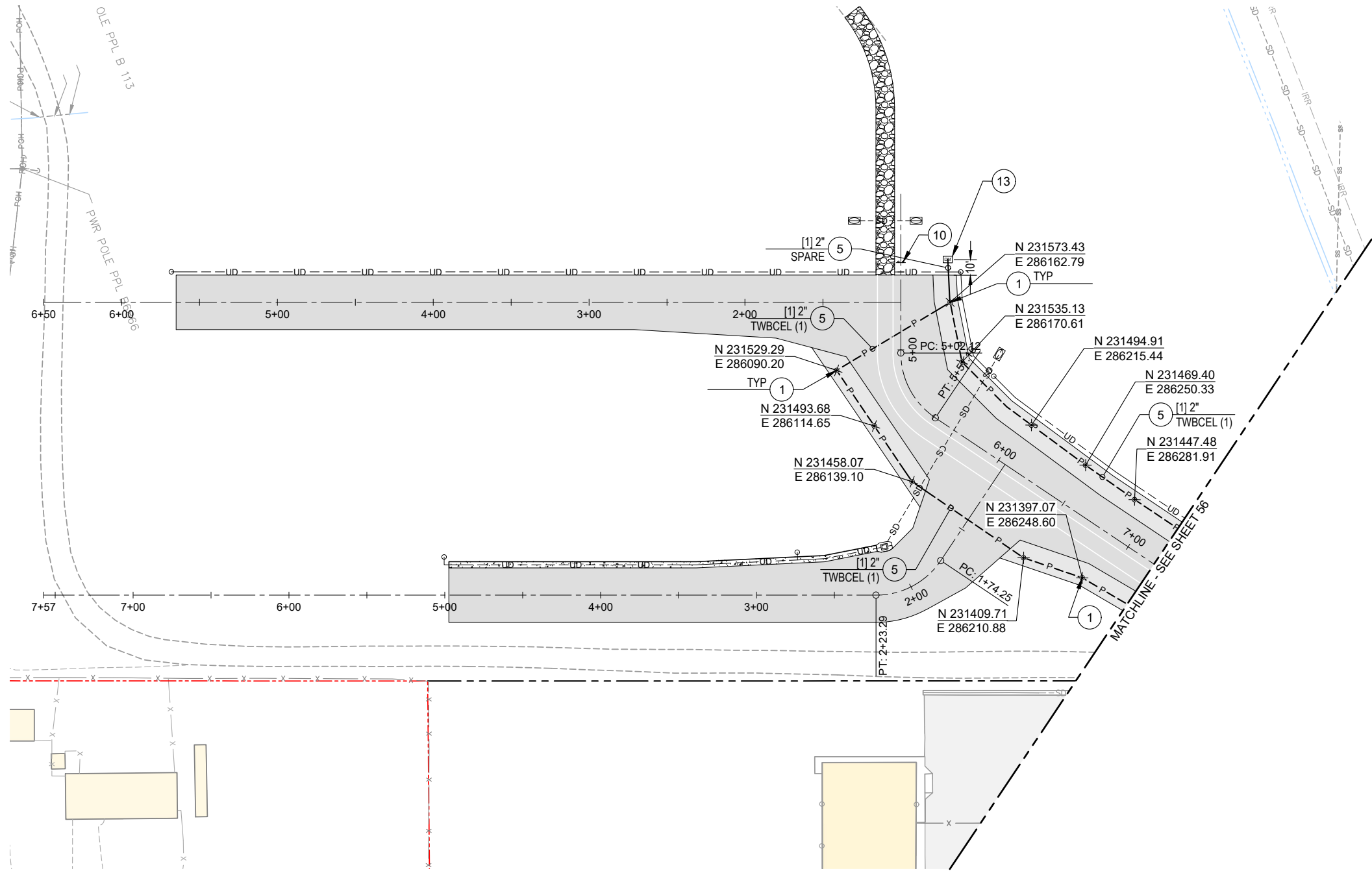
**SWALE BYPASS STRUCTURE
DETAILS**

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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ELECTRICAL CONSTRUCTION NOTES

- 1 INSTALL NEW LED ELEVATED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 1/61
 - 2 INSTALL NEW LED FLUSH-MOUNTED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 3/61
 - 3 REMOVE LIGHT AND TRANSFORMER AND INSTALL SOLID LID, SIMILAR TO DETAIL 1/64
 - 4 INSTALL HANDHOLE, SEE DETAIL 1/60
 - 5 INSTALL CONDUIT, SIZE AS NOTED, SEE DETAILS ON SHEET 59
 - 6 REMOVE EXISTING CABLE AND INSTALL NEW CIRCUIT(S) IN EXISTING CONDUIT OR ELECTRICAL STRUCTURE, AS NOTED
 - 7 EXPOSE AND CONNECT TO EXISTING CONDUIT
 - 8 INSTALL GUIDANCE SIGN, SEE DETAIL 2/62 AND SHEET 58 FOR PANEL LEGEND
 - 9 CONCRETE ENCASEMENT OF DUCTS, SEE DETAIL 2/60
 - 10 INSTALL RETROREFLECTIVE TAXIWAY END SIGN, SEE DETAIL 1/63
 - 11 EXTEND EXISTING GUIDANCE SIGN FOUNDATION AND INSTALL NEW GUIDANCE SIGN, SEE DETAIL 3/63
 - 12 GUIDANCE SIGN PANEL REPLACEMENT, SEE SHEET 58
 - 13 INSTALL CONCRETE DUCT MARKER, SEE DETAIL 2/65
 - 14 INSTALL AUTOMATIC VALVE SYSTEM, SEE DETAIL 1/38
- A SIDE 'A'
2 GUIDANCE SIGN DESIGNATION, SEE SHEET 58 FOR LEGEND
B SIDE 'B'

ELECTRICAL LEGEND

- C--- NEW CIRCUITS TO BE PLACED IN EXISTING CONDUIT, REPLACE EXISTING CIRCUITS IN CONDUITS AS INDICATED
- P--- NEW CONDUITS WITH NEW CIRCUITS, AND/OR NEW SPARE CONDUITS
- (1) 2" 5 INDICATES NUMBER OF NEW CONDUITS AND SIZE

CIRCUIT LEGEND

| ABBREVIATION | DESCRIPTIONS | CABLE COLOR |
|--------------|---|-------------|
| TWBCEL | TAXIWAY B AND TAXIWAY C EDGE LIGHTS | TERRA COTTA |
| TWBCGS | TAXIWAY B AND TAXIWAY C GUIDANCE SIGN | GREY |
| TWAE | TAXIWAY A EDGE LIGHTS | BLACK |
| VALVE PWR | THREE XX CONDUCTORS | TBD |
| (1) | INDICATES NUMBER OF NO.8 5KV CABLES PER CIRCUIT | |

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



| | | |
|----------------|-----------|-------|
| ADDENDUM NO. 1 | 5/24/2024 | JGL |
| REVISIONS: | DATE | APPD. |

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| DATE: | MAY 2024 |
| DESIGN: | KJK |
| DRAWN: | JAW |
| CHECKED: | JGL |
| REVISION NUMBER: | 0 |
| SCALE: | AS SHOWN |

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

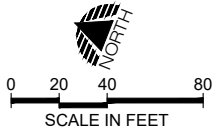
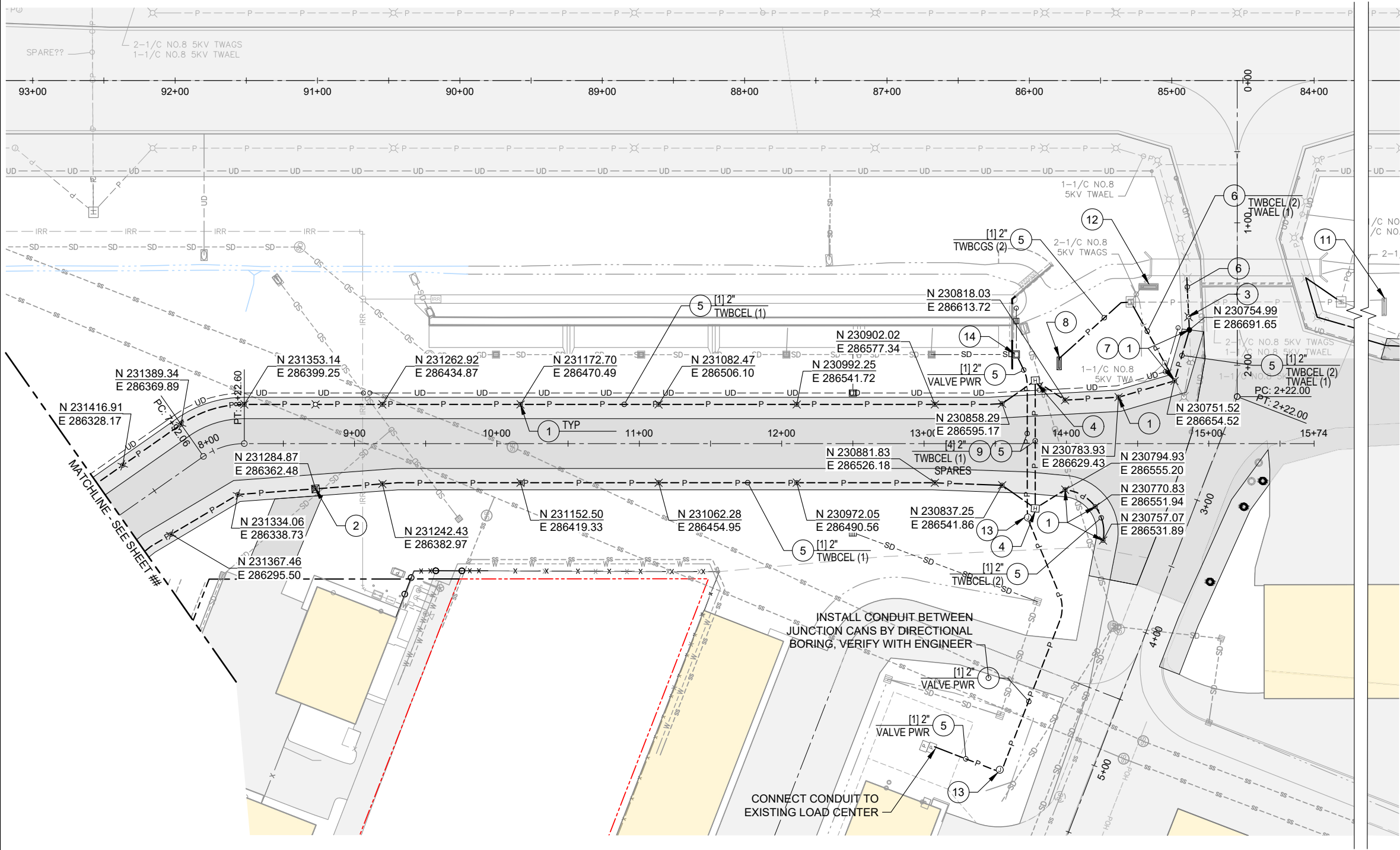
ELECTRICAL PLAN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
55
OF 71

05/24/24 - 2:16pm - BFortenberry - P:\R\RV\2020-W04 NW Devel\0400CAD\DWG\Sheets\RV\2020-W04_Electrical.dwg



ELECTRICAL CONSTRUCTION NOTES

- 1 INSTALL NEW LED ELEVATED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 1 61
 - 2 INSTALL NEW LED FLUSH-MOUNTED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 3 61
 - 3 REMOVE LIGHT AND TRANSFORMER AND INSTALL SOLID LID, SIMILAR TO DETAIL 1 64
 - 4 INSTALL HANDHOLE, SEE DETAIL 1 60
 - 5 INSTALL CONDUIT, SIZE AS NOTED, SEE DETAILS ON SHEET 59
 - 6 REMOVE EXISTING CABLE AND INSTALL NEW CIRCUIT(S) IN EXISTING CONDUIT OR ELECTRICAL STRUCTURE, AS NOTED
 - 7 EXPOSE AND CONNECT TO EXISTING CONDUIT
 - 8 INSTALL GUIDANCE SIGN, SEE DETAIL 2 62 AND SHEET 58 FOR PANEL LEGEND
 - 9 CONCRETE ENCASEMENT OF DUCTS, SEE DETAIL 2 60
 - 10 INSTALL RETROREFLECTIVE TAXIWAY END SIGN, SEE DETAIL 1 63
 - 11 EXTEND EXISTING GUIDANCE SIGN FOUNDATION AND INSTALL NEW GUIDANCE SIGN, SEE DETAIL 3 63
 - 12 GUIDANCE SIGN PANEL REPLACEMENT, SEE SHEET 58
 - 13 INSTALL CONCRETE DUCT MARKER, SEE DETAIL 2 65
 - 14 INSTALL AUTOMATIC VALVE SYSTEM, SEE DETAIL 1 38
- A SIDE 'A'
2 GUIDANCE SIGN DESIGNATION, SEE SHEET 58 FOR LEGEND
B SIDE 'B'

ELECTRICAL LEGEND

- c --- NEW CIRCUITS TO BE PLACED IN EXISTING CONDUIT, REPLACE EXISTING CIRCUITS IN CONDUITS AS INDICATED
- P --- NEW CONDUITS WITH NEW CIRCUITS, AND/OR NEW SPARE CONDUITS
- (1) 2" 5 INDICATES NUMBER OF NEW CONDUITS AND SIZE

CIRCUIT LEGEND

| ABBREVIATION | DESCRIPTIONS | CABLE COLOR |
|--------------|---|-------------|
| TWBCEL | TAXIWAY B AND TAXIWAY C EDGE LIGHTS | TERRA COTTA |
| TWBCGS | TAXIWAY B AND TAXIWAY C GUIDANCE SIGN | GREY |
| TWAE | TAXIWAY A EDGE LIGHTS | BLACK |
| VALVE PWR | THREE XX CONDUCTORS | TBD |
| (1) | INDICATES NUMBER OF NO.8 5KV CABLES PER CIRCUIT | |

PRECISION APPROACH
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5125 Southwest Hout Street
Corvallis, OR 97333
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DATE: MAY 2024
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

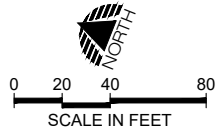
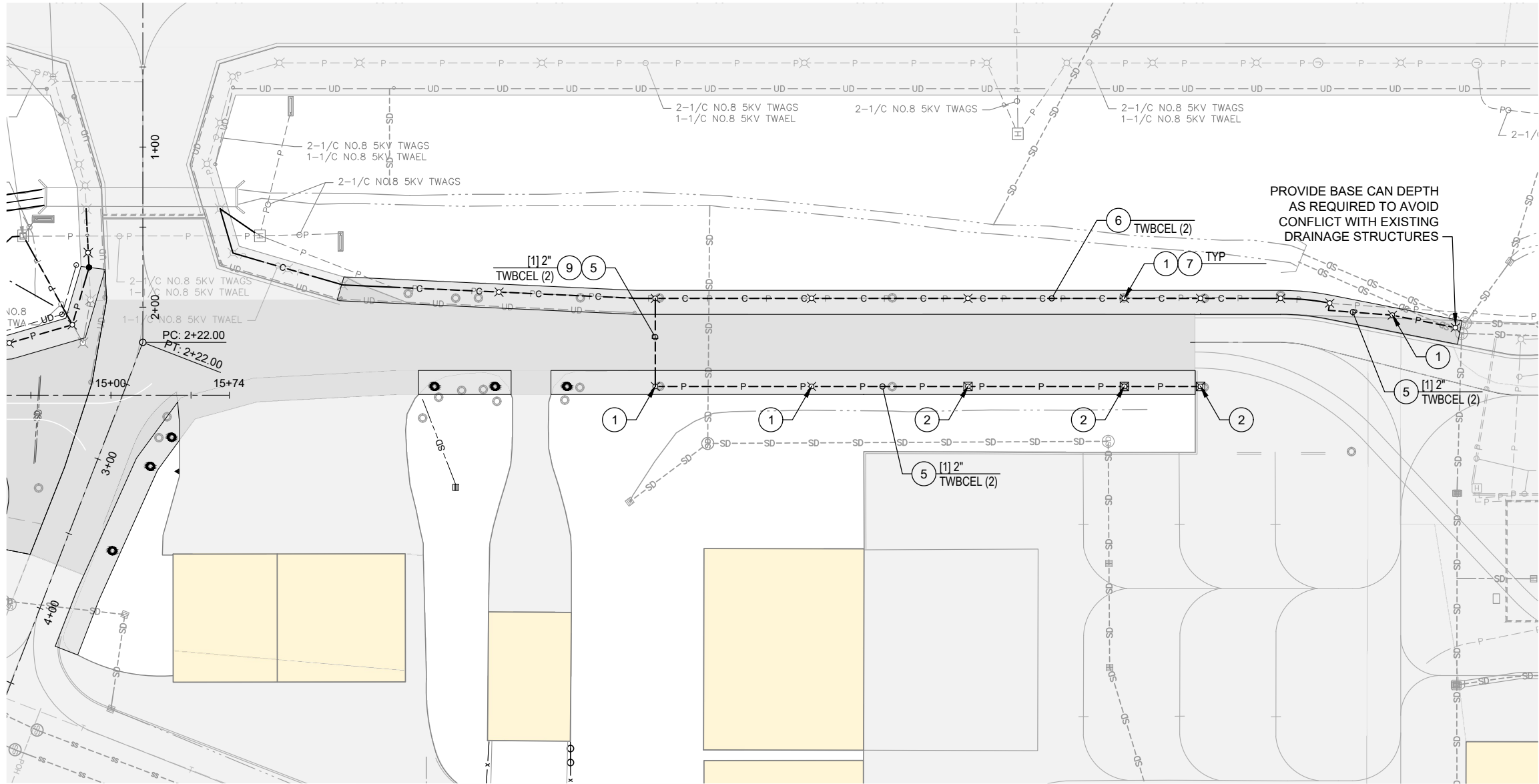
ELECTRICAL PLAN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
56
OF 71

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ELECTRICAL CONSTRUCTION NOTES

1. INSTALL NEW LED ELEVATED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL ¹/₆₁
 2. INSTALL NEW LED FLUSH-MOUNTED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL ³/₆₁
 3. REMOVE LIGHT AND TRANSFORMER AND INSTALL SOLID LID, SIMILAR TO DETAIL ¹/₆₄
 4. INSTALL HANDHOLE, SEE DETAIL ¹/₆₀
 5. INSTALL CONDUIT, SIZE AS NOTED, SEE DETAILS ON SHEET 59
 6. REMOVE EXISTING CABLE AND INSTALL NEW CIRCUIT(S) IN EXISTING CONDUIT OR ELECTRICAL STRUCTURE, AS NOTED
 7. EXPOSE AND CONNECT TO EXISTING CONDUIT
 8. INSTALL GUIDANCE SIGN, SEE DETAIL ²/₆₂ AND SHEET 58 FOR PANEL LEGEND
 9. CONCRETE ENCASEMENT OF DUCTS, SEE DETAIL ²/₆₀
 10. INSTALL RETROREFLECTIVE TAXIWAY END SIGN, SEE DETAIL ¹/₆₃
 11. EXTEND EXISTING GUIDANCE SIGN FOUNDATION AND INSTALL NEW GUIDANCE SIGN, SEE DETAIL ³/₆₃
 12. GUIDANCE SIGN PANEL REPLACEMENT, SEE SHEET 58
 13. INSTALL CONCRETE DUCT MARKER, SEE DETAIL ²/₆₅
 14. INSTALL AUTOMATIC VALVE SYSTEM, SEE DETAIL ¹/₃₈
- ^A/₂ SIDE 'A'
^B GUIDANCE SIGN DESIGNATION, SEE SHEET 58 FOR LEGEND
^B SIDE 'B'

ELECTRICAL LEGEND

- C --- NEW CIRCUITS TO BE PLACED IN EXISTING CONDUIT, REPLACE EXISTING CIRCUITS IN CONDUITS AS INDICATED
- P --- NEW CONDUITS WITH NEW CIRCUITS, AND/OR NEW SPARE CONDUITS
- (1) 2" ⁵ INDICATES NUMBER OF NEW CONDUITS AND SIZE

CIRCUIT LEGEND

| ABBREVIATION | DESCRIPTIONS | CABLE COLOR |
|--------------|---|-------------|
| TWBCEL | TAXIWAY B AND TAXIWAY C EDGE LIGHTS | TERRA COTTA |
| TWBCGS | TAXIWAY B AND TAXIWAY C GUIDANCE SIGN | GREY |
| TWAEL | TAXIWAY A EDGE LIGHTS | BLACK |
| VALVE PWR | THREE XX CONDUCTORS | TBD |
| (1) | INDICATES NUMBER OF NO.8 5KV CABLES PER CIRCUIT | |

REPLACE ENTIRE SHEET

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Corvallis, OR 97333
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1 ADDENDUM NO. 1 5/24/2024 JGL
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

ELECTRICAL PLAN - ADDITIVE

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.

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OF 71



8

31

B2

3



1 REPLACE ENTIRE SHEET



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| REVISION NUMBER: | 0 |

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

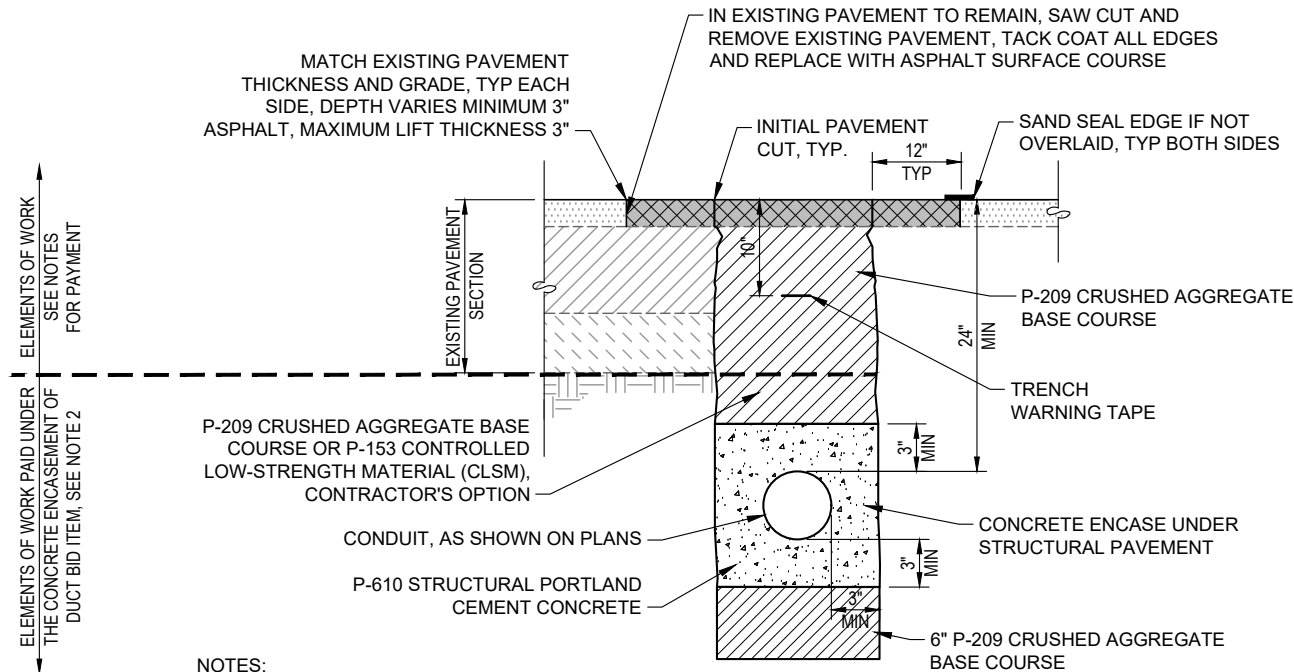
PROJECT NUMBER:
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58

OF **71**

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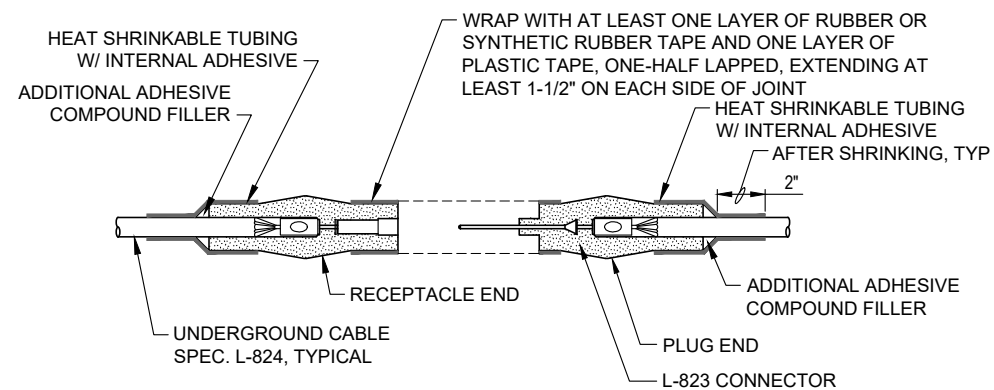
NOTES:

- EXCAVATION, SAND BEDDING AND AGGREGATE BACKFILL CONSIDERED INCIDENTAL TO THE ELECTRICAL TRENCH, EXISTING PAVEMENT BID ITEM.
- ALL CONDUIT AND CIRCUIT TO BE INSTALLED WITHIN THE LIMITS OF THE CONCRETE ENCASED DUCT SHALL BE PAID UNDER THE RESPECTIVE BID ITEM. ALL OTHER ELEMENTS OF WORK TO INSTALL CONCRETE ENCASED DUCT, INCLUDING TRENCHING AND BACKFILL, SHALL BE PAID UNDER THE CONCRETE ENCASEMENT OF DUCT BID ITEM. CONCRETE ENCASEMENT OF CONDUIT IS PAID FOR PER LINEAR FOOT OF TRENCH, REGARDLESS OF THE NUMBER OF CONDUITS INSTALLED, SEE SPECIFICATIONS.
- INITIAL SAWCUT TO PROVIDE AREA OF PAVEMENT REMOVAL FOR TRENCHING. ADDITIONAL SAWCUT INTENDED TO COMPENSATE FOR PAVEMENT UNDERMINING. BACKFILL TO WITHIN 10" OF EXISTING SURFACE PRIOR TO MAKING ADDITIONAL SAWCUT, VERIFY WITH RPR.
- PAVEMENT REMOVAL, ASPHALT TACK COAT AND ASPHALT SURFACE COURSE SHALL BE PAID UNDER THE RESPECTIVE BID ITEMS.
- FOR HORIZONTAL EXTENT OF THE CONCRETE ENCASED DUCT, SEE DETAIL 3/60.

ELECTRICAL TRENCH,
EXISTING PAVED AREA DETAIL

NTS

3
60
1
59



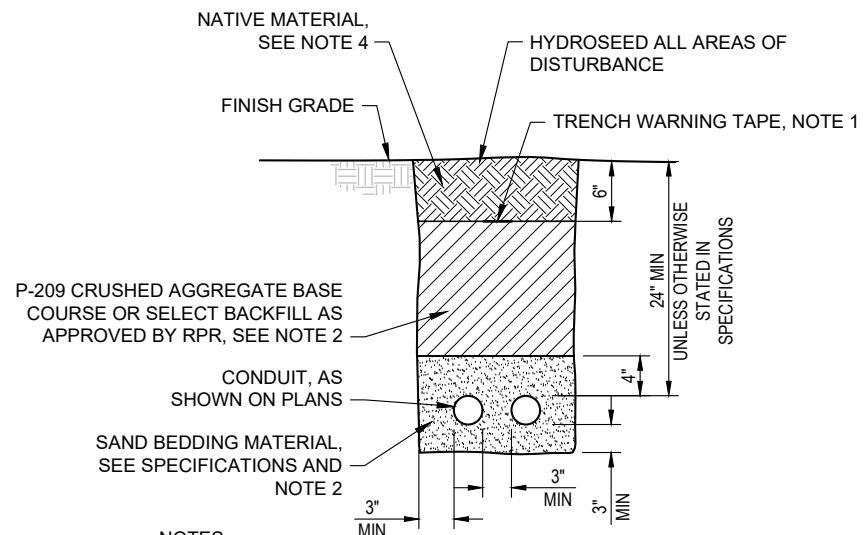
NOTE:

SPLICES TO BE USED AT JUNCTION OF HOMERUN AT THE EEB.

CABLE SPLICE - TYPE B

NTS

3
59



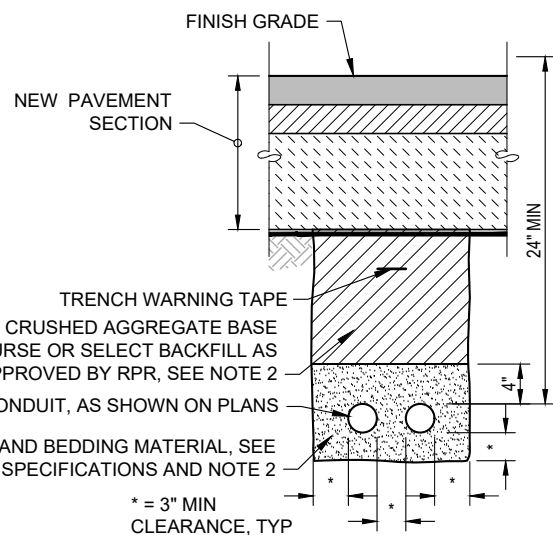
NOTES:

- WARNING TAPE NOT REQUIRED FOR DUCT(S) ENCASED IN CONCRETE.
- BACKFILL CONSIDERED INCIDENTAL TO ELECTRICAL TRENCH NON- PAVED AREA BID ITEM.
- 1-DUCT INSTALLATION SIMILAR. MORE THAN TWO DUCTS MAINTAIN HORIZONTAL AND VERTICAL SPACING.
- STRIP AND STOCKPILE NATIVE TOP SOIL PRIOR TO TRENCHING, PLACE AND COMPACT TO A DENSE AND UNYIELDING CONDITION AS APPROVED BY THE RPR PRIOR TO HYDROSEEDING.

NON-PAVED AREA
ELECTRICAL TRENCH DETAIL

NTS

2
59



NOTES:

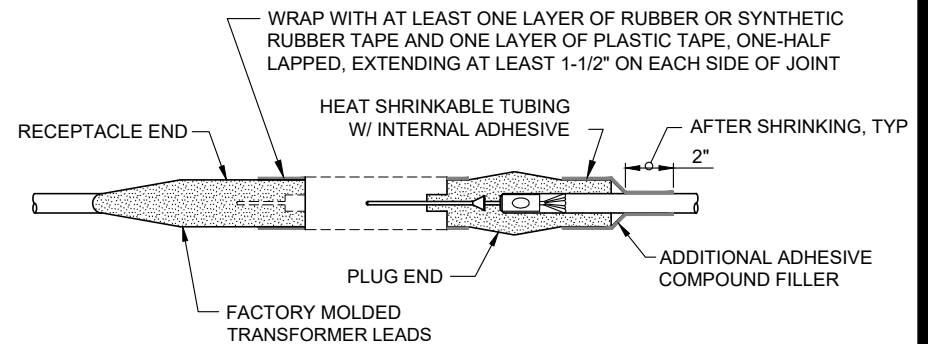
- WARNING TAPE NOT REQUIRED FOR DUCT(S) ENCASED IN CONCRETE.
- BACKFILL CONSIDERED INCIDENTAL TO ELECTRICAL TRENCH, NEW PAVEMENT AREA BID ITEM.

PAVED AREA TRENCH DETAIL

NTS

4
59

REPLACE ENTIRE SHEET



NOTES:

- FOR SPLICES AT LIGHT FIXTURES AND SIGNS.
- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE

CABLE SPLICE - TYPE C

NTS

5
59

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
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1 ADDENDUM NO. 1

REVISIONS:

5/24/2024 JGL

DATE APPD.

DATE: MAY 2024
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CHECKED: JGL
REVISION NUMBER: 0

SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

ELECTRICAL DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

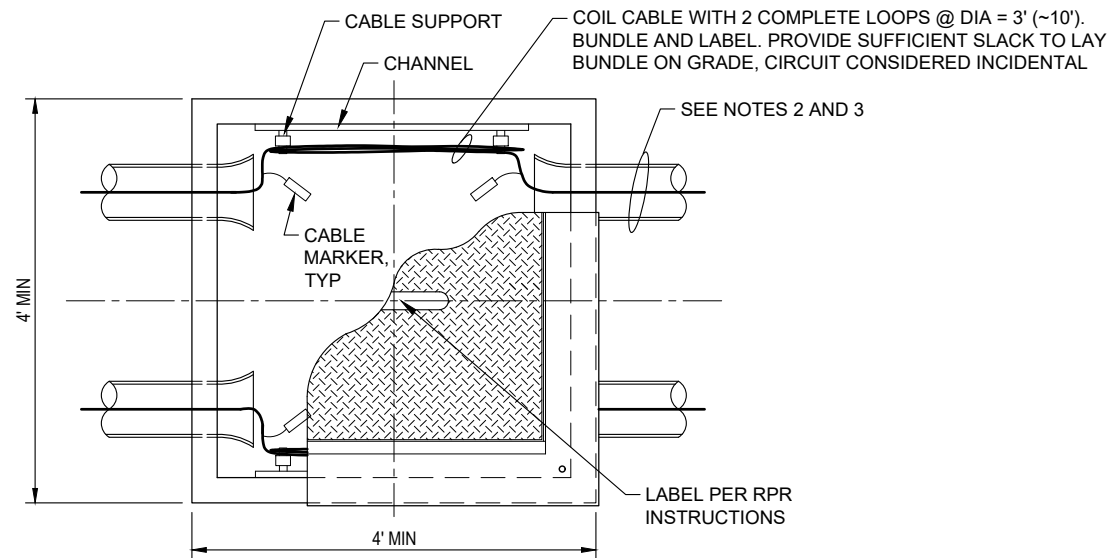
PROJECT NUMBER:
RVI-2020-W04

SHEET NO.

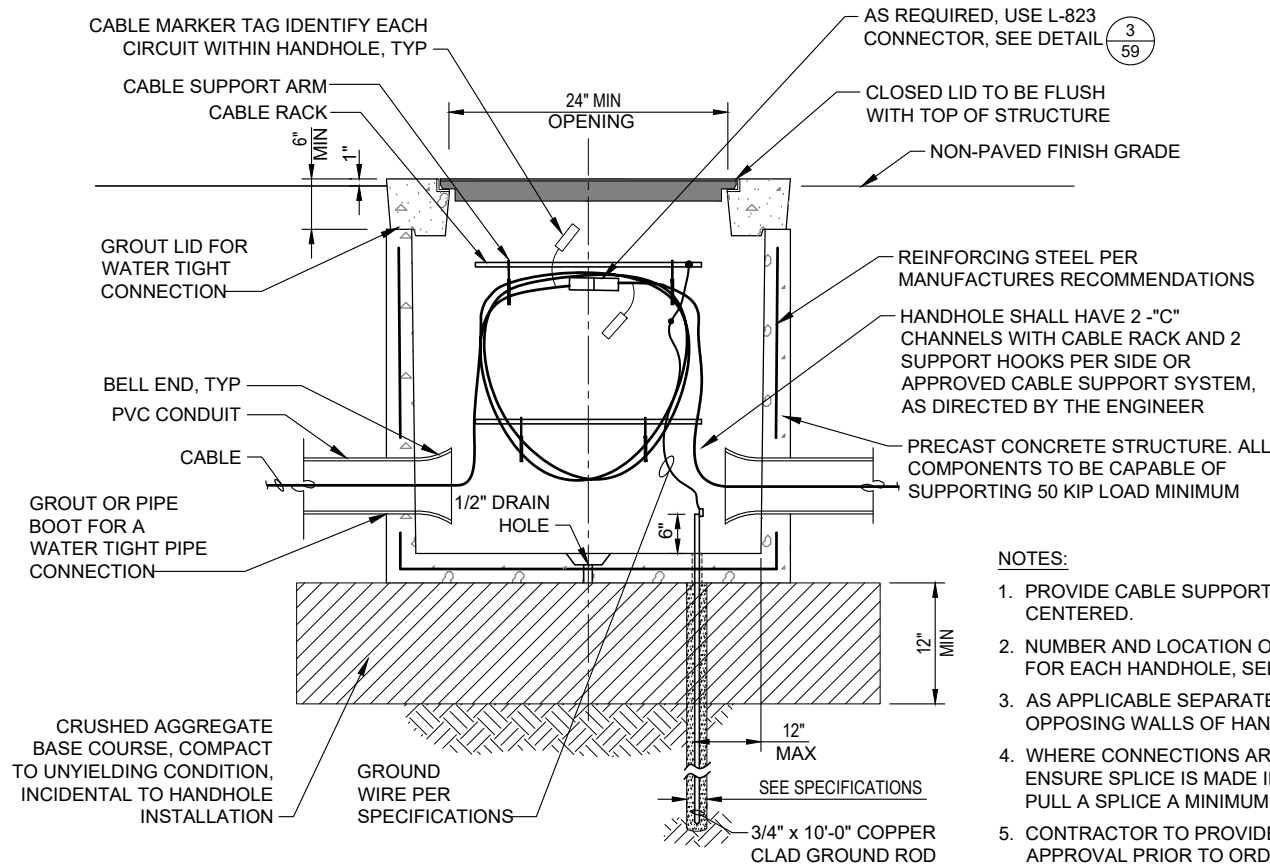
59

OF 71

05/24/24 - 2:17pm - BFortenberry - P:\RVI\2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_ELDE.dwg



PLAN VIEW



PROFILE VIEW

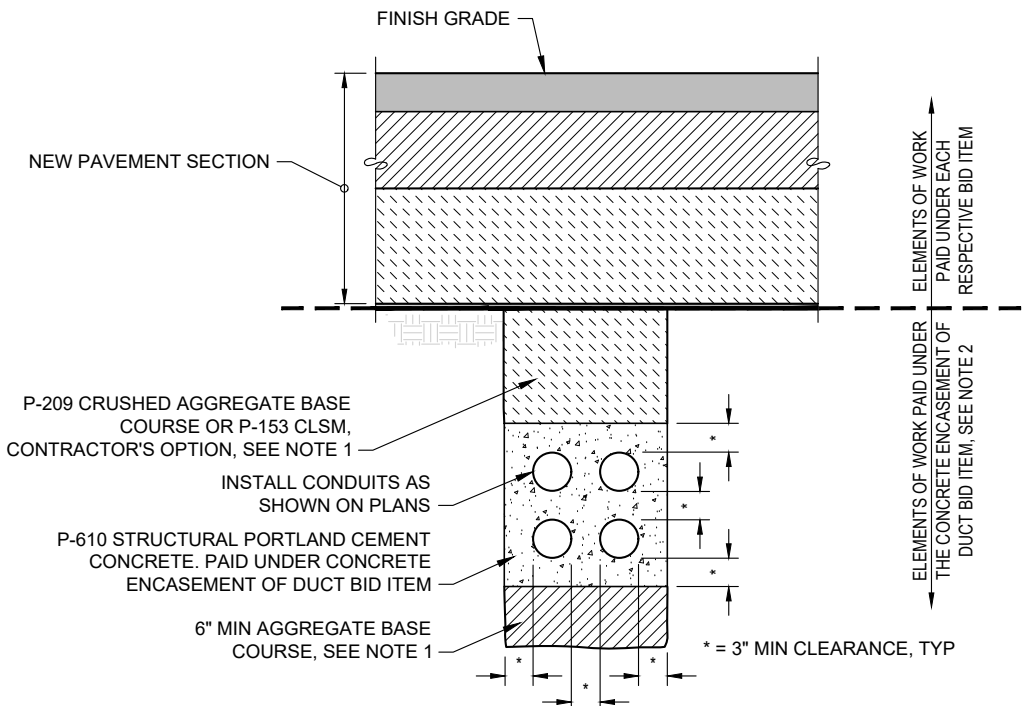
HANDHOLE INSTALLATION DETAIL

NTS

1
60

NOTES:

1. PROVIDE CABLE SUPPORT LEVEL, MINIMUM, WITH SPLICE
CENTERED.
2. NUMBER AND LOCATION OF CONDUITS AND CABLE WILL VARY
FOR EACH HANDHOLE, SEE PLANS.
3. AS APPLICABLE SEPARATE HIGH AND LOW VOLTAGE CABLES TO
OPPOSING WALLS OF HANDHOLE OR AS DIRECTED BY THE RPR.
4. WHERE CONNECTIONS ARE MADE TO EXISTING CIRCUITS
ENSURE SPLICE IS MADE IN HANDHOLE WITH ENOUGH SLACK TO
PULL A SPLICE A MINIMUM 18" ABOVE HANDHOLE.
5. CONTRACTOR TO PROVIDE SHOP DRAWINGS FOR RPR
APPROVAL PRIOR TO ORDERING STRUCTURES.
6. LID TO HINGE OPEN AWAY FROM TAXIWAY.
7. VERIFY NEW HANDHOLE LOCATION WITH RPR.



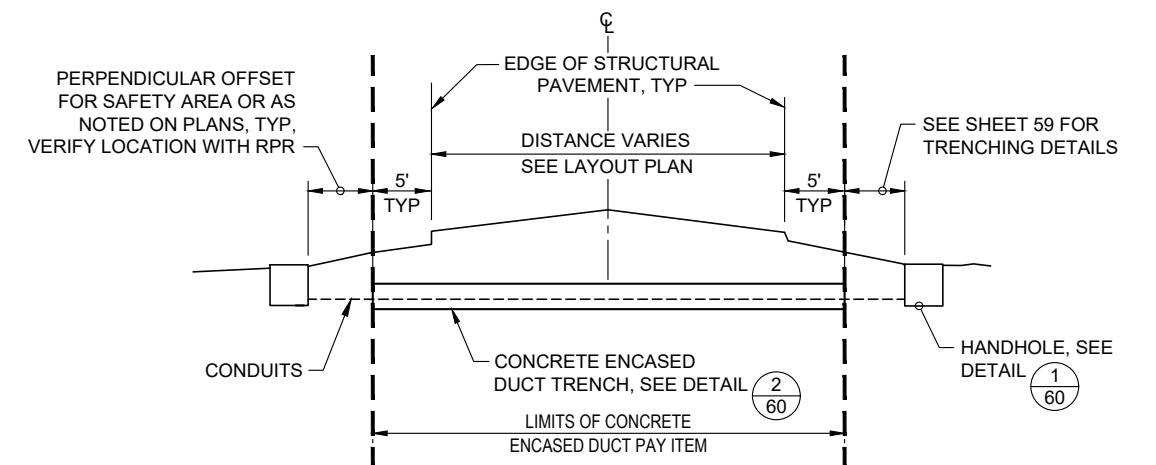
NOTES:

1. CONCRETE ENCASED DUCTS SHALL BE IN PLACE PRIOR TO CONSTRUCTING NEW PAVEMENT SECTION.
2. ALL CONDUIT AND CIRCUIT TO BE INSTALLED WITHIN THE LIMITS OF THE CONCRETE ENCASED DUCT
SHALL BE PAID UNDER THE RESPECTIVE BID ITEM. ALL OTHER ELEMENTS OF WORK TO INSTALL
CONCRETE ENCASED DUCT, INCLUDING TRENCHING AND BACKFILL, SHALL BE PAID UNDER THE
CONCRETE ENCASEMENT OF DUCT BID ITEM. CONCRETE ENCASEMENT OF CONDUIT IS PAID FOR PER
LINEAR FOOT OF TRENCH, REGARDLESS OF THE NUMBER OF CONDUITS INSTALLED, SEE SPECIFICATIONS.
3. FOR HORIZONTAL EXTENT OF THE CONCRETE ENCASED DUCT, SEE DETAIL (3/60)

CONCRETE ENCASED
OF DUCT TRENCH DETAIL

NTS

2
60



PAVEMENT CONDUIT CROSSING DETAIL

NTS

3
60

1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



1 ADDENDUM NO. 1

REVISIONS:

5/24/2024 JGL
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REVISION
NUMBER: 0

SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

ELECTRICAL DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

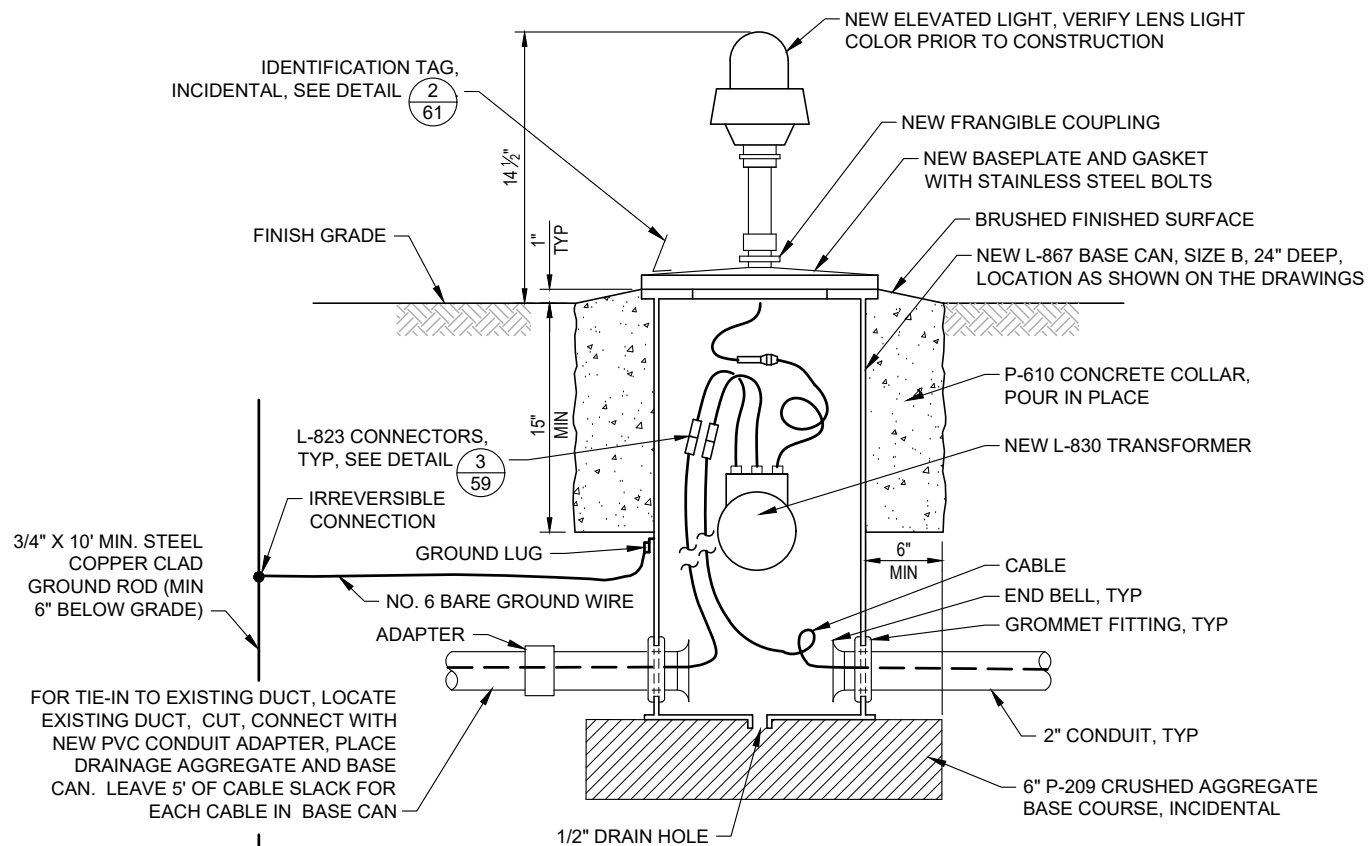
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RVI-2020-W04

SHEET NO.

60

OF 71

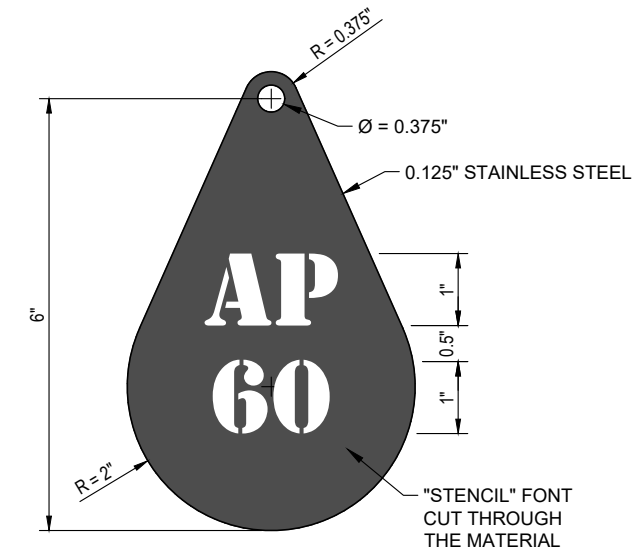
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- NOTES:
- WHERE NEW ELEVATED LIGHTS ARE INSTALLED, ANY PARALLEL CONDUITS SHALL BYPASS THE JUNCTION CAN UNLESS OTHERWISE INDICATED ON PLANS.
 - WHERE NEW ELEVATED LIGHTS ARE TO BE INSTALLED IN EXISTING BASE CANS, REMOVE AND REPLACE EXISTING TRANSFORMER AND SPLICES.

ELEVATED LIGHT INSTALLATION DETAIL

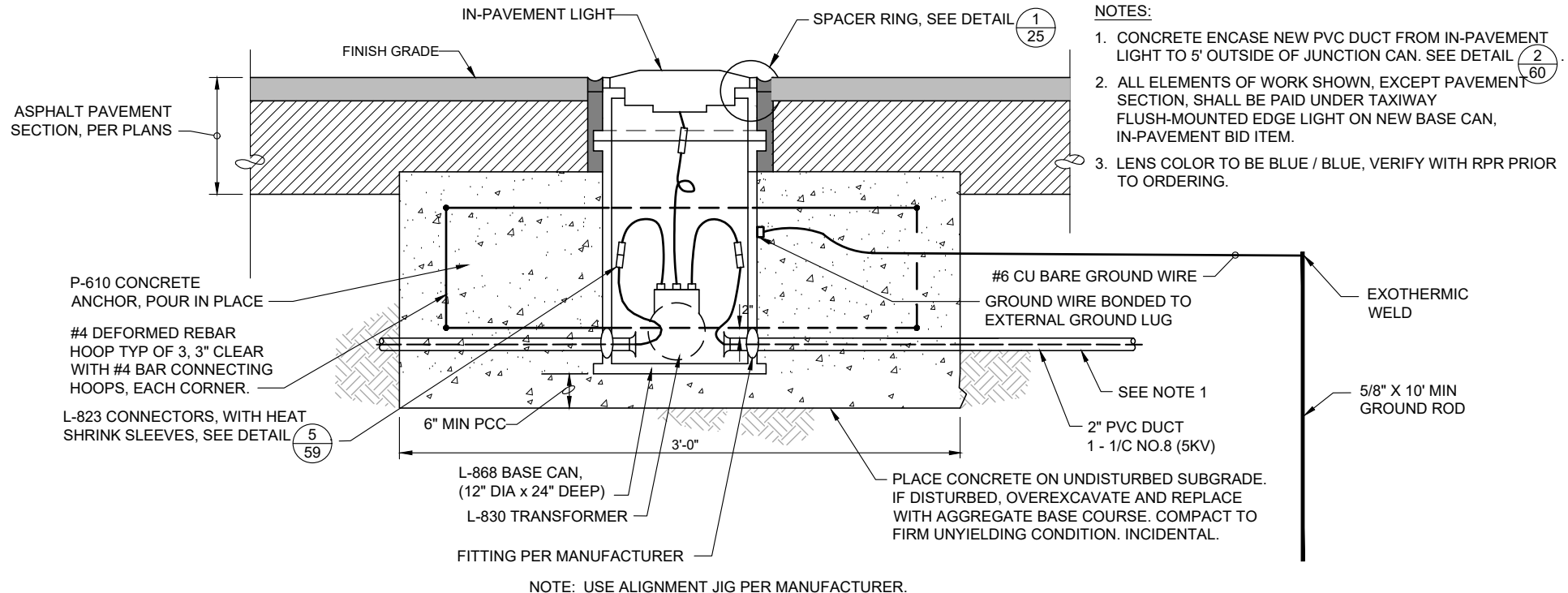
1
61



- NOTES:
- ATTACH TO BASE CAN USING BOLT PROVIDED BY LIGHT MANUFACTURER, VERIFY WITH ENGINEER.
 - END IDENTIFICATIONS TAGS SHALL BE CONSIDERED INCIDENTAL TO THE TAXIWAY EDGE LIGHT ON THE NEW BASE CAN BID ITEM.

IDENTIFICATION TAG DETAIL

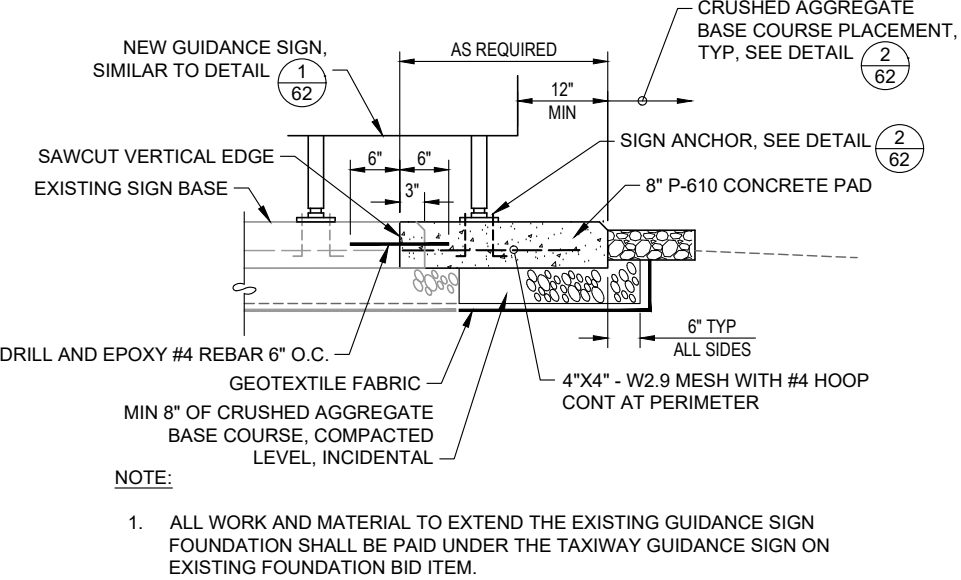
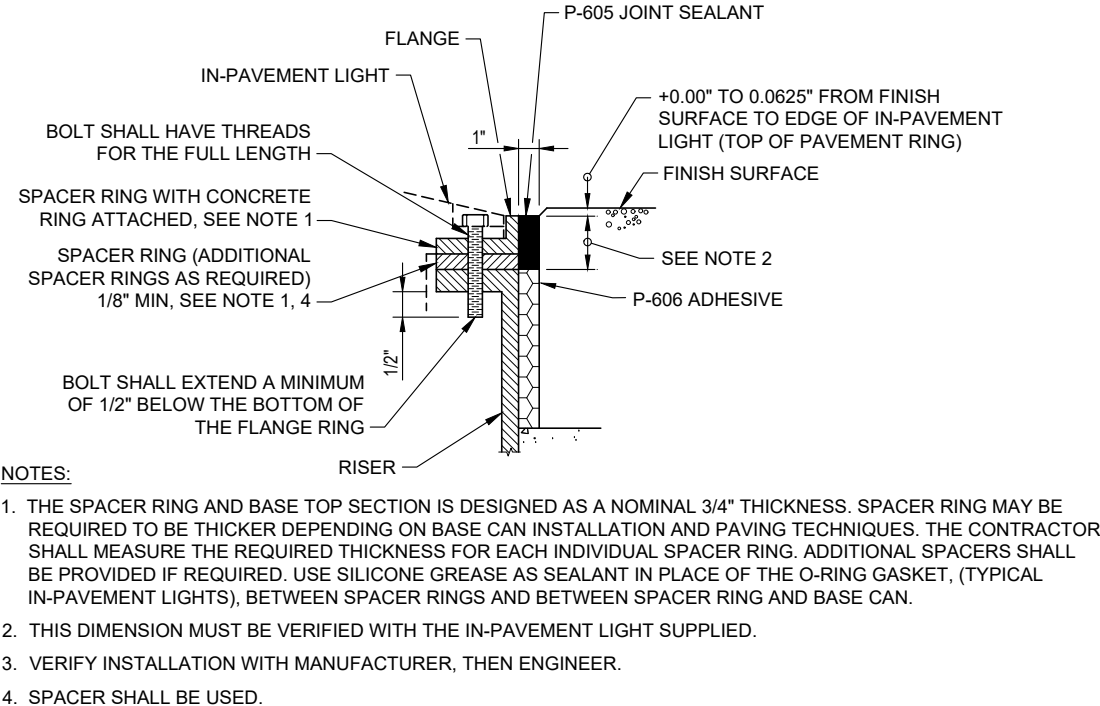
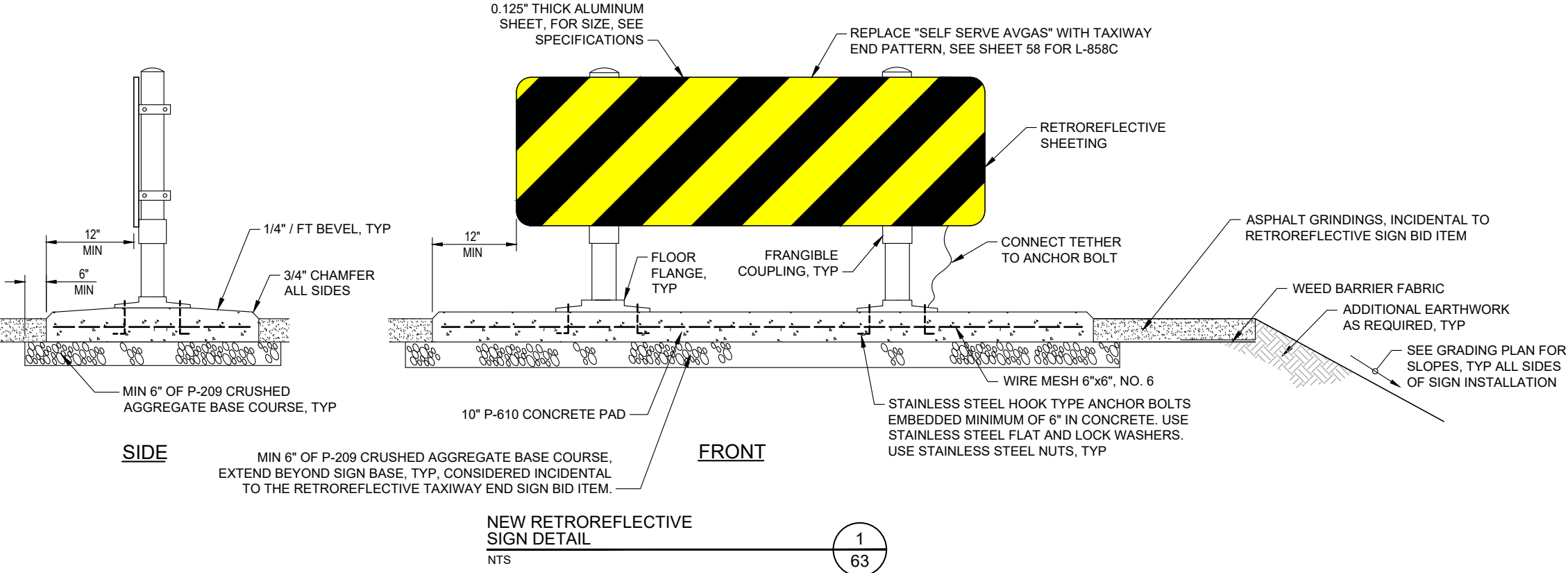
2
61



TAXIWAY IN-PAVEMENT LIGHT DETAIL

3
61

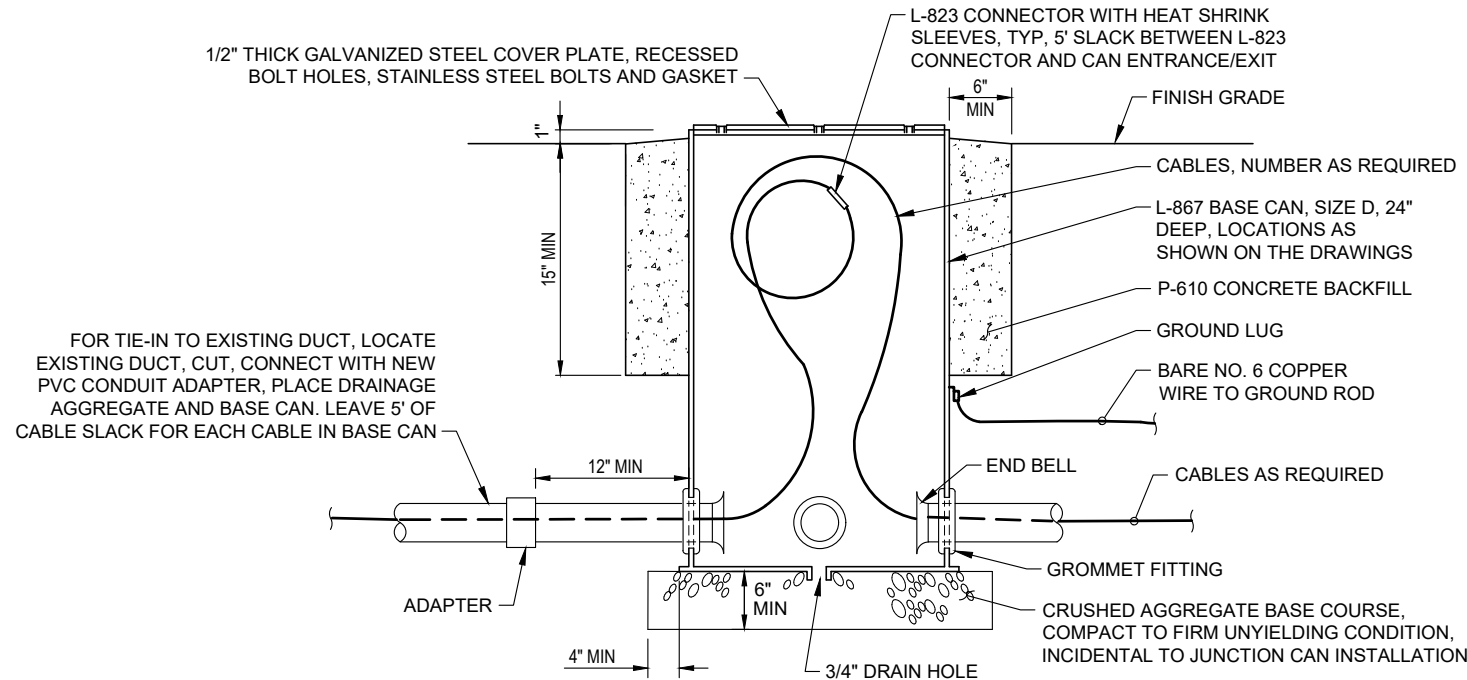
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1 REPLACE ENTIRE SHEET

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|--|--|---|--|---|
| <p>PRECISION APPROACH</p> <p>ENGINEERING</p> <p>5125 Southwest Hout Street Corvallis, OR 97333 541-754-0043</p> | <p>REGISTERED PROFESSIONAL ENGINEER 77750PE DIGITALLY SIGNED JOSHUA G. LEIKER JUNE 2, 2010 OREGON EXPIRES 12/31/2024</p> | <p>DATE: MAY 2024 DESIGN: KJK DRAWN: JAW CHECKED: JGL REVISION NUMBER: 0</p> <p>SCALE: AS SHOWN</p> | <p>ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES</p> <p>ELECTRICAL DETAILS</p> <p>PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063</p> | <p>PROJECT NUMBER: RVI-2020-W04</p> <p>SHEET NO. 63 OF 71</p> |
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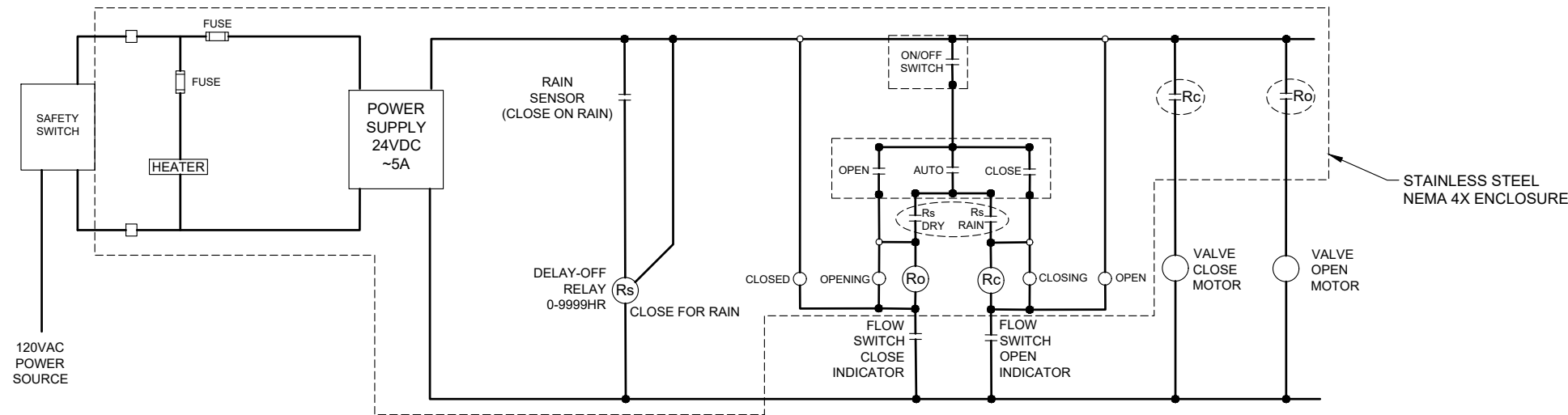
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JUNCTION CAN INSTALLATION DETAIL

1
64

NTS



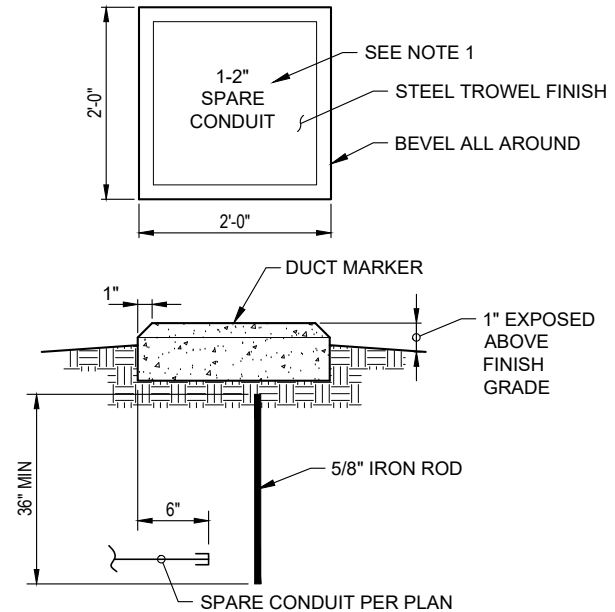
GENERAL NOTES:

1. POWER SUPPLY PROVIDES 24VDC SOURCE FOR RAIN SENSOR, RELAYS, AND VALVE MOTOR.
2. RAIN SENSOR CLOSSES UPON SENSING RAIN.
3. DELAY-OFF RELAY CLOSSES WHEN RAIN SENSOR DETECTS RAIN AND IS DELAYED UP TO 9999 HOURS WHEN RAIN STOPS.
4. OPEN-AUTO-CLOSE SWITCH FOR MANUAL VALVE OPERATION.
5. ON/OFF SWITCH FOR CONTROL DEFEAT.
6. WHEN RAIN IS DETECTED, Rs CLOSSES ENERGIZING Rc RELAY, Rc RELAY ACTIVATES CLOSE MOTOR, VALVE CLOSSES.
7. WHEN RAIN STOPS, AFTER DELAY, Rs ACTIVATES Ro RELAY ACTIVATES OPEN MOTOR, VALVE OPENS.
8. VALVE USES 24VDC CONTROL AND HAS STATUS CONTACTS USED FOR CONTROL.
9. LIGHTS ARE PROVIDED TO INDICATE VALVE STATE AND CLOSE OR OPEN IN PROGRESS.

DRAINAGE VALVE CONTROLLER

3
64

NTS



NOTES:

1. VERIFY TEXT AND SYMBOL WITH ENGINEER
2. DUCT AND CABLE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE 2-INCH PVC CONDUIT BID ITEM.

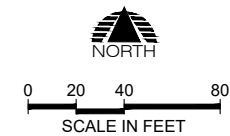
SPARE CONDUIT MARKER DETAIL

2
64

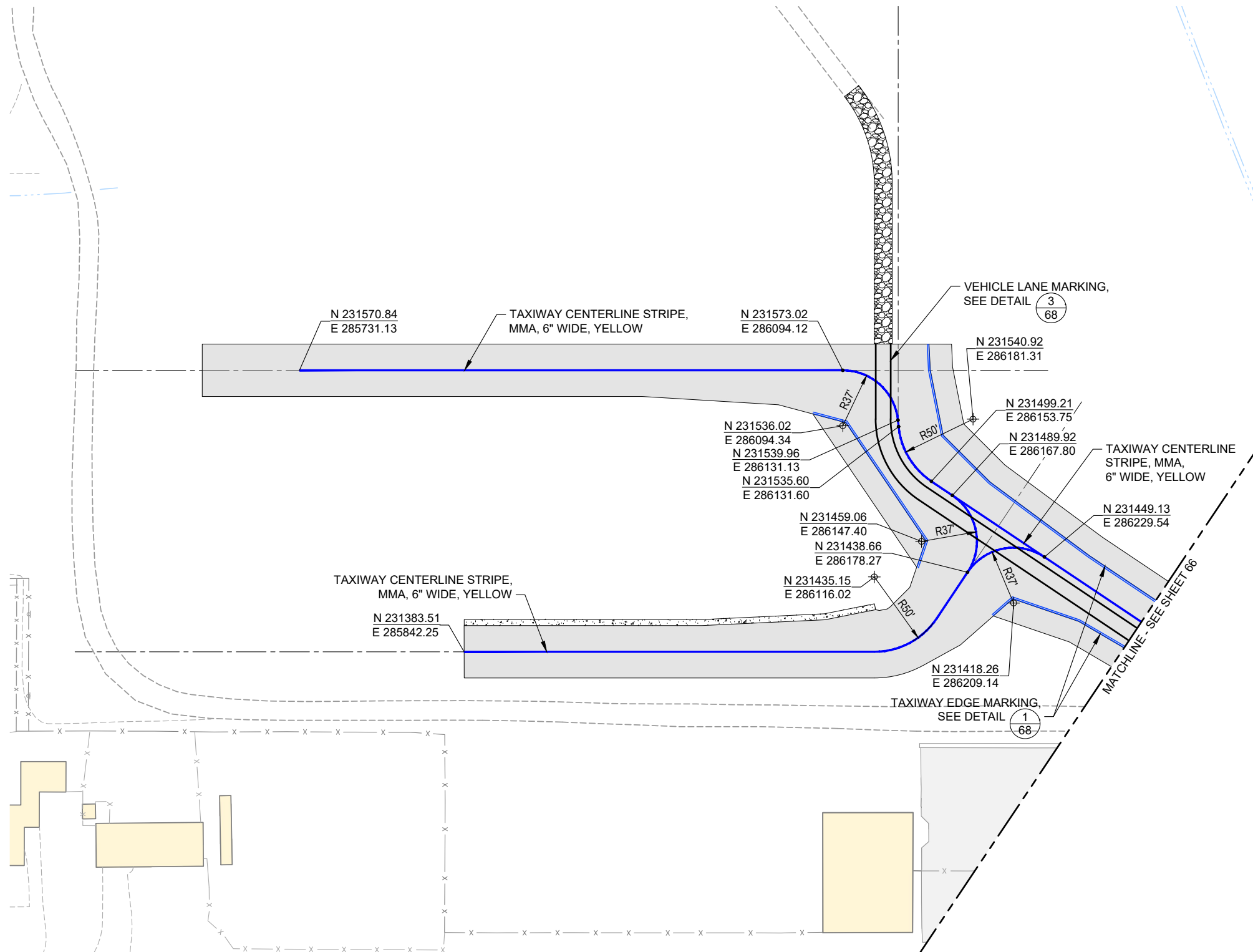
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1 REPLACE ENTIRE SHEET

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LEGEND



1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
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| 1 ADDENDUM NO. 1 | 5/24/2024 | JGL |
| REVISIONS: | DATE | APPD. |

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| DATE: | MAY 2024 |
| DESIGN: | KJK |
| DRAWN: | JAW |
| CHECKED: | JGL |
| REVISION NUMBER: | 0 |
| SCALE: | AS SHOWN |

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

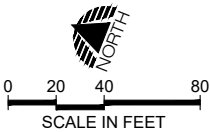
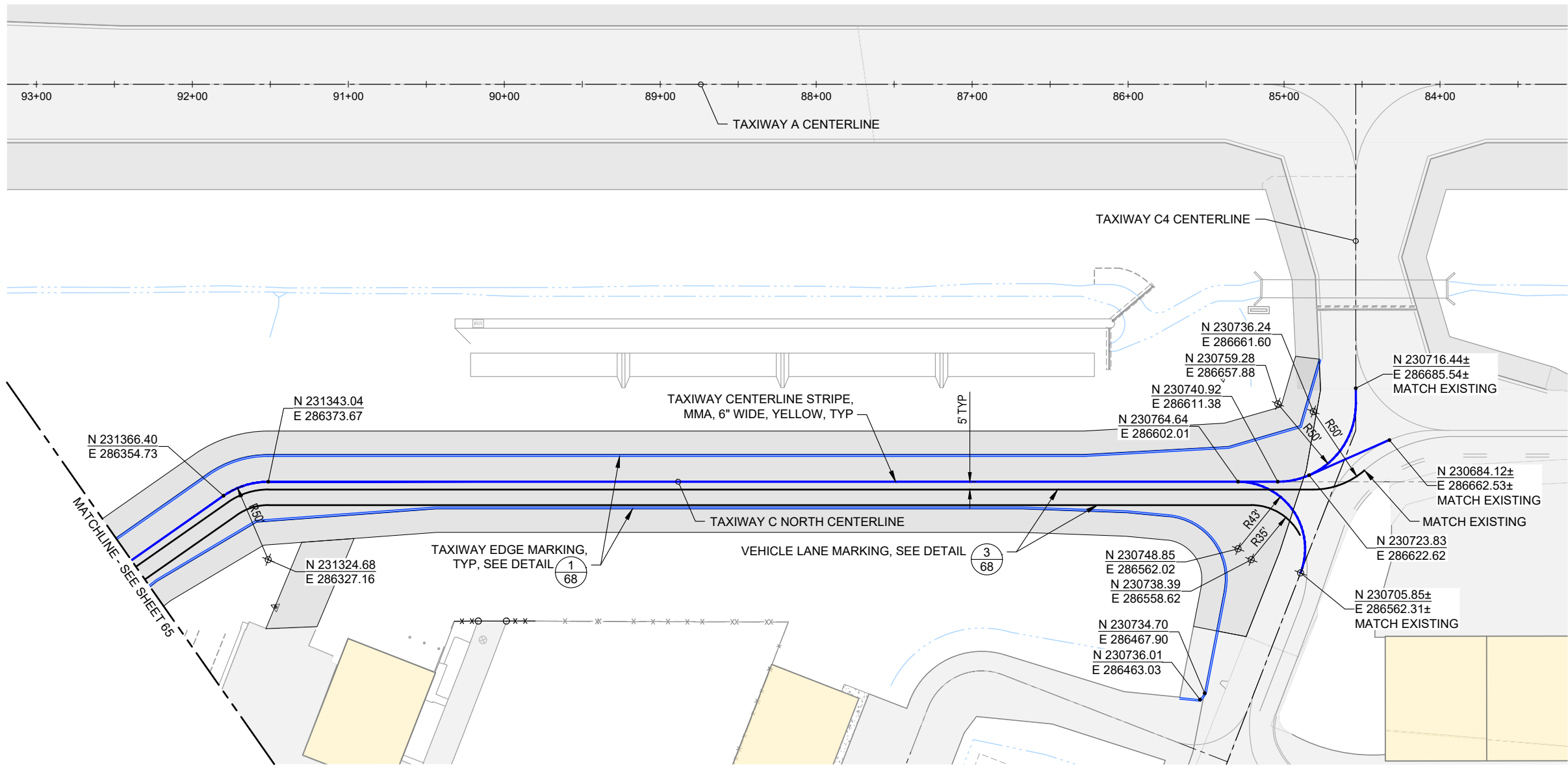
**PAVEMENT MARKING AND
RETROREFLECTIVE MARKER PLAN**

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
RVI-2020-W04

SHEET NO.
65
OF 71

05/24/24 - 2:18pm - BFortenberry - P:\RVI-2020-W04 NW Devel\0400CAD\DWG\Sheets\RVI-2020-W04_Marking.dwg



1 REPLACE ENTIRE SHEET

PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043



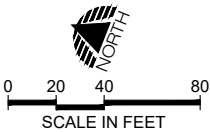
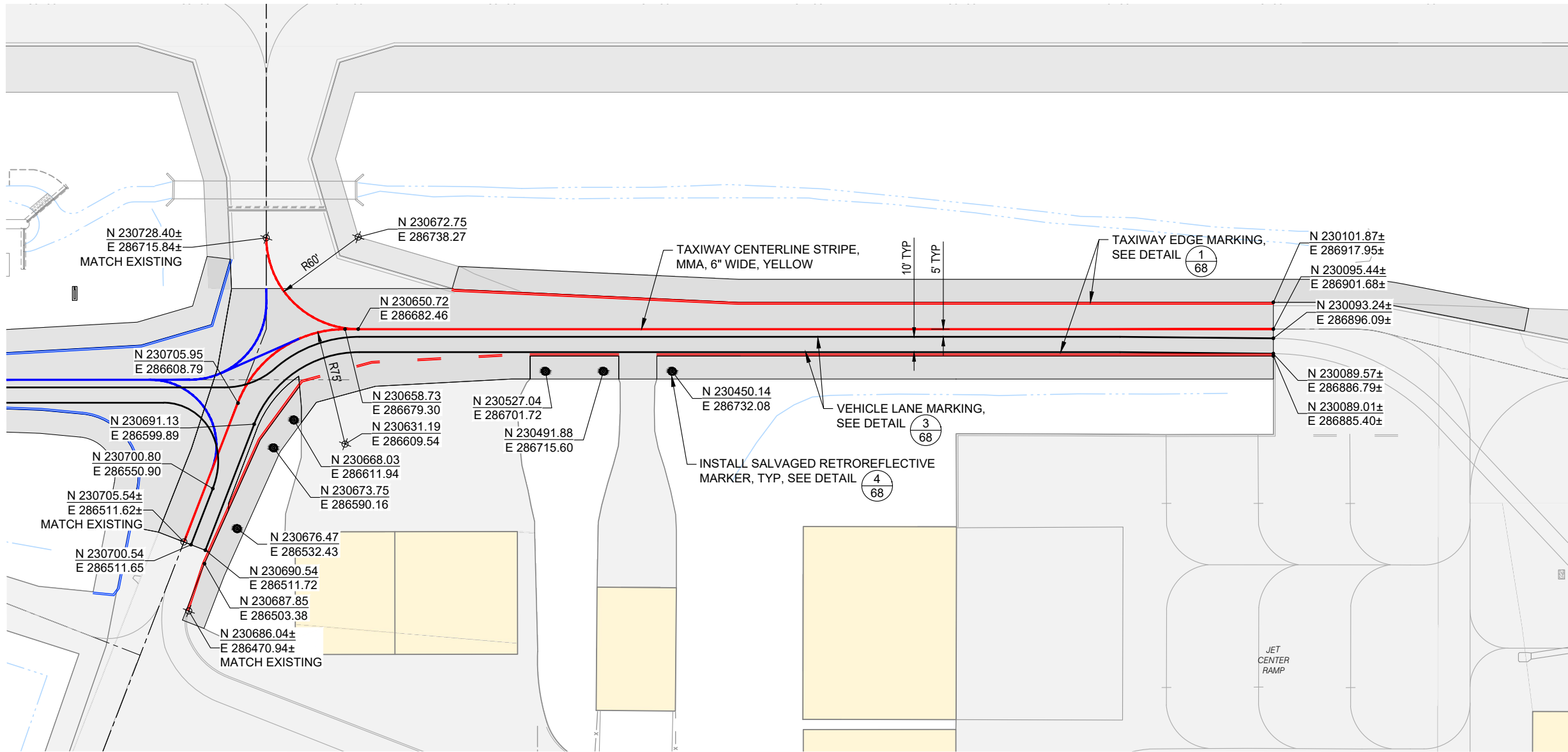
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REVISIONS: DATE APPD.

DATE: MAY 2024
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CHECKED: JGL
REVISION NUMBER: 0
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
**PAVEMENT MARKING AND
RETROREFLECTIVE MARKER PLAN**
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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1 REPLACE ENTIRE SHEET

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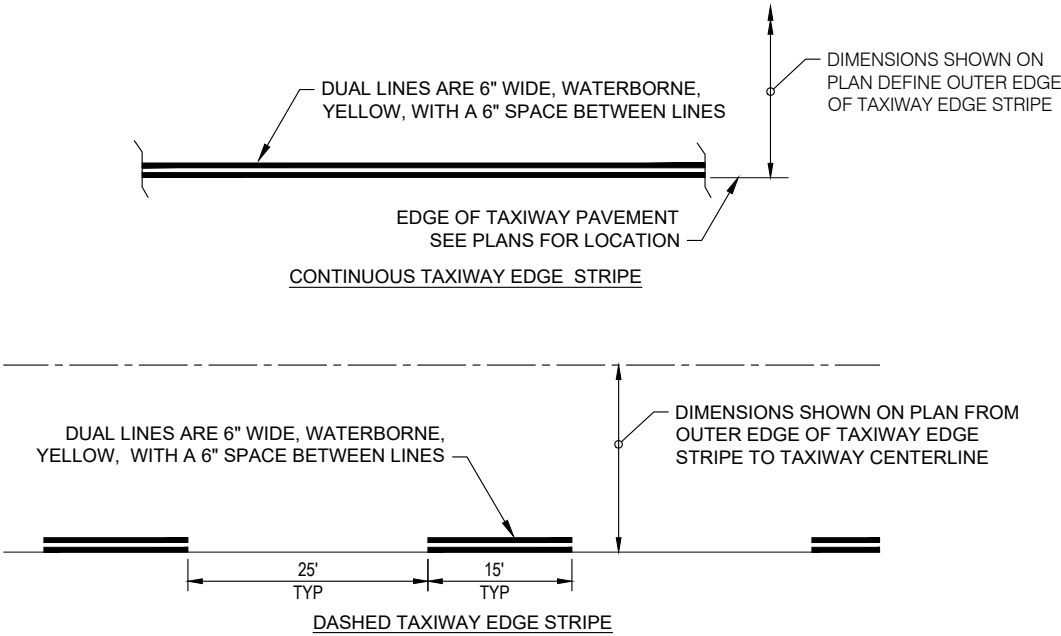
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| REVISIONS: | DATE | APPD. |

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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
**PAVEMENT MARKING AND
RETROREFLECTIVE MARKER PLAN**
- ADDITIVE
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

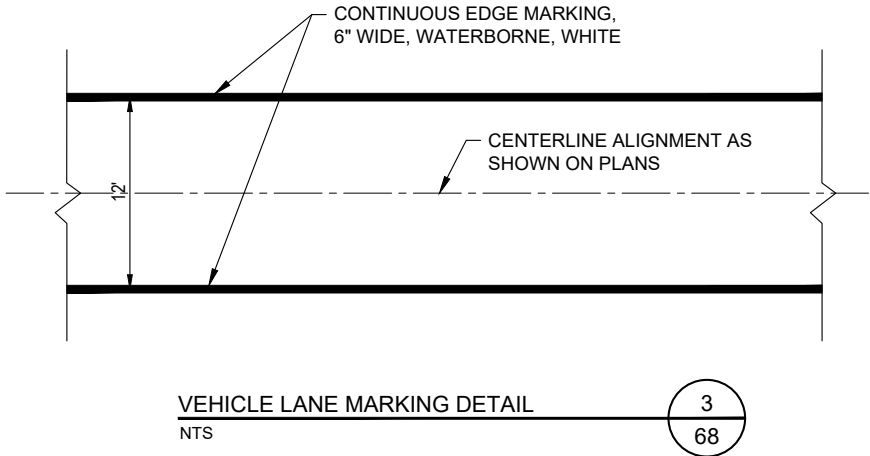
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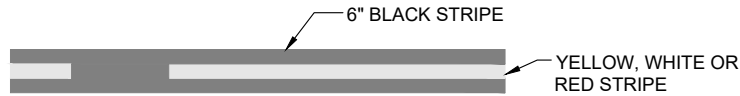
NOTE: SEE PLAN FOR TYPE
TAXIWAY EDGE MARKING DETAIL
NTS

1
68



VEHICLE LANE MARKING DETAIL
NTS

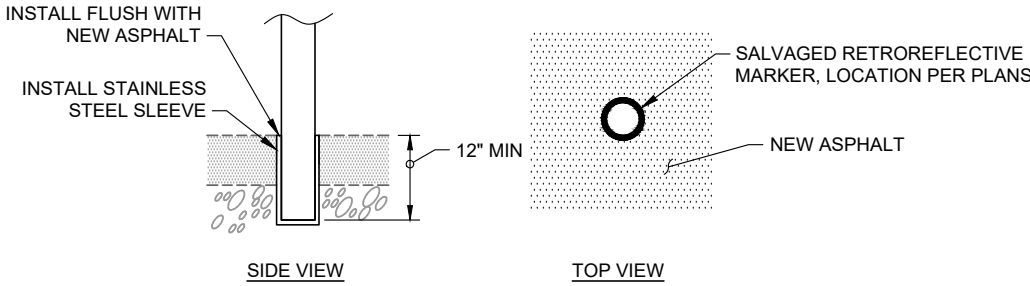
3
68



- NOTE:
1. ALL MARKING ON CONCRETE PAVEMENT SHALL HAVE A BLACK BORDER.
 2. BLACK BORDER MARKING SHALL MATCH MATERIAL TYPE OF STRIPE AS SHOWN ON PLANS.
 3. ALL BLACK MARKINGS SHALL BE WATERBORNE.

MARKINGS OUTLINED IN BLACK DETAIL
NTS

2
68



- NOTE:
1. CONTRACTOR SHALL CORE PAVEMENT TO INSTALL SLEEVE TO ALLOW FOR RETROREFLECTIVE MARKER REMOVAL.
 2. MARKER SHOULD FIT SNUGGLY WITHIN THE SLEEVE AND SHALL BE RIGID WHEN EXPOSED TO HIGH SPEED WINDS.
 3. IF SALVAGED RETROREFLECTIVE MARKERS ARE UNABLE TO MEET MINIMUM REQUIREMENTS, BRING TO THE ATTENTION OF THE RPR.
 4. ALL WORK TO REINSTALL THE RETROREFLECTIVE MARKERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE SALVAGED RETROREFLECTIVE MARKER PLACEMENT BID ITEM.

RETROREFLECTIVE MARKER ENLARGED PLAN VIEW DETAIL
NTS

4
68

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PRECISION APPROACH
ENGINEERING
5125 Southwest Hout Street
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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

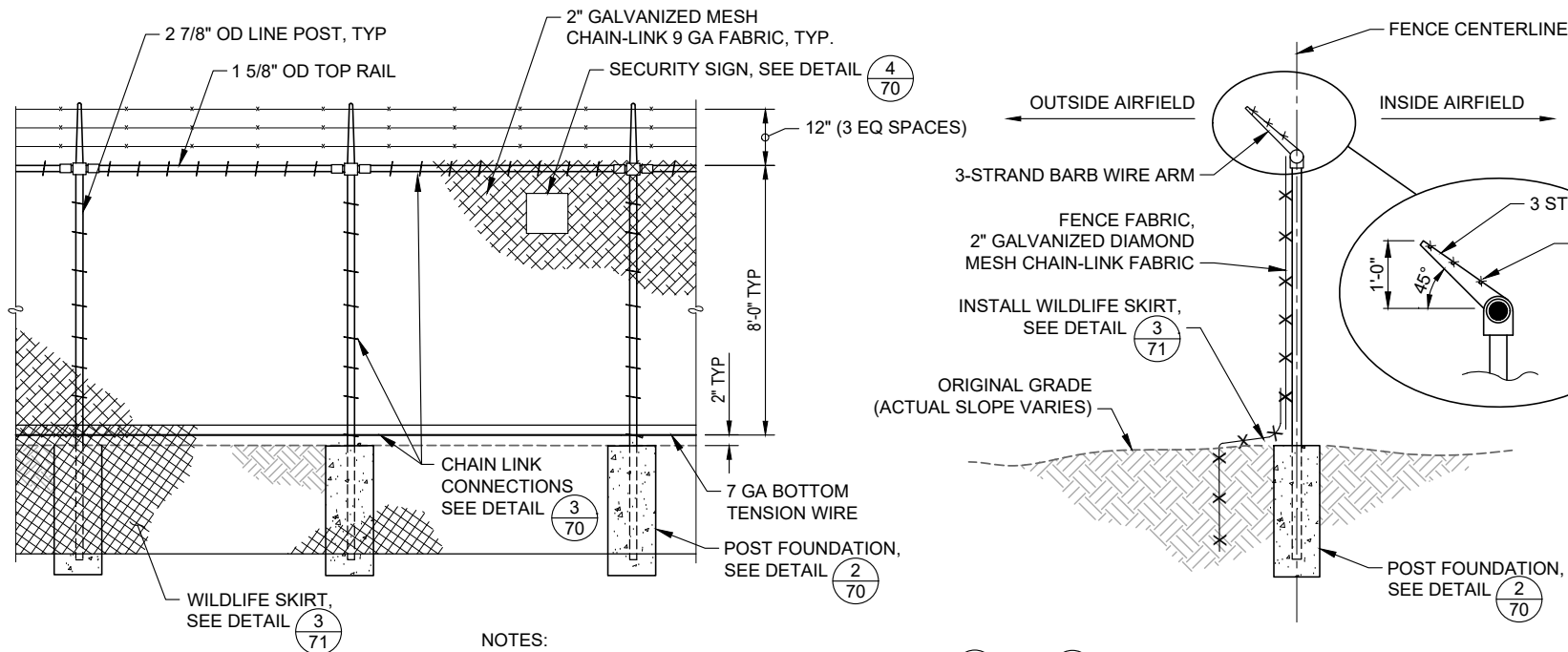
MARKING DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

PROJECT NUMBER:
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NOTES:

1. FOR CORNERS AND ANGLE POINTS, SEE DETAILS $\frac{2}{69}$ AND $\frac{4}{71}$.
2. H-BRACES TO BE PROVIDED AT ANGLE POINTS IN FENCE ALIGNMENT AND EITHER SIDE OF GATES.
3. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE -COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.
4. CONTRACTOR SHALL FIELD STAKE PROPOSED FENCE ALIGNMENT AND PROPERTY LINE PRIOR TO CONSTRUCTION. FENCE ALIGNMENT ADJUSTMENTS MAY BE REQUIRED, VERIFY WITH ENGINEER.

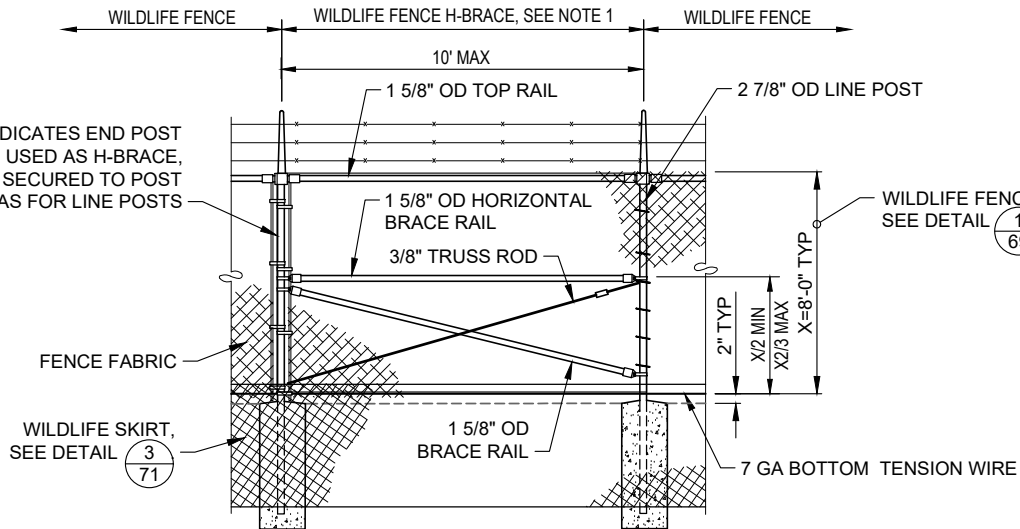
WILDLIFE FENCE DETAIL

NTS

1

69

DETAIL SHOWN INDICATES END POST CONDITION, WHEN USED AS H-BRACE, FABRIC SHALL BE SECURED TO POST SIMILAR AS FOR LINE POSTS



NOTES:

1. ALL BRACING, HARDWARE, AND INCIDENTALS NECESSARY TO INSTALL H-BRACE AS SHOWN AND REQUIRED SHALL BE PAID UNDER THE DOUBLE SWING GATE AND FENCE LINE LUMP SUM BID ITEM.
2. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE -COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.

WILDLIFE FENCE H-BRACE DETAIL

NTS

2

69

1 REPLACE ENTIRE SHEET

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5125 Southwest Hout Street
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REVISIONS:

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REVISION NUMBER: 0

SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

FENCE DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

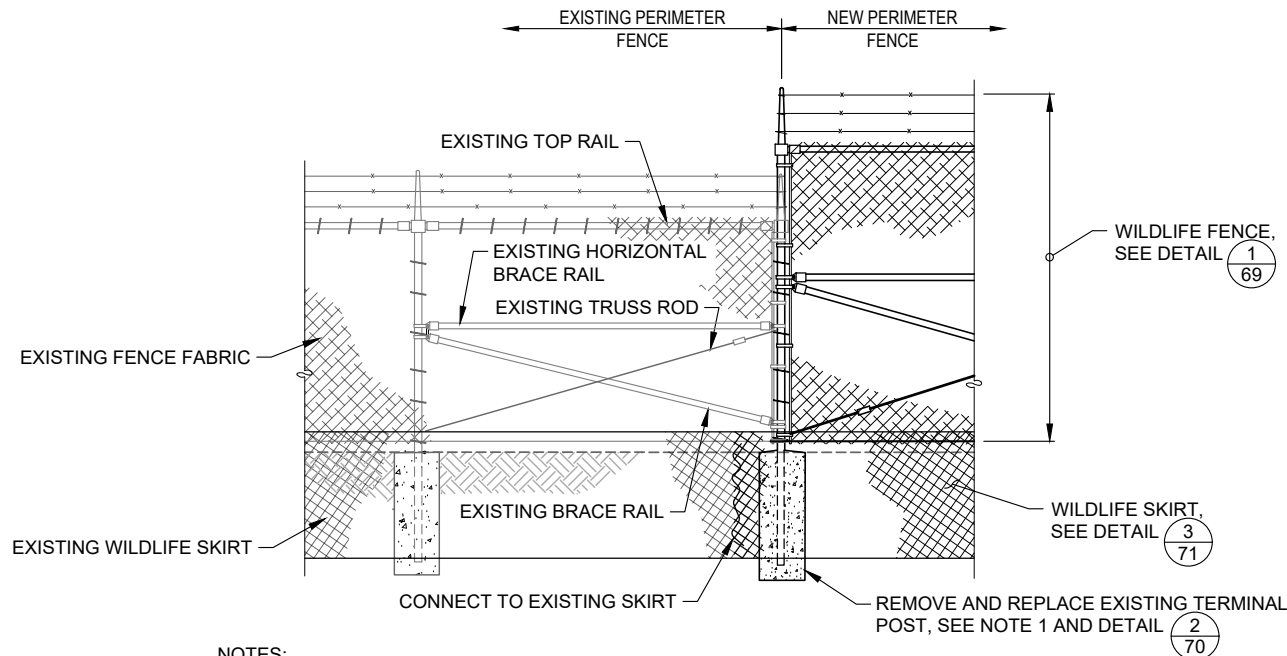
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OF 71

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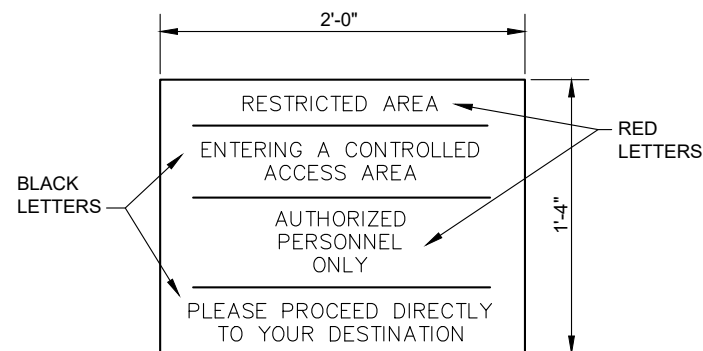
NOTES:

- CONTRACTOR SHALL DETACH EXISTING FENCE FABRIC, BARB WIRE, AND BRACING IN ORDER TO REMOVE AND REPLACE EXISTING TERMINAL POST. REMOVAL OF EXISTING POST AND FOUNDATION SHALL BE PAID UNDER THE FENCE REMOVAL BID ITEM. REATTACHMENT OF EXISTING FENCE MATERIAL SHALL BE PAID UNDER THE DOUBLE SWING GATE LUMP SUM BID ITEM.

CONNECTION TO EXISTING
FENCE DETAIL

NTS

1
70



NOTES:

- VERIFY COLOR SCHEME WITH ENGINEER.
- FASTEN SIGN SECURELY TO THE GATE, LOCATION TO BE DETERMINED BY THE ENGINEER.
- FOR SIGN ATTACHMENT, SEE DETAIL 6/70

AUTOMATED GATE SIGN DETAIL

NTS

4
70



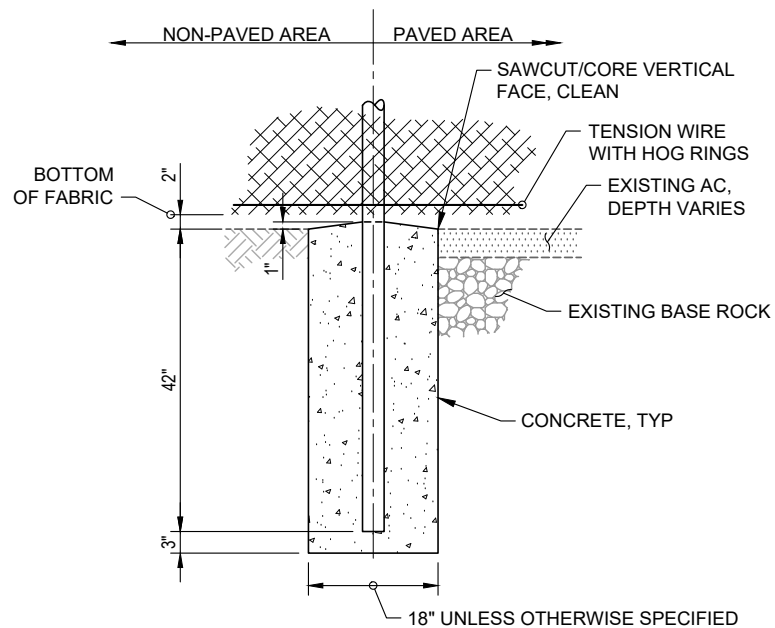
NOTES:

- ONE SIGN TO BE LOCATED ON EACH SIDE OF EVERY VEHICLE GATE.
- SEE ENGINEER FOR GATE NUMBERS.
- MATCH EXISTING SIGN SIZE AND COLORS.
- FOR SIGN ATTACHMENT, SEE DETAIL 6/70

GATE NUMERAL
SIGN DETAIL

NTS

5
70



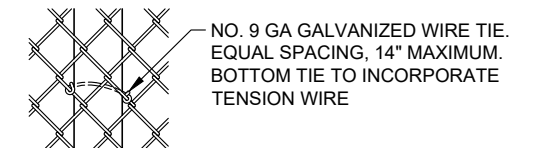
NOTES:

- A SONOTUBE MAY BE REQUIRED FOR CONCRETE FORMING IN UNEVEN TERRAIN CONDITIONS, INCIDENTAL.
- IN AREAS OF PAVEMENT, CONTRACTOR SHALL SAWCUT AND REMOVE EXISTING PAVEMENT PRIOR TO FOUNDATION EXCAVATION. FINISH CONCRETE FLUSH WITH AC SURFACE, INCIDENTAL.

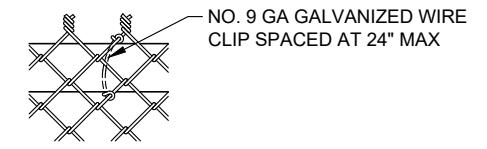
POST FOUNDATION DETAIL

NTS

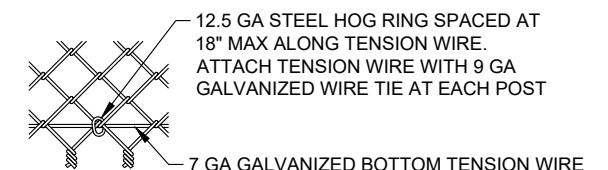
2
70



LINE POST CONNECTION



TOP RAIL CONNECTION

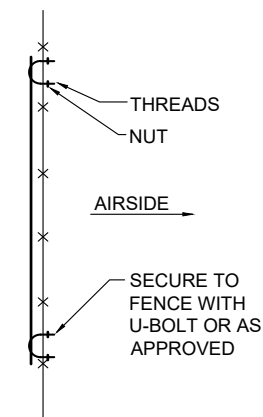


TENSION WIRE CONNECTION

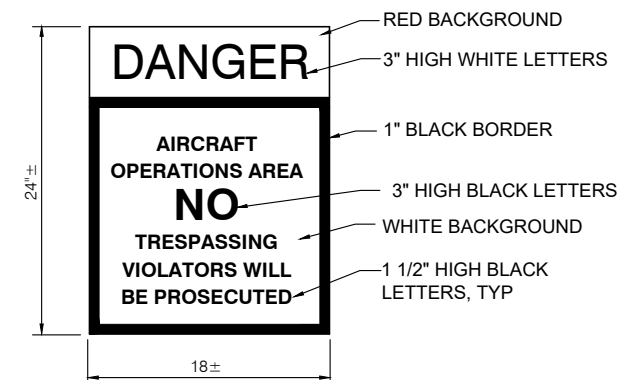
CONNECTION DETAIL

NTS

3
70



SIDE VIEW



FACE VIEW

NOTES:

- VERIFY SIGN DIMENSIONS WITH SIGN MANUFACTURER.
- SIGN TO BE PLACED ON ALL GATES AND AT A MAXIMUM OF 100' INTERVALS ALONG FENCE.
- INSTALLATION WITH VANDAL PROOF HARDWARE PER MANUFACTURERS RECOMMENDATIONS, AS APPROVED BY THE ENGINEER.

FENCE SIGN DETAIL

NTS

6
70

1 REPLACE ENTIRE SHEET



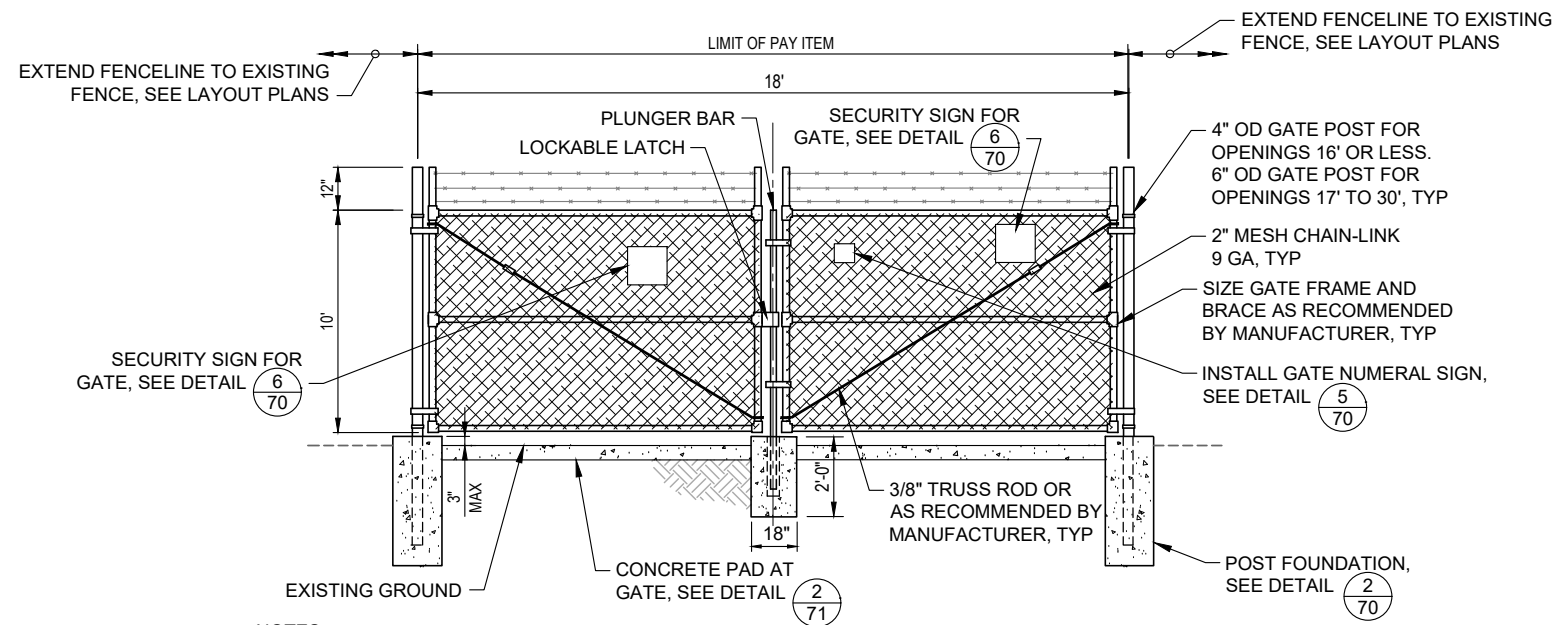
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| CHECKED: | JGL |
| REVISION NUMBER: | 0 |
| SCALE: | AS SHOWN |

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| ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES |
| FENCE DETAILS |
| PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063 |

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| PROJECT NUMBER: RVI-2020-W04 |
| SHEET NO. 70 OF 71 |

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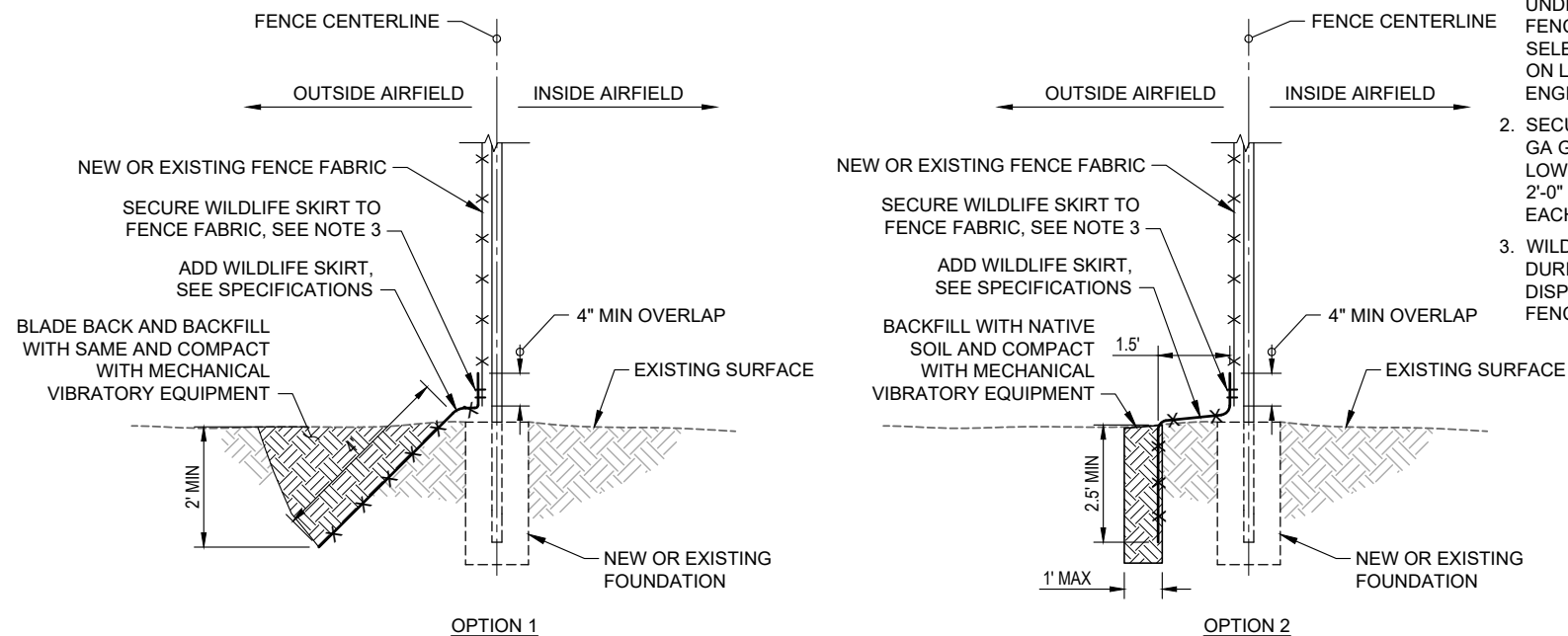
NOTES:

1. HEAVY DUTY LOCK MANUFACTURED BY BEST LOCK WITH 3 KEYS SHALL BE PROVIDED FOR EACH GATE INSTALLED, INCIDENTAL TO BID ITEM. PROVIDE A BEST LOCK 7 PIN CORE SERIES, COORDINATE CORE SERIES WITH OWNER PRIOR TO ORDERING. PROVIDE H- BRACE SECTION EITHER SIDE OF GATE, SEE DETAIL 2/69
2. CONTRACTOR SHALL GRADE A SMOOTH TRANSITION COMPATIBLE WITH THE GATE ENTRANCE FOR THE COMPLETE SWING ARC IN THE OPENING DIRECTION IN NONPAVED AREAS. ADDITIONAL MATERIALS AND WORK SHALL BE PAID FOR UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM.
3. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE -COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.

SWING GATE DETAIL

NTS

1/71



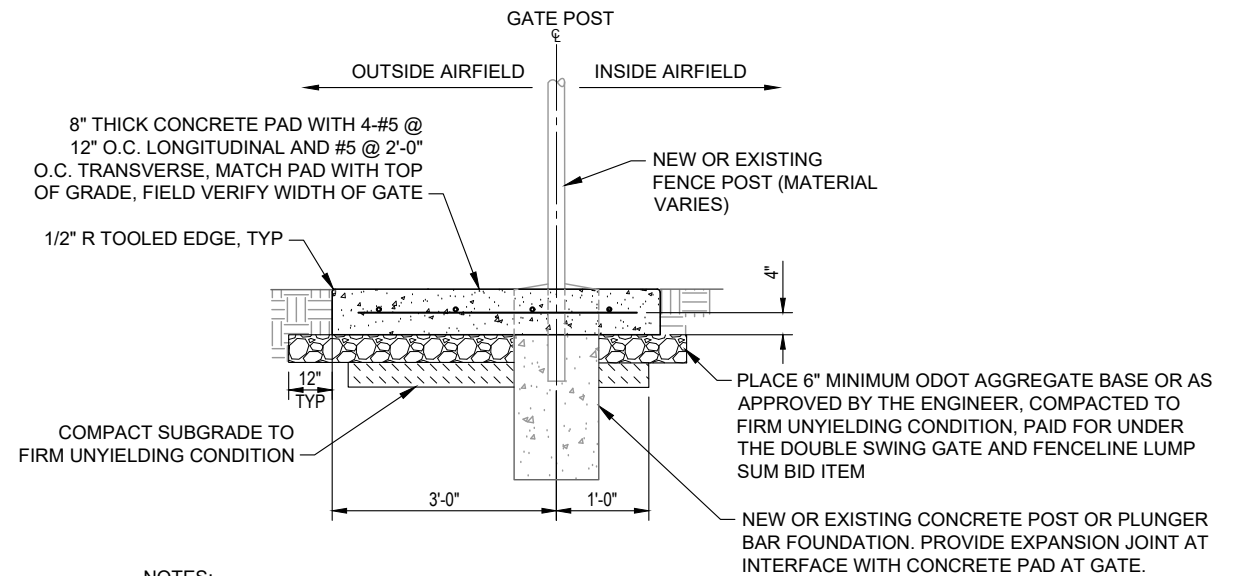
WILDLIFE SKIRT INSTALLATION DETAIL

NTS

3/71

NOTES:

1. OPTION 1 AND 2 INSTALLATION TO BE PAID UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM. OPTION SELECTION SHALL BE DETERMINED BASED ON LOCAL SITE CONDITIONS, VERIFY WITH ENGINEER PRIOR TO CONSTRUCTION.
2. SECURE WILDLIFE SKIRT TO FENCE USING 9 GA GALVANIZED STEEL HOG RINGS AT LOWEST FENCE POINT. ATTACH FABRIC 2'-0" O.C. ALONG FENCE AND WITHIN 2" EACH SIDE OF POSTS.
3. WILDLIFE SKIRT FABRIC SHALL BE REMOVED DURING FENCE REMOVAL OPERATIONS AND DISPOSED OF OFFSITE, PAID UNDER THE FENCE REMOVAL BID ITEM.



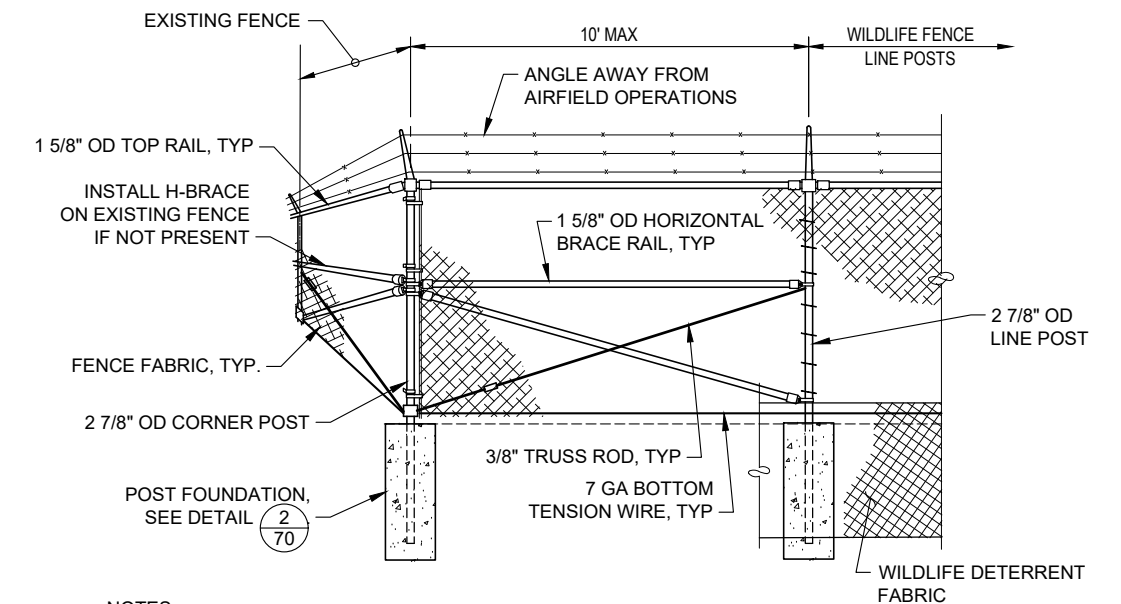
NOTES:

1. GATE CONCRETE PAD TO BE INSTALLED ON ALL NEW GATES. ALL WORK, MATERIALS AND INCIDENTALS SHALL BE PAID FOR UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM.

CONCRETE PAD AT GATE DETAIL

NTS

2/71



NOTES:

1. ALL POSTS, BRACING, HARDWARE, CHAIN-LINK FABRIC, CONCRETE, AND INCIDENTALS NECESSARY TO PROVIDE ITEM AS REQUIRED SHALL BE PAID FOR UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM.
2. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE -COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.

WILDLIFE FENCE CORNER DETAIL

NTS - SOME ITEMS NOT SHOWN FOR CLARITY

4/71

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ENGINEERING
5125 Southwest Hout Street
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541•754•0043



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ROGUE VALLEY INTERNATIONAL - MEDFORD
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

GATE DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0037-063

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