

## **ADDENDUM NO. 1**

Date: May 24, 2024

To the Contract Documents for the construction of  
**TAXIWAY C EXTENSION AND NORTHWEST TAXILANES**  
**ROGUE VALLEY INTERNATIONAL – MEDFORD AIRPORT**  
**AIP NO. 3-41-0037-063-2024; CONTRACT NO: AIP-063-FY24**

### **TO ALL PLANHOLDERS AND/OR PROSPECTIVE BIDDERS:**

The following changes, additions, and/or deletions are hereby made a part of the Contract Documents for the construction of **“TAXIWAY C EXTENSION AND NORTHWEST TAXILANES”** dated 2024, as fully and completely as if the same were fully set forth therein.

### **DRAWINGS**

- Sheet No. 1 – 71
  - Replace entire sheet set with the revised Sheet No. 1 – 71, labeled “**Addendum No. 1**”

### **GENERAL INFORMATION**

All Bidders shall acknowledge receipt and acceptance of this Addendum No. 1 in the Proposal, or by submitting the Addendum with the bid package. Proposals submitted without Addendum acknowledgement, or without this Addendum, will be considered nonresponsive.

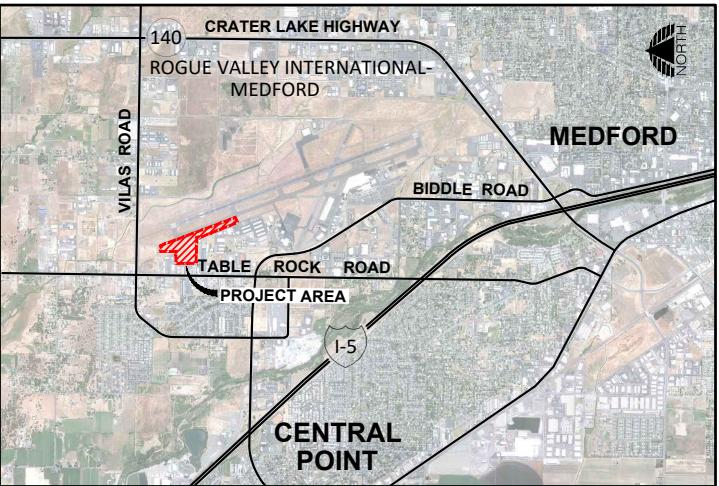


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Josh Lekkerkerker, PE  
Precision Approach Engineering, Inc.

# ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

AIP NO. 3-41-0037-063

## SITE MAP



## VICINITY MAP



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## OWNER



JACKSON COUNTY - AIRPORT AUTHORITY  
ROGUE VALLEY INTERNATIONAL - MEDFORD AIRPORT  
1000 TERMINAL LOOP PARKWAY  
MEDFORD, OREGON 97504

1 REPLACE ENTIRE SHEET

PRECISION APPROACH  
ENGINEERING  
5125 Southwest Hout Street  
Corvallis, OR 97333  
541•754•0043



ADDENDUM NO. 1

5/24/2024 JGL  
REVISIONS: DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0

SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

## COVER SHEET

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
RVI-2020-W04  
SHEET NO.  
1  
OF 71

## LEGEND

### CIVIL SYMBOLS LEGEND

EXISTING	NEW	DESCRIPTION
— — — — —	— — — — —	EDGE OF PAVEMENT
— — — — —	— — — — —	EDGE OF GRAVEL
— 101.8 — — — —	— 101 — — — —	CONTOUR
— · · · · — — — —	— · · · · — — — —	DITCH FLOW LINE
— x — x — — — —	— x — x — — — —	FENCE
— SD — SD — — — —	— SD — SD — — — —	STORM DRAIN
— UD — — — — —	— UD — — — — —	UNDER DRAIN
— > — > — — — —	— > — > — — — —	CULVERT
— SS — SS — — — —	— SS — SS — — — —	SANITARY SEWER
— W — W — — — —	— W — W — — — —	WATER
— G — — — — —	— G — — — — —	GAS
— T — — — — —	— T — — — — —	TELEPHONE
— IRR — — — — —	— IRR — — — — —	IRRIGATION
— T — — — — —	— T — — — — —	SLOPE SYMBOL
— SD — — — — —	— SD — — — — —	CATCH BASIN
— S — — — — —	— S — — — — —	STORM DRAIN MANHOLE
— O — — — — —	— O — — — — —	CLEANOUT
— C — — — — —	— C — — — — —	CULVERT END STRUCTURE
— ~ — — — — —	— ~ — — — — —	SURFACE FLOW DIRECTION
— SS — — — — —	— SS — — — — —	SANITARY MANHOLE
— ♀ — — — — —	— ♀ — — — — —	FIRE HYDRANT
— W — — — — —	— W — — — — —	WATER METER
— W — — — — —	— W — — — — —	WATER VAULT
— X — — — — —	— X — — — — —	WATER VALVE
— X — — — — —	— X — — — — —	IRRIGATION VALVE
— O — — — — —	— O — — — — —	TIE DOWN
— P — — — — —	— P — — — — —	SIGN
— A — — — — —	— A — — — — —	SURVEY CONTROL POINT
• — — — — —	• — — — — —	SURVEY FOUND MONUMENT
— B — — — — —	— B — — — — —	BRASS CAP
— B — — — — —	— B — — — — —	BENCHMARK
— A — — — — —	— A — — — — —	AIRPORT BEACON
— S — — — — —	— S — — — — —	SHRUBS
— T — — — — —	— T — — — — —	TREES
— A — — — — —	— A — — — — —	ABANDONED FEATURE LINE
— D — — — — —	— D — — — — —	DEMO FEATURE LINE
— D — — — — —	— D — — — — —	DEMO FEATURE LINE
— SIDA — — — —	— SIDA — — — —	SECURITY IDENTIFICATION DISPLAY AREA (SIDA) LINE

### ELECTRICAL LEGEND

EXISTING	NEW	DESCRIPTION
HH	HH	HANDHOLE
P	P	POWER VAULT
DM	DM	DUCT MARKER
J	J	JUNCTION CAN
E	E	ELECTRICAL RISER
★	★	RUNWAY EDGE LIGHT AND THRESHOLD LIGHT C = CLEAR R = RED G = GREEN Y = YELLOW O = OPAQUE
★	★	RUNWAY LIGHT IN PAVEMENT (FLUSH MOUNT)
★	★	REIL (RUNWAY END IDENTIFIER LIGHT)
★	★	PAPI (PRECISION APPROACH PATH INDICATOR)
★	★	TAXIWAY LIGHT (BLUE LENS)
★	★	TAXIWAY LIGHT IN PAVEMENT (FLUSH MOUNT)
★	★	RETROREFLECTIVE MARKER
POH	POH	POWER CIRCUIT / OVERHEAD
PUG	P	POWER UNDERGROUND
○ — ○	○ — ○	POWER POLE
— — — — —	— — — — —	GUY WIRE
○ — ○	○ — ○	STREET LIGHT
— — — — —	— — — — —	GUIDANCE SIGN

### AVIATION LEGEND

EXISTING	FUTURE	DESCRIPTION
— — OFA — — —	— — OFA(F) — — —	OBJECT FREE AREA (OFA)
— — OFZ — — —	— — OFZ(F) — — —	OBJECT FREE ZONE (OFZ)
— — OFZ-OFA — —	— — OFZ-OFA(F) — —	OFZ-OFA
— — ROFA — — —	— — ROFA(F) — — —	RUNWAY OBJECT FREE AREA
— — ROFZ — — —	— — ROFZ(F) — — —	RUNWAY OBJECT FREE ZONE
— — RPZ — — —	— — RPZ(F) — — —	RUNWAY PROTECTION ZONE
— — RSA — — —	— — RSA(F) — — —	RUNWAY SAFETY AREA
— — TSA — — —	— — TSA(F) — — —	TAXIWAY SAFETY AREA
— — TLOFA — — —	— — TLOFA(F) — — —	TAXILANE OBJECT FREE AREA
— — TOFA — — —	— — TOFA(F) — — —	TAXIWAY OBJECT FREE AREA

## GENERAL NOTES

### CONSTRUCTION NOTES:

- CONTRACTOR SHALL SUBMIT SCHEDULE AND SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO OWNER IN ACCORDANCE WITH THE SPECIFICATIONS. WORK SHALL NOT START UNTIL ACCEPTANCE OF THE SCHEDULE, SPCD, AND ISSUANCE OF "NOTICE-TO-PROCEED" (NTP) BY THE OWNER.
- DAMAGE TO HAUL ROUTES AND EXISTING FACILITIES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL COMPLETE THE WORK WITHIN THE NUMBER OF CALENDAR DAYS FROM THE NTP AS SPECIFIED IN THE BIDDER'S PROPOSAL.
- THE CONTRACTOR SHALL OBTAIN PERMITS AND OWNER SHALL PAY PERMIT FEES ASSOCIATED WITH IMPROVEMENTS.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY ALL UTILITY COMPANIES AND AIRPORT OPERATIONS TO COORDINATE UTILITY LOCATES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL UTILITIES AND REPLACING UTILITIES DAMAGED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SURVEY WORK REQUIRED FOR THE CONSTRUCTION OF THE PROJECT AND TO ENSURE CONSTANT GRADE ACROSS IMPROVEMENTS TO MAINTAIN DRAINAGE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO WORK IMPROVEMENTS WHEN IN DOUBT ABOUT GRADES AND DRAINAGE. SEE GENERAL REQUIREMENTS.
- CONTRACTOR SHALL BE RESPONSIBLE TO FIELD VERIFY CONDUIT AND CABLE ROUTING PRIOR TO INSTALLATION. TRACING OF EXISTING LINES MAY BE REQUIRED.
- CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN AND CLEAN STREETS AND PAVEMENT OPEN TO TRAFFIC, KEEP FREE OF SOIL OR OTHER DEBRIS FROM CONSTRUCTION, AND REPAIR ALL DAMAGE TO STREETS AND OTHER FACILITIES RESULTING FROM CONSTRUCTION. CLEANING AND REPAIR SHALL BE MADE IN A TIMELY MANNER AS DETERMINED BY THE ENGINEER.
- CONTRACTOR SHALL RECORD ALL CONSTRUCTION CHANGES WHICH DEVIATE FROM THE PLANS AND PROVIDE THE OWNER WITH A CLEARLY MARKED SET OF RECORD DRAWINGS AT THE COMPLETION OF THE PROJECT.

### CONSTRUCTION FUNDING:

- EXECUTION OF ALL WORK IN THIS CONTRACT IS CONTINGENT UPON JACKSON COUNTY'S RECEIPT OF FEDERAL FUNDS. THESE FUNDS WILL NOT BE ASSURED UNTIL A GRANT IS EXECUTED BETWEEN THE FAA AND JACKSON COUNTY AFTER THE CONTRACT BID OPENING. GRANT EXECUTION IS EXPECTED TO OCCUR IN THE SUMMER/FALL OF 2024. OWNER SHALL HAVE THE FINAL AUTHORITY REGARDING START DATE OF CONSTRUCTION.
- ELEMENTS OF WORK ARE AIRPORT IMPROVEMENT PROGRAM (AIP) ELIGIBLE UNLESS OTHERWISE NOTED.

### CONSTRUCTION SCHEDULE:

- THE CONTRACTOR SHALL PREPARE AND SUBMIT A "CONSTRUCTION SCHEDULE" TO THE ENGINEER AND OWNER FOR REVIEW A MINIMUM OF 14 DAYS PRIOR TO STARTING CONSTRUCTION OR PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, WHICHEVER OCCURS EARLIER. THE PLAN MUST BE ACCEPTABLE TO THE OWNER PRIOR TO BEGINNING WORK.
- THE CONSTRUCTION SCHEDULE SHALL BE REVISED ANY TIME AN ELEMENT OF WORK DOES NOT COINCIDE WITH THE INITIAL TIME LINE PRESENTED, OR IF REQUESTED BY THE ENGINEER OR OWNER.
- ALL MATERIAL SUBMITTALS SHALL BE SUBMITTED, REVIEWED AND RETURNED WITH "NO EXCEPTIONS NOTED" PRIOR TO USE IN CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE REQUIRED INFORMATION WITH APPROPRIATE TIME PRIOR TO THE DESIRED USE.

### WEEKLY CONSTRUCTION MEETING:

- THE CONTRACTOR, ENGINEER, OWNER, OWNER'S REPRESENTATIVE, AND OTHER INVITED REPRESENTATIVES OR GUESTS WILL CONDUCT A WEEKLY CONSTRUCTION MEETING TO DISCUSS PROJECT ACTIVITIES. THE MEETING SHALL BE USED TO DISCUSS CONSTRUCTION ISSUES, PROJECT SCHEDULE, AND AIRCRAFT OPERATIONS DURING CONSTRUCTION AND OTHER AGENDA ITEMS RELATED TO THE PROJECT. SPECIFIC ACTIVITIES ANTICIPATED FOR THE FOLLOWING WEEK SHALL BE DISCUSSED, PARTICULARLY THOSE IMPACTING AIRPORT OPERATIONS AND SAFETY. A TWO-WEEK LOOK AHEAD SHALL ALSO BE PROVIDED.

### CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICER:

- THE CONTRACTOR SHALL HAVE A FULL TIME SUPERINTENDENT ON THE PROJECT SITE ANY TIME WORK IS BEING ACCOMPLISHED ON THE AIRPORT. THIS INCLUDES WORK BEING PERFORMED BY SUBCONTRACTORS OR SERVICE PROVIDERS, UNLESS OTHERWISE APPROVED BY THE OWNER.
- THE CONTRACTOR SHALL ASSIGN ONE PERSON TO BE THE SAFETY OFFICER DURING THE PROJECT. THE SAFETY OFFICER SHALL BE ONSITE AT ALL TIMES WHEN WORK IS OCCURRING IN AN ACTIVE AOA UNLESS OTHERWISE APPROVED BY THE OWNER. THE SAFETY OFFICER SHALL BE RESPONSIBLE FOR THE EDUCATION AND TRAINING OF ALL PERSONNEL WHO WILL ACCESS AIRPORT PROPERTY. THE OFFICER SHALL MAINTAIN RECORDS INDICATING THAT PERSONNEL HAVE BEEN TRAINED, AND ARE FAMILIAR WITH SAFETY RULES AND REGULATIONS RELATED TO OPERATIONS AT THE AIRPORT DURING CONSTRUCTION. PERSONNEL WHO VIOLATE SAFETY REQUIREMENTS MAY BE REMOVED FROM THE PROJECT AT THE DISCRETION OF THE OWNER. THE SAFETY OFFICER SHALL ALSO BE RESPONSIBLE FOR FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT. SEE CONSTRUCTION SAFETY AND PHASING PLAN FOR ADDITIONAL REQUIREMENTS.
- THE CONTRACTOR'S SUPERINTENDENT AND SAFETY OFFICER SHALL ATTEND THE WEEKLY CONSTRUCTION MEETING.

1 REPLACE ENTIRE SHEET

DATE: MAY 2024	DESIGN: KJK	ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES	
DRAWN: JAW	CHECKED: JGL	LEGEND AND GENERAL NOTES	
REVISION NUMBER: 0	SCALE: AS SHOWN	PRECISION APPROACH ENGINEERING, INC.	
REVISIONS: 5/24/2024 JGL		AIP NO. 3-41-0037-063	
REVISIONS: DATE APPD.		OF 71	



## BASE BID SCHEDULE

BID ITEM	SPECIFICATION SECTION	DESCRIPTION	UNIT	BID QUANTITY	CONSTRUCTION QUANTITY
1	E-020	CONSTRUCTION SURVEY AND STAKING	LS	1	
2	E-020	SECURITY BADGE	EA	30	
3	C-100	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	LS	1	
4	C-102	INSTALLATION AND REMOVAL OF SILT FENCE	LF	1,680	
5	C-102	FIBER MATTING	SY	2,680	
6	C-102	TEMPORARY EROSION CONTROL	LS	1	
7	C-105	MOBILIZATION	LS	1	
8	E-110	CABLE REMOVAL	LF	340	
9	E-110	CLEANOUT REMOVAL	EA	1	
10	E-110	FENCE REMOVAL	LF	52	
11	E-110	POROUS PAVER REMOVAL	SY	160	
12	E-110	REMOVAL OF EXISTING EDGE LIGHT AND BASE CAN	EA	2	
13	E-110	REMOVAL OF EXISTING EDGE LIGHT FIXTURE	EA	1	
14	E-110	REMOVAL OF EXISTING GUIDANCE SIGN AND PANELS	EA	1	
15	E-110	REMOVAL OF EXISTING GUIDANCE SIGN PANEL	EA	3	
16	E-110	REMOVAL OF PIPES	LF	170	
17	E-110	RETROREFLECTIVE MARKER REMOVAL	EA	1	
18	E-110	TRASH RACK REMOVAL	EA	2	
19	E-180	DRAINAGE GEOTEXTILE FABRIC	SY	13,670	
20	E-180	RIPRAP GEOTEXTILE FABRIC	SY	340	
21	E-180	SUBGRADE GEOTEXTILE FABRIC	SY	13,670	
22	ODOT 00390	CLASS 50 RIPRAP PROTECTION	SY	10	
23	ODOT 00390	CLASS 100 RIPRAP PROTECTION	SY	340	
24	ODOT 00390	QUARRY SPALL PROTECTION	SY	10	
25	P-101	ASPHALT PAVEMENT REMOVAL, FULL DEPTH	SY	1,040	
26	P-101	REMOVAL OF FOREIGN SUBSTANCES/CONTAMINATES	SF	300	
27	P-152	MUCK EXCAVATION	CY	4,000	
28	P-152	STRUCTURAL FILL	CY	4,000	
29	P-152	UNCLASSIFIED EXCAVATION	CY	10,880	
30	P-209	CRUSHED AGGREGATE BASE COURSE	CY	2,240	
31	P-401	ASPHALT SURFACE COURSE	TON	3,330	
32	P-407	ASPHALT TREATED PERMEABLE BASE COURSE	TON	3,990	
33	P-603	EMULSIFIED ASPHALT TACK COAT	TON	3	
34	P-620	METHACRYLYTE MARKING, YELLOW, TWO COAT	SF	1,100	
35	P-620	WATERBORNE MARKING, BLACK, ONE COAT	SF	250	
36	P-620	WATERBORNE MARKING, YELLOW/WHITE, TWO COAT	SF	3,320	
37	F-162	DOUBLE SWING GATE AND FENCELINE	LS	1	
38	D-701	6-INCH HDPE PIPE	LF	30	
39	D-701	12-INCH HDPE PIPE	LF	200	
40	D-701	12-INCH REINFORCED CONCRETE PIPE	LF	210	
41	D-701	18-INCH HDPE PIPE	LF	330	
42	D-701	PIPE CONNECTION TO EXISTING STRUCTURE	EA	1	
43	D-705	6-INCH PERFORATED HDPE PIPE, NEW PAVEMENT AREA	LF	310	
44	D-705	6-INCH PERFORATED HDPE PIPE, NON-PAVED AREA	LF	1,600	
45	D-705	UNDERDRAIN CONNECTION TO PIPE	EA	3	
46	D-751	ADJUSTABLE POND INLET SYSTEM	EA	1	
47	D-751	CATCH BASIN	EA	4	
48	D-751	CLEANOUT	EA	13	
49	D-751	CONCRETE DRAINAGE COLLAR	EA	1	
50	D-751	CONCRETE PIPE SLOPED END	EA	4	
51	D-751	CONTROL STRUCTURE TRASH RACK	EA	2	
52	D-751	DRAINAGE INLET/OUTLET STRUCTURE	EA	2	
53	D-751	DRAINAGE STRUCTURE ADJUSTMENT	EA	2	
54	D-751	SOUTH TREATMENT LOW FLOW BYPASS SYSTEM	LS	1	
55	D-754	CONCRETE PYLONS	EA	18	
56	D-754	CONCRETE FLUME	LF	90	
57	D-754	CONCRETE POND VALLEY GUTTER EXTENSION	SF	2,400	
58	D-754	CONCRETE VALLEY GUTTER	SF	1,100	
59	D-754	NORTH TREATMENT BYPASS CONTROL STRUCTURE	LS	1	
60	D-754	NORTH TREATMENT FLOW CONTROL STRUCTURE	LS	1	
61	D-754	POND BYPASS FLOW CONTROL STRUCTURE MODIFICATIONS	LS	1	
62	D-754	SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS	LS	1	
63	T-901	SEEDING	AC	9	
64	E-920	WATER QUALITY HIGH FLOW MEDIA SYSTEM	SF	1,320	
65	T-905	TOPSOIL	CY	5,870	
66	L-108	NO. 8 AWG CABLE INSTALLED IN CONDUIT, BLACK	LF	150	
67	L-108	NO. 8 AWG CABLE INSTALLED IN CONDUIT, TERRA COTTA	LF	2,730	
68	L-108	NO. 8 AWG CABLE INSTALLED IN CONDUIT, GRAY	LF	140	

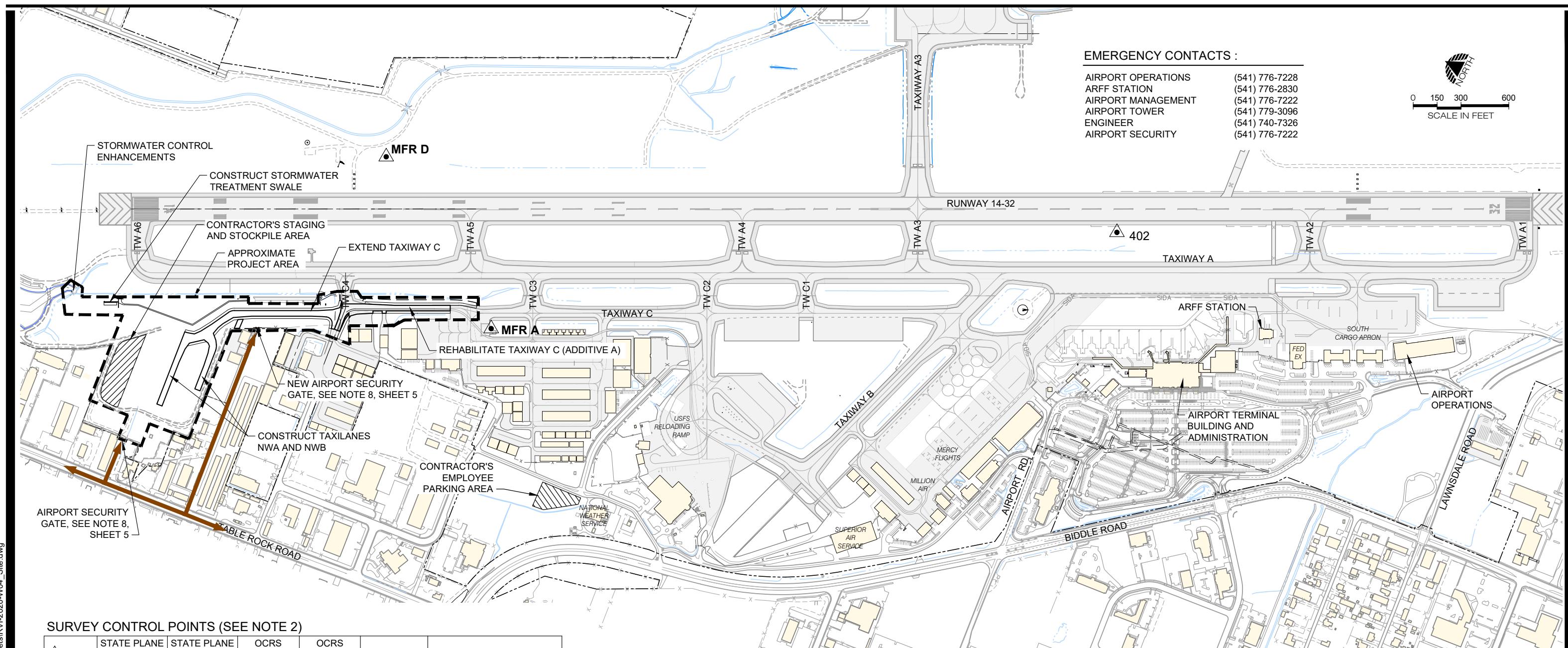
## BASE BID SCHEDULE (cont.)

BID ITEM	SPECIFICATION SECTION	DESCRIPTION	UNIT	BID QUANTITY	CONSTRUCTION QUANTITY
69	L-110	2-INCH PVC CONDUIT	LF	2,930	
70	L-110	CONCRETE ENCASEMENT OF DUCT	LF	300	
71	L-110	ELECTRICAL TRENCH, NEW PAVEMENT AREA	LF	2,340	
72	L-110	ELECTRICAL TRENCH, NON-PAVED AREA	LF	230	
73	E-112	DIRECTIONAL BORE	LS	1	
74	L-115	HANDHOLE	EA	2	
75	L-115	JUNCTION CAN	EA	2	
76	L-115	INSTALL SOLID LID ON EXISTING BASE CAN	EA	1	
77	L-125	RETROREFLECTIVE TAXIWAY END SIGN	EA	1	
78	L-125	TAXIWAY EDGE LIGHT ON NEW BASE CAN	EA	35	
79	L-125	TAXIWAY FLUSH-MOUNTED EDGE LIGHT ON NEW BASE CAN	EA	1	
80	L-125	TAXIWAY GUIDANCE SIGN ON EXISTING FOUNDATION	EA	1	
81	L-125	TAXIWAY GUIDANCE SIGN PANEL REPLACEMENT	EA	3	
82	L-125	TAXIWAY GUIDANCE SIGN, 3 MODULE	EA	1	

## ADDITIVE BID SCHEDULE

BID ITEM	SPECIFICATION SECTION	DESCRIPTION	UNIT	BID QUANTITY	CONSTRUCTION QUANTITY
A-1	E-020	CONSTRUCTION SURVEY AND STAKING	LS	1	
A-2	C-100	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	LS	1	
A-3	C-102	INSTALLATION AND REMOVAL OF STRAW WATTLE	LF	810	
A-4	C-102	TEMPORARY EROSION CONTROL	LS	1	
A-5	C-105	MOBILIZATION	LS	1	
A-6	E-110	CABLE REMOVAL	LF	1,780	
A-7	E-110	RETROREFLECTIVE MARKER REMOVAL	EA	21	
A-8	E-180	SUBGRADE GEOTEXTILE FABRIC	SY	3,100	
A-9	P-101	ASPHALT PAVEMENT REMOVAL, FULL DEPTH	SY	500	
A-10	P-101	ASPHALT PAVEMENT REMOVAL, PARTIAL DEPTH	SY	4,110	
A-11	P-101	PAVEMENT CRACK SEALING	LF	3,000	
A-12	P-152	MUCK EXCAVATION	CY	400	
A-13	P-152	STRUCTURAL FILL	CY	400	
A-14	P-152	UNCLASSIFIED EXCAVATION	CY	1,950	
A-15	P-154	SUBBASE COURSE	CY	1,060	
A-16	P-209	CRUSHED AGGREGATE BASE COURSE	CY	540	
A-17	P-401	ASPHALT SURFACE COURSE	TON	1,300	
A-18	P-603	EMULSIFIED ASPHALT TACK COAT	TON	3	
A-19	P-620	METHACRYLYTE MARKING, YELLOW, TWO COAT	SF	420	
A-20	P-620	WATERBORNE MARKING, BLACK, ONE COAT	SF	50	
A-21	P-620	WATERBORNE MARKING, YELLOW/WHITE, TWO COAT	SF	2,170	
A-22	D-701	6-INCH HDPE PIPE	LF	60	
A-23	D-705	6-INCH PERFORATED HDPE PIPE, NEW PAVEMENT AREA	LF	190	
A-24	D-705	6-INCH PERFORATED HDPE PIPE, NON-PAVED AREA	LF	1,990	
A-25	D-705	UNDERDRAIN CONNECTION TO PIPE	EA	3	
A-26	D-751	CATCH BASIN	EA	1	
A-27	D-751	CLEANOUT	EA	7	
A-28	T-901	SEEDING	AC	1	
A-29	T-905	TOPSOIL	CY	500	
A-30	L-108	NO. 8 AWG CABLE INSTALLED IN CONDUIT, TERRA COTTA	LF	2,540	
A-31	L-110	2-INCH PVC CONDUIT	LF	510	
A-32	L-110	CONCRETE ENCASEMENT OF DUCT	LF	50	
A-33	L-110	ELECTRICAL TRENCH, EXISTING PAVEMENT AREA	LF	40	
A-34	L-110	ELECTRICAL TRENCH, NEW PAVEMENT AREA	LF	480	
A-35	L-125	TAXIWAY EDGE LIGHT ON NEW BASE CAN	EA	12	
A-36	L-125	TAXIWAY FLUSH-MOUNTED EDGE LIGHT ON NEW BASE CAN	EA	3	
A-37	L-125	REMOVED RETROREFLECTIVE MARKER PLACEMENT	EA	6	

REPLACE ENTIRE SHEET



#### SURVEY CONTROL POINTS (SEE NOTE 2)

PT ID	STATE PLANE NORTHING	STATE PLANE EASTING	OCRS NORTHING	OCRS EASTING	NAVD88 ELEV	DESCRIPTION
MFR A	268990.000	4278621.217	229809.071	286876.657	1298.54	PACS (AD9170)
MFR D	269972.418	4279415.623	230817.780	287637.500	1294.07	NEW PACS
402	265492.270	4280508.210	226376.815	288880.720	1316.2	SACS (ARP 1962)

HORIZONTAL DATUM: NAD83 OREGON STATE PLANE, SOUTH ZONE 3602, INTERNATIONAL FEET AND  
OCRS GRANTS PASS-ASHLAND NAD83, INTERNATIONAL FEET

VERTICAL DATUM: NAVD88

#### SURVEY NOTES:

1. HORIZONTAL AND VERTICAL CONTROL SURVEY REFERENCE POINTS ARE PROVIDED BY THE ENGINEER FOR CONTROL OF THE PROJECT. CONTRACTOR SHALL LAYOUT ALL WORK FROM REFERENCE POINTS AS SHOWN ON THE DRAWINGS. ALL LAYOUT AND CONSTRUCTION SURVEY SHALL BE CONDUCTED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF OREGON.
2. PRIOR TO BEGINNING ANY LAYOUT, THE CONTRACTOR'S SURVEYOR SHALL OCCUPY ALL SURVEY POINTS REFERENCED IN THE TABLE SHOWN ON THIS SHEET AND VERIFY DATA GIVEN. ANY DISCREPANCY SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEER'S ATTENTION FOR CLARIFICATION OR CORRECTION. NOTES AND A LETTER APPROVED AND SIGNED BY A STATE LICENSED PROFESSIONAL LAND SURVEYOR (PLS) SHOWING CONFIRMATION OF THE HORIZONTAL AND VERTICAL CONTROL SHALL BE PROVIDED TO THE ENGINEER PRIOR TO STARTING CONSTRUCTION OR CONSTRUCTION LAYOUT, SEE SPECIFICATIONS.

#### GENERAL NOTES

1. ALL CONTRACTOR AND SUBCONTRACTOR PERSONNEL SHALL OBTAIN PROPER AIRPORT SECURITY IDENTIFICATION. PERSONNEL WITHOUT PROPER IDENTIFICATION AND TRAINING ARE PROHIBITED WITHIN THE FENCED AREA OF THE AIRPORT. SEE THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) SECTION IN PROJECT SPECIFICATIONS.
2. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE GENERAL REQUIREMENTS SECTION OF THE CONTRACT DOCUMENTS WHERE REQUIREMENTS RELATING TO LAYOUT OF THE WORK, TESTING, AND OTHER CONTRACT PROVISIONS ARE DESCRIBED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE TO LOCATE AND PROTECT UTILITIES DURING THIS PROJECT. COORDINATE WITH ENGINEER, UTILITY COMPANIES, AND AIRPORT OPERATIONS PRIOR TO EXCAVATION.
4. LIMITS OF WORK SHOWN ON THIS SHEET ARE APPROXIMATE AND SHOW GENERAL AREAS OF WORK. SEE SPECIFIC SHEETS FOR ACTUAL LIMITS OF WORK.
5. SEE CSPP FOR ADDITIONAL STAGING AREA REQUIREMENTS.

#### MAJOR WORK ELEMENTS

- EXCAVATION/EMBANKMENT - EARTHWORK
- ASPHALT PAVEMENT SECTION CONSTRUCTION
- DRAINAGE IMPROVEMENTS (PIPE AND STRUCTURES)
- TAXIWAY EDGE LIGHTING AND SIGNAGE
- PAVEMENT MARKING
- EROSION CONTROL/SEEDING



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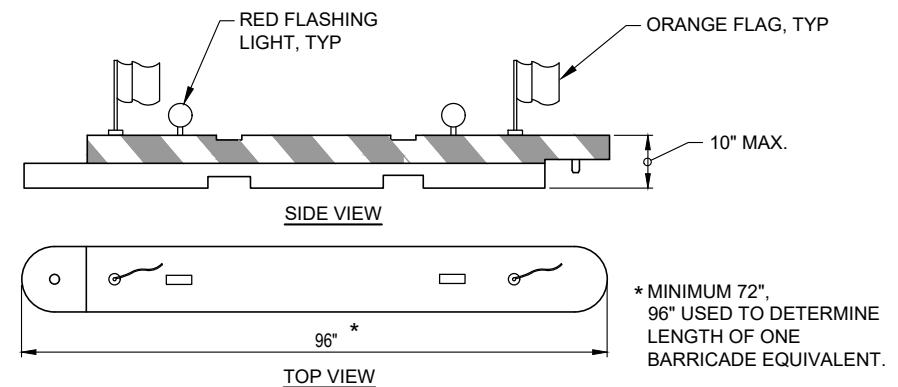
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**SITE PLAN**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**4**  
OF **71**

### CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES:

1. THE CONSTRUCTION SAFETY AND PHASING PLAN DRAWINGS SUPPLEMENT THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) TEXT AND OTHER REQUIREMENTS CONTAINED WITHIN THE CONTRACT DOCUMENT.
2. EXCEPT FOR NOTED CLOSURES, ACTIVE AIRPORT AND AIR OPERATION AREAS (AOA) TO REMAIN OPEN TO AIRCRAFT OPERATIONS DURING ENTIRE COURSE OF WORK. SEE SPECIFICATIONS AND CONSTRUCTION SAFETY AND PHASING PLAN TEXT FOR ADDITIONAL REQUIREMENTS, CONTRACTOR SHALL COORDINATE WORK TO ASSURE MINIMUM INCONVENIENCE TO AIRPORT OPERATIONS. IN ALL CIRCUMSTANCES SAFETY SHALL TAKE PRECEDENCE.
3. THE CONTRACTOR SHALL NOTIFY THE ENGINEER WITH REQUESTS FOR NOTICES TO AIRMEN (NOTAMS) A MINIMUM OF 48 HOURS PRIOR TO IMPLEMENTATION. CONTRACTOR SHALL COORDINATE WITH THE ENGINEER FOR CONSTRUCTION RELATED AOA CLOSURES. CLOSURES ARE SUBJECT TO APPROVAL BY THE OWNER. SEE CONSTRUCTION OPERATIONS AND SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
4. CONTRACTOR SHALL DELINEATE LOCATION OF THE TOFA AT TRAFFIC ACCESS POINTS AND PROVIDE OTHER AIRFIELD DELINEATION TO SEPARATE CONSTRUCTION ACTIVITIES FROM AIRPORT OPERATIONS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER. CONES, STAKES, OR OTHER METHODS, AS APPROVED BY THE ENGINEER, MAY BE USED OUTSIDE THE TOFA.
5. BARRICADES, LIGHTS AND OTHER CONSTRUCTION CONTROL DEVICES FURNISHED, PLACED AND MAINTAINED BY THE CONTRACTOR SHALL BE PROVIDED AT VARIOUS LOCATIONS AS NECESSARY TO ADEQUATELY SEPARATE CONSTRUCTION ACTIVITIES FROM ACTIVE AIRPORT OPERATIONS AREAS (AOA). BARRICADES SHOWN ON THE DRAWINGS ARE FOR REFERENCE AND THE NUMBER AND LOCATION OF BARRICADES MAY CHANGE TO MEET SAFETY REQUIREMENTS. SEE CONSTRUCTION OPERATIONS AND SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
6. LOCATION OF CONTRACTOR'S STAGING AND STOCKPILE AREAS ARE APPROXIMATE. VERIFY LIMITS AND LOCATION WITH ENGINEER PRIOR TO MOBILIZATION. ALL VEHICLES NOT ESSENTIAL FOR CONSTRUCTION, INCLUDING CONTRACTOR-EMPLOYEE VEHICLES, SHALL REMAIN OUTSIDE THE SECURITY FENCE. CONSTRUCTION EQUIPMENT PARKING SHALL BE CONFINED TO THE CONTRACTOR'S STAGING AREA OR THE WORK AREA. VERIFY ALL EQUIPMENT PARKING NOT WITHIN THE DESIGNATED STAGING AREA WITH ENGINEER.
7. PRIOR TO REOPENING A CLOSED AREA FOR OPERATIONS THE CONTRACTOR SHALL PROVIDE ADEQUATE TIME FOR THE ENGINEER OR OWNER TO INSPECT FOR CLEANLINESS AND CONFORMANCE TO REGULATIONS INCLUDING GRADING REQUIREMENTS OF THE ASSOCIATED SAFETY AREAS. THE CONTRACTOR SHALL ALSO VERIFY THAT AIRFIELD LIGHTING AND OTHER NAVADS ARE OPERATING PROPERLY. THE TIME NEEDED FOR INSPECTION AND POSSIBLE NECESSARY CORRECTIVE ACTION SHALL BE INCLUDED WITHIN THE ALLOWED CLOSURE PERIOD.
8. AIRFIELD SECURITY, INCLUDING FENCING AND GATES, SHALL BE MAINTAINED AT ALL TIMES; SEE CSPP TECHNICAL DOCUMENT FOR ADDITIONAL REQUIREMENTS. CONTRACTOR SHALL NOT BLOCK VEHICLE ACCESS ROADS OR GATES AT ANY TIME. GATE GUARD REQUIRED ANYTIME GATE REMAINS OPEN DUE TO CONSTRUCTION.
9. CONTRACTOR SHALL PROVIDE CONTINUOUS VEHICLE ROUTING, MEETING OWNER PROVIDED REQUIREMENTS, AROUND OR THROUGH WORK AREAS AT ALL TIMES, AS APPROVED BY THE ENGINEER. THIS MAY REQUIRE TEMPORARY DELINEATION, SIGNAGE, AND ROADWAY CONSTRUCTION.
10. CONTRACTOR SHALL MAKE AVAILABLE AT ALL TIMES A VACUUM SWEEPER TRUCK AND/OR OTHER ENGINEER-APPROVED EQUIPMENT AS REQUIRED TO KEEP ALL ON-AIRPORT CONSTRUCTION ROUTES CLEAN AND FREE OF FOD. IT IS ANTICIPATED THAT CONTINUOUS MONITORING OF SURFACES WITHIN, AND ADJACENT TO WORK AREAS WILL BE REQUIRED DURING CONSTRUCTION.
11. UNLESS OTHERWISE APPROVED BY THE OWNER, CONTRACTOR ACCESS TO THE PROJECT SITE SHALL BE LIMITED TO DESIGNATED LOCATIONS AND ROUTES SHOWN ON THE PLANS. SEE CONSTRUCTION SAFETY AND PHASING PLAN, AND EROSION AND SEDIMENT CONTROL PLAN FOR SPECIFIC RESTRICTIONS.
12. ALL STAGING AREAS AND HAUL ROUTES, INCLUDING PUBLIC ROADS USED FOR ACCESS, SHALL BE MAINTAINED DURING CONSTRUCTION AND SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION AT THE COMPLETION OF PROJECT WORK.
13. CONTRACTOR SHALL USE STEEL PLATES, LIGHTER LOADS, LIGHTER EQUIPMENT, OR OTHER APPROVED METHODS TO PROTECT EXISTING PAVEMENTS. CONTRACTOR SHALL REPAIR, BY METHOD APPROVED BY ENGINEER AND OWNER, AT CONTRACTOR'S EXPENSE, ALL DAMAGE TO EXISTING PAVEMENTS RESULTING FROM CONSTRUCTION ACTIVITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY EXISTING PAVEMENT CONDITIONS WITH ENGINEER AND OWNER PRIOR TO CONSTRUCTION AND TO DEVELOP AN OWNER-APPROVED PLAN TO PROTECT EXISTING PAVEMENTS FROM DAMAGE DUE TO CONSTRUCTION ACTIVITIES.
14. WORK AREAS SHOWN IN THE CSPP ARE APPROXIMATE. SEE APPROPRIATE DRAWING FOR SPECIFIC WORK LIMITS. CONTRACTOR SHALL COORDINATE ALL WORK ELEMENTS COMPATIBLE WITH INTENDED PHASE UNLESS OTHERWISE APPROVED BY THE ENGINEER.
15. TRAFFIC ROUTES SHOWN ARE APPROXIMATE, AND ARE NOT LIMITED TO LOCATIONS SHOWN. CONTRACTOR SHALL REMAIN CLEAR OF AIRCRAFT AND AIRPORT OPERATIONS AT ALL TIMES.
16. SEQUENCING OF PHASE NUMBERS IS FOR GENERAL INFORMATION ONLY. ACTUAL SEQUENCE OF CONSTRUCTION PHASES WILL VARY BASED ON AIRPORT DEMAND AND WILL BE DETERMINED SOLELY BY THE OWNER AFTER BID OPENING. PHASE 1 IS THE ANTICIPATED START.
17. ALL PORTIONS OF WORK SHOWN ON THE CSPP NOT COVERED BY PAYMENT UNDER A SPECIFIC BID ITEM OR LISTED AS INCIDENTAL TO A BID ITEM SHALL BE CONSIDERED INCIDENTAL TO THE MOBILIZATION BID ITEM.



#### NOTES:

1. FOR DELINEATION ON PAVEMENT BETWEEN ACTIVE AIRCRAFT OPERATIONS AREAS AND CONSTRUCTION SITES.
2. BARRICADE COLORS SHALL ALTERNATE ORANGE AND WHITE.

BARRICADE DETAIL  
NTS

1  
5

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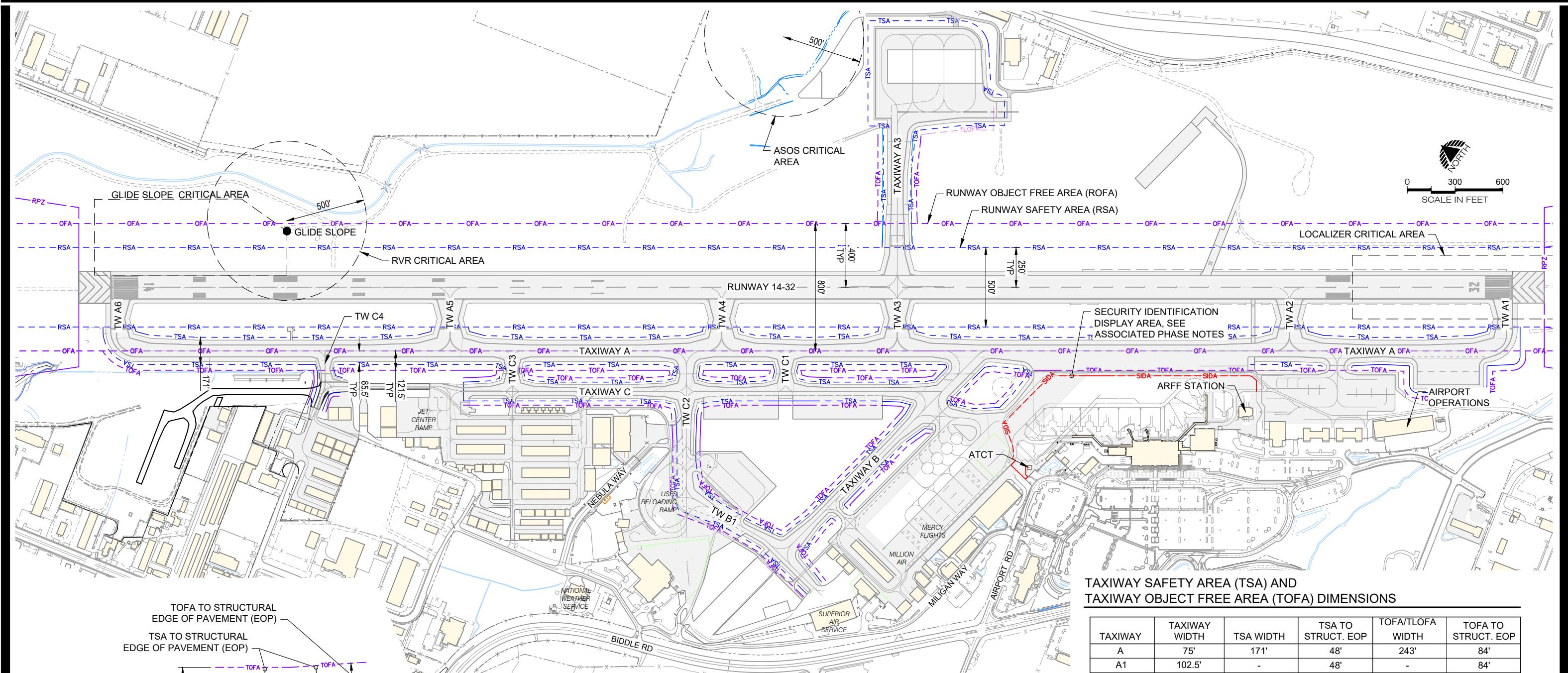


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ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**CONSTRUCTION SAFETY AND PHASING PLAN - NOTES**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER: RVI-2020-W04  
SHEET NO. 5 OF 71



## TAXIWAY SAFETY AREA (TSA) AND TAXIWAY OBJECT FREE AREA (TOFA) DIMENSIONS

TAXIWAY	TAXIWAY WIDTH	TSA WIDTH	TSA TO STRUCT. EOP	TOFA/TLOFA WIDTH	TOFA TO STRUCT. EOP
A	75'	171'	48'	243'	84'
A1	102.5'	-	48'	-	84'
A2-A5	130'	-	48'	-	84'
A6	102.5	-	48'	-	84'
B	50'	118'	34'	171'	60.5'
B1	50'	118'	34'	171'	60.5'
B2	40'	-	34'	-	60.5'
B3	55'-60'	-	34'	-	60.5'
C	50'	118'	34'	171'	60.5'
C1-C2	50'	118'	34'	171'	60.5'
C3	35'	79'	22'	124'	44.5'
C4	35'	79'	22'	124'	44.5'
TAXILANE	25'	49'	-	89'/79'	-

NOTE: SEE CRITICAL AREA DETAIL 1 FOR DEFINITION OF WIDTHS

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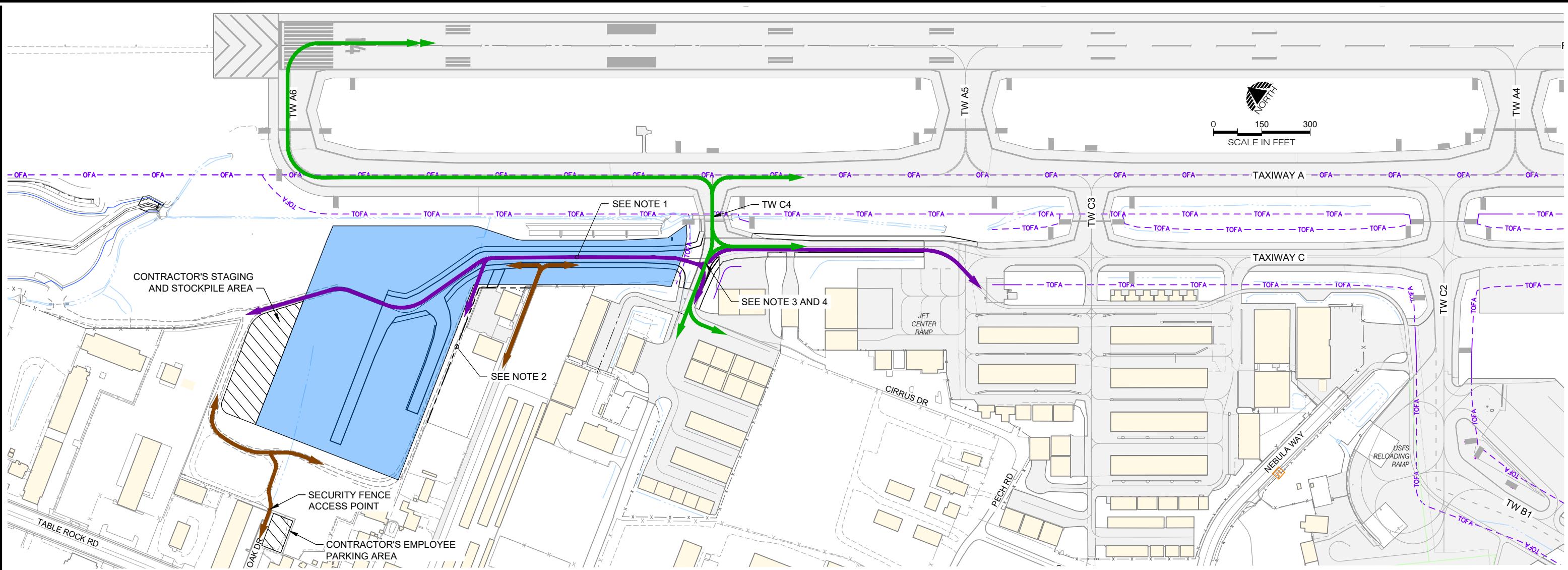
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# **CONSTRUCTION SAFETY AND PHASING PLAN - CRITICAL AREAS**

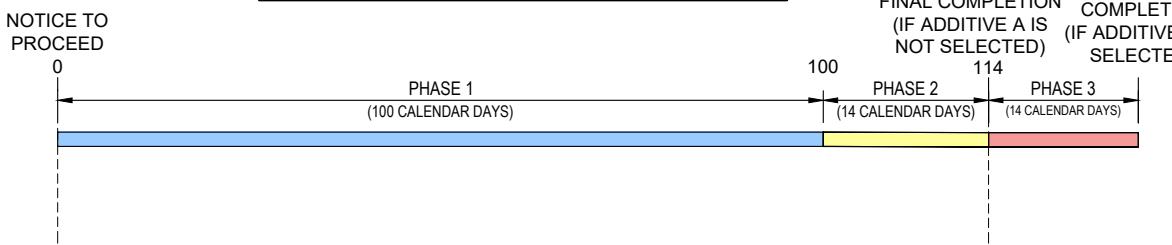
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PROJECT NUMBER:  
**RV1-2020-W04**

HEET NO.  
**6**



#### CONSTRUCTION PHASING SCHEDULE



#### PROJECT CONSTRUCTION TIME

PHASE	AIR OPERATIONS AREA (AOA) IMPACT	ALLOWABLE DAILY WORK PERIOD	MAXIMUM NO. OF CALENDAR DAYS
1	NO AOA IMPACT	ANY *	100
2	TAXIWAY C AND C3 CLOSED	ANY *	14
3	TAXIWAY C BETWEEN TW B3 AND TW C4 CLOSED	ANY *	14

\* IF CONTRACTOR ELECTS TO PERFORM NIGHTTIME WORK, A WORK AREA LIGHTING PLAN SHALL BE SUBMITTED TO THE RPR. WORK LIGHTING PLAN SHALL INCLUDE AT A MINIMUM THE QUANTITY, LOCATION, AND LIGHT BEAM DIRECTION FOR ALL REQUIRED PROJECT LIGHTING. SEE CSPP TECHNICAL SECTION 2.21 FOR MORE INFORMATION.

#### PHASE 1 NOTES - 100 CALENDAR DAYS

1. CONTRACTOR SHALL PROVIDE CONTINUOUS VEHICLE ACCESS ROUTE FOR AIRPORT VEHICLES THROUGH THE CONSTRUCTION ZONE DURING ALL PHASES OF THE WORK.
2. AIRPORT PROPERTY LEASED TO TENANT, CONSTRUCTION ACCESS AND ACTIVITY SHALL BE LIMITED. COORDINATE WITH RPR PRIOR TO ENTERING LEASED SPACE.
3. LIMITED CONSTRUCTION WITHIN TAXIWAY C4 TOFA MAY BE PERMITTED IF TAXIWAY C4 IS TEMPORARILY CLOSED FOR OPERATION. COORDINATE WITH RPR.
4. CONSTRUCTION ACCESS WITHIN TAXIWAY A AND TAXIWAY C4 TOFA IS NOT PERMITTED WITHIN PHASE 1 WORK. AOA SHALL REMAIN OPEN.

#### PHASE 2 NOTES - 14 CALENDAR DAYS

1. CONSTRUCTION ACCESS WITHIN TAXIWAY A TOFA IS NOT PERMITTED DURING PHASE 2. TAXIWAY A SHALL REMAIN OPEN AT ALL TIMES.
2. AT THE COMPLETION OF PHASE 2, ALL PAVEMENTS SHOULD BE INSTALLED, ELECTRICAL SYSTEMS SHOULD BE OPERATIONAL, ALL DRAINAGE AND TREATMENT SYSTEMS SHOULD BE INSTALLED AND OPERATIONAL, PAVEMENT MARKING SHOULD BE PLACED, AND THE PROJECT SHOULD BE CONSIDERED SUBSTANTIALLY COMPLETE.

#### PHASE 3 - 14 CALENDAR DAYS

1. PHASE 3 WORK AREA IS ONLY ALLOWED IF ADDITIVE A IS SELECTED.
2. CONSTRUCTION TRAFFIC CROSSES ACTIVE TAXIWAY C4 DURING THIS PHASE. TRAFFIC SHALL YIELD TO AIRCRAFT AT ALL TIMES. FOD SHALL BE REMOVED AND CLEANED FROM ACTIVE SURFACE IMMEDIATELY UPON TRIGGERING EVENT.

#### PHASING LEGEND

<span style="background-color: blue; display: inline-block; width: 15px; height: 10px;"></span>	PHASE 1 WORK AREA
<span style="background-color: yellow; display: inline-block; width: 15px; height: 10px;"></span>	PHASE 2 WORK AREA
<span style="background-color: red; display: inline-block; width: 15px; height: 10px;"></span>	PHASE 3 WORK AREA
<span style="background-color: green; display: inline-block; width: 15px; height: 10px;"></span>	AIRCRAFT TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
<span style="background-color: brown; display: inline-block; width: 15px; height: 10px;"></span>	CONSTRUCTION TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
<span style="background-color: purple; display: inline-block; width: 15px; height: 10px;"></span>	AIRPORT VEHICLE TRAFFIC, SEE GENERAL NOTE 15, SHEET 5
<span style="border: 1px solid black; border-radius: 50%; width: 10px; height: 10px; display: inline-block;"></span>	AIRFIELD SIGN TO BE SCREENED, OR PARTIALLY SCREENED BY APPROVED METHODS PRIOR TO STARTING PHASE
<span style="background-color: black; border: 1px solid black; width: 10px; height: 10px; display: inline-block;"></span>	PORTABLE LOW PROFILE BARRICADES WITH RED LIGHTS TO BE PLACED BY THE CONTRACTOR DURING RUNWAY, TAXIWAY AND/OR OTHER WORK AREA CLOSURES FOR EACH PHASE OR AS DIRECTED BY THE ENGINEER. MAY REQUIRE ADJUSTMENT DURING CONSTRUCTION ACTIVITIES; SEE CONSTRUCTION SAFETY AND PHASING PLAN AND DETAIL 1
<span style="background-color: black; border: 1px solid black; width: 10px; height: 10px; display: inline-block;"></span>	REPLACE ENTIRE SHEET 5

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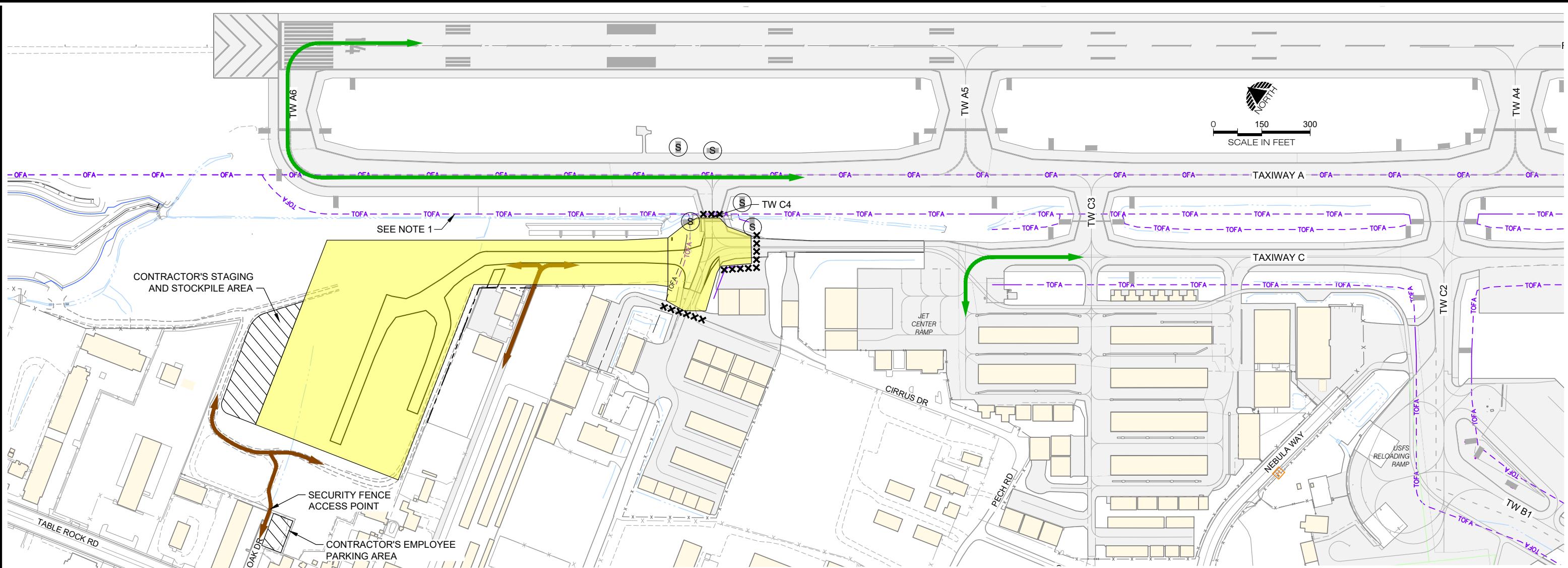


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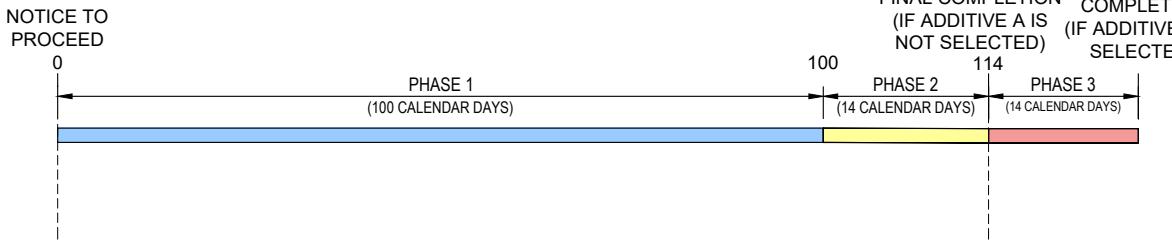
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TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 1**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**7**  
OF 71



#### CONSTRUCTION PHASING SCHEDULE



#### PHASE 1 NOTES - 100 CALENDAR DAYS

1. CONTRACTOR SHALL PROVIDE CONTINUOUS VEHICLE ACCESS ROUTE FOR AIRPORT VEHICLES THROUGH THE CONSTRUCTION ZONE DURING ALL PHASES OF THE WORK.
2. AIRPORT PROPERTY LEASED TO TENANT, CONSTRUCTION ACCESS AND ACTIVITY SHALL BE LIMITED. COORDINATE WITH RPR PRIOR TO ENTERING LEASED SPACE.
3. LIMITED CONSTRUCTION WITHIN TAXIWAY C4 TOFA MAY BE PERMITTED IF TAXIWAY C4 IS TEMPORARILY CLOSED FOR OPERATION. COORDINATE WITH RPR.
4. CONSTRUCTION ACCESS WITHIN TAXIWAY A AND TAXIWAY C4 TOFA IS NOT PERMITTED WITHIN PHASE 1 WORK. AOA SHALL REMAIN OPEN.

#### PHASE 2 NOTES - 14 CALENDAR DAYS

1. CONSTRUCTION ACCESS WITHIN TAXIWAY A TOFA IS NOT PERMITTED DURING PHASE 2. TAXIWAY A SHALL REMAIN OPEN AT ALL TIMES.
2. AT THE COMPLETION OF PHASE 2, ALL PAVEMENTS SHOULD BE INSTALLED, ELECTRICAL SYSTEMS SHOULD BE OPERATIONAL, ALL DRAINAGE AND TREATMENT SYSTEMS SHOULD BE INSTALLED AND OPERATIONAL, PAVEMENT MARKING SHOULD BE PLACED, AND THE PROJECT SHOULD BE CONSIDERED SUBSTANTIALLY COMPLETE.

#### PHASE 3 - 14 CALENDAR DAYS

1. PHASE 3 WORK AREA IS ONLY ALLOWED IF ADDITIVE A IS SELECTED.
2. CONSTRUCTION TRAFFIC CROSSES ACTIVE TAXIWAY C4 DURING THIS PHASE. TRAFFIC SHALL YIELD TO AIRCRAFT AT ALL TIMES. FOD SHALL BE REMOVED AND CLEANED FROM ACTIVE SURFACE IMMEDIATELY UPON TRIGGERING EVENT.

#### PHASING LEGEND

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<span style="background-color: green; display: inline-block; width: 15px; height: 15px;"></span>	AIRCRAFT TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
<span style="background-color: brown; display: inline-block; width: 15px; height: 15px;"></span>	CONSTRUCTION TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
<span style="background-color: purple; display: inline-block; width: 15px; height: 15px;"></span>	AIRPORT VEHICLE TRAFFIC, SEE GENERAL NOTE 15, SHEET 5
<span style="background-color: black; border: 1px solid black; display: inline-block; width: 15px; height: 15px;"></span>	AIRFIELD SIGN TO BE SCREENED, OR PARTIALLY SCREENED BY APPROVED METHODS PRIOR TO STARTING PHASE
<span style="background-color: black; border: 1px solid black; display: inline-block; width: 15px; height: 15px;"></span>	PORTABLE LOW PROFILE BARRICADES WITH RED LIGHTS TO BE PLACED BY THE CONTRACTOR DURING RUNWAY, TAXIWAY AND/OR OTHER WORK AREA CLOSURES FOR EACH PHASE OR AS DIRECTED BY THE ENGINEER. MAY REQUIRE ADJUSTMENT DURING CONSTRUCTION ACTIVITIES; SEE CONSTRUCTION SAFETY AND PHASING PLAN AND DETAIL 1 OF 5

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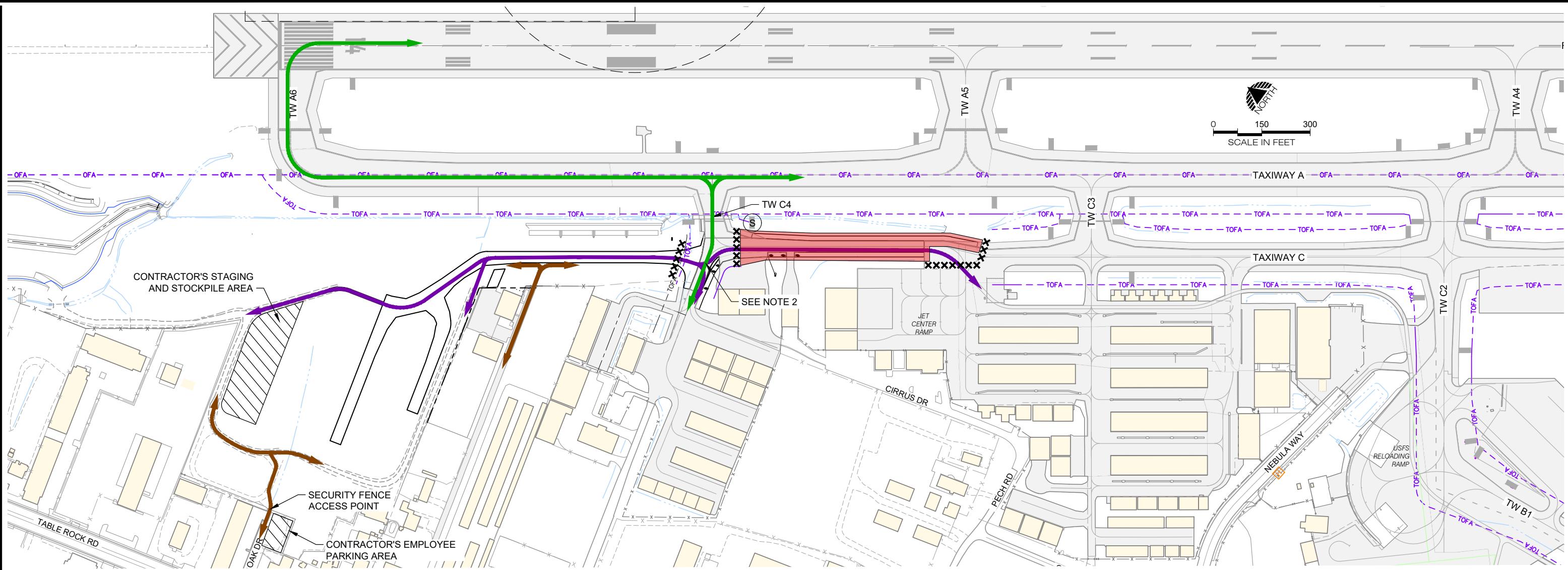
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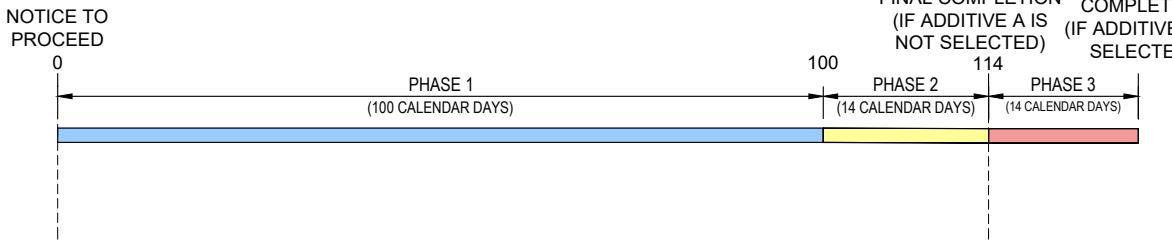
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 2**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**8**  
OF **71**



#### CONSTRUCTION PHASING SCHEDULE



#### PHASE 1 NOTES - 100 CALENDAR DAYS

1. CONTRACTOR SHALL PROVIDE CONTINUOUS VEHICLE ACCESS ROUTE FOR AIRPORT VEHICLES THROUGH THE CONSTRUCTION ZONE DURING ALL PHASES OF THE WORK.
2. AIRPORT PROPERTY LEASED TO TENANT, CONSTRUCTION ACCESS AND ACTIVITY SHALL BE LIMITED. COORDINATE WITH RPR PRIOR TO ENTERING LEASED SPACE.
3. LIMITED CONSTRUCTION WITHIN TAXIWAY C4 TOFA MAY BE PERMITTED IF TAXIWAY C4 IS TEMPORARILY CLOSED FOR OPERATION. COORDINATE WITH RPR.
4. CONSTRUCTION ACCESS WITHIN TAXIWAY A AND TAXIWAY C4 TOFA IS NOT PERMITTED WITHIN PHASE 1 WORK. AOA SHALL REMAIN OPEN.

#### PHASE 2 NOTES - 14 CALENDAR DAYS

1. CONSTRUCTION ACCESS WITHIN TAXIWAY A TOFA IS NOT PERMITTED DURING PHASE 2. TAXIWAY A SHALL REMAIN OPEN AT ALL TIMES.
2. AT THE COMPLETION OF PHASE 2, ALL PAVEMENTS SHOULD BE INSTALLED, ELECTRICAL SYSTEMS SHOULD BE OPERATIONAL, ALL DRAINAGE AND TREATMENT SYSTEMS SHOULD BE INSTALLED AND OPERATIONAL, PAVEMENT MARKING SHOULD BE PLACED, AND THE PROJECT SHOULD BE CONSIDERED SUBSTANTIALLY COMPLETE.

#### PHASE 3 - 14 CALENDAR DAYS

1. PHASE 3 WORK AREA IS ONLY ALLOWED IF ADDITIVE A IS SELECTED.
2. CONSTRUCTION TRAFFIC CROSSES ACTIVE TAXIWAY C4 DURING THIS PHASE. TRAFFIC SHALL YIELD TO AIRCRAFT AT ALL TIMES. FOD SHALL BE REMOVED AND CLEANED FROM ACTIVE SURFACE IMMEDIATELY UPON TRIGGERING EVENT.

#### PHASING LEGEND

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<span style="background-color: red; border: 1px solid black; display: inline-block; width: 15px; height: 15px;"></span>	PHASE 3 WORK AREA
<span style="background-color: green; border: 1px solid black; display: inline-block; width: 15px; height: 15px;"></span>	AIRCRAFT TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
<span style="background-color: brown; border: 1px solid black; display: inline-block; width: 15px; height: 15px;"></span>	CONSTRUCTION TRAFFIC, SEE CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES ON SHEET 5
<span style="background-color: purple; border: 1px solid black; display: inline-block; width: 15px; height: 15px;"></span>	AIRPORT VEHICLE TRAFFIC, SEE GENERAL NOTE 15, SHEET 5
<span style="border: 1px solid black; border-radius: 50%; width: 15px; height: 15px; display: inline-block; text-align: center; line-height: 15px;">S</span>	AIRFIELD SIGN TO BE SCREENED, OR PARTIALLY SCREENED BY APPROVED METHODS PRIOR TO STARTING PHASE
<span style="background-color: black; border: 1px solid black; display: inline-block; width: 15px; height: 15px;"></span>	PORTABLE LOW PROFILE BARRICADES WITH RED LIGHTS TO BE PLACED BY THE CONTRACTOR DURING RUNWAY, TAXIWAY AND/OR OTHER WORK AREA CLOSURES FOR EACH PHASE OR AS DIRECTED BY THE ENGINEER. MAY REQUIRE ADJUSTMENT DURING CONSTRUCTION ACTIVITIES; SEE CONSTRUCTION SAFETY AND PHASING PLAN AND DETAIL 1/5

REPLACE ENTIRE SHEET

#### PRINT IN COLOR

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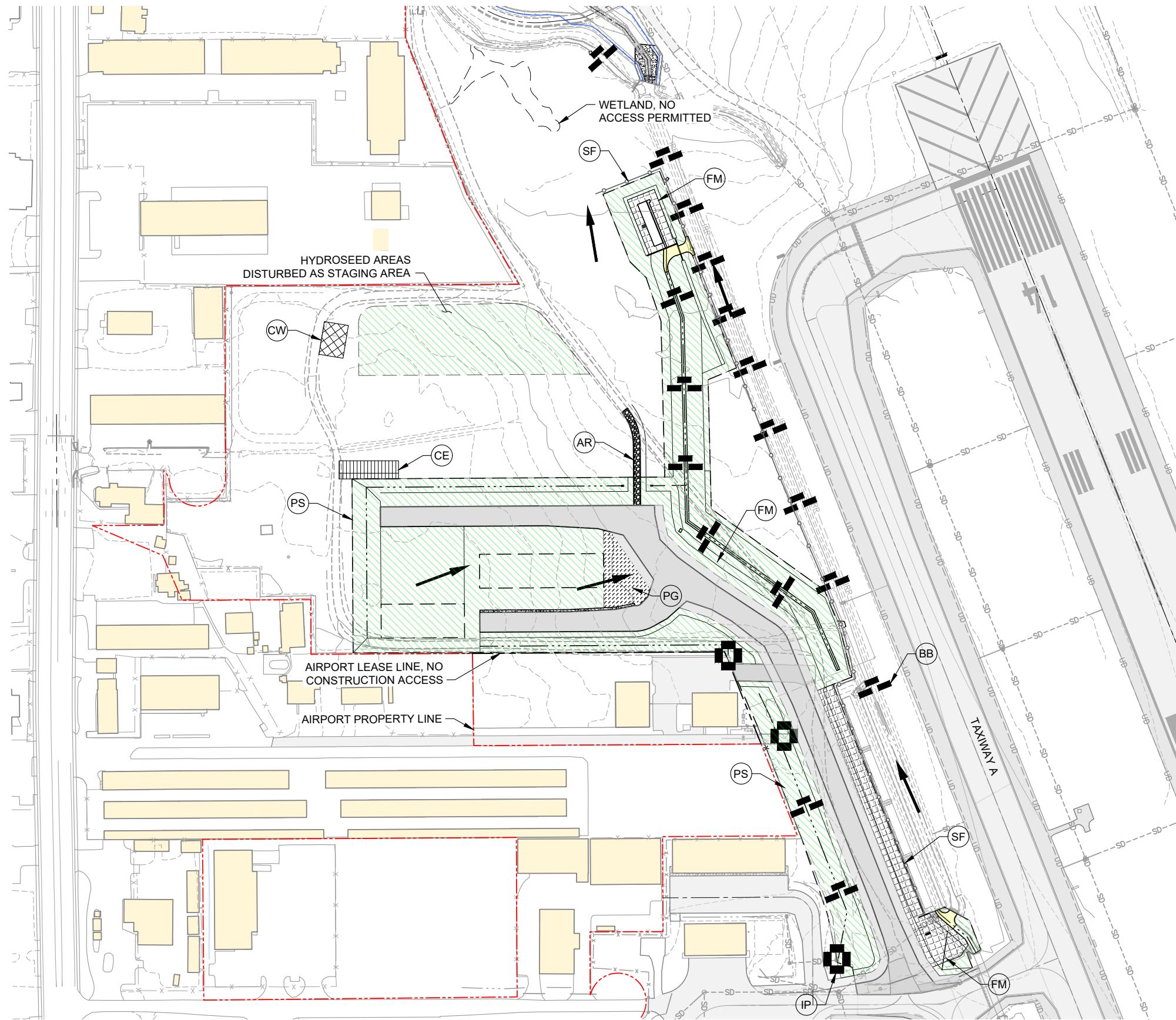


ADDENDUM NO. 1  
REVISIONS: 5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 3  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER: RVI-2020-W04  
SHEET NO. 9 OF 71



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SCALE IN FEET

#### LEGEND - STANDARD PRACTICE CODING SYSTEM:

CODE	SYMBOL	DESCRIPTION
BB		BIOFILTER BAGS, SEE DETAIL 14
IP		STORM DRAIN INLET PROTECTION, SEE DETAIL 14
SF		SILT FENCE, SEE DETAIL 13
PS		TEMPORARY AND PERMANENT SEEDING
FM		FIBER MATTING AND PERMANENT SEEDING, SEE SPECIFICATIONS
PG		PERMANENT GRINDINGS, MIN. 6" DEPTH, SEE NOTE 6
CE		CONSTRUCTION ENTRANCE, SEE DETAIL 14
CW		CONCRETE WASHOUT, SEE DETAIL 13
AR		RELOCATED AIRPORT ROADWAY
SW		STRAW WATTLE, SEE DETAIL 13
		DIRECTION OF FLOW
		SALVAGED EXISTING ROUND AGGREGATE, SEE NOTE 9
		RIPRAP PROTECTION, SEE PLANS

#### NOTES

1. BIOFILTER BAGS SHOULD BE INSTALLED AS OUTLET PROTECTION ONCE NEW STORM OUTFALLS ARE INSTALLED, SEE DRAINAGE PLANS.
2. ADD BIOFILTER BAGS AND OTHER EROSION CONTROL FACILITIES AS REQUIRED OR DIRECTED BY THE RPR.
3. EROSION CONTROL ITEMS TO BE PLACED PRIOR TO CONSTRUCTION IN EACH WORK AREA. ADDITIONAL EROSION CONTROL ITEMS TO BE PLACED IN COMPLETED DITCHES AND SWALES AFTER THEIR COMPLETION.
4. HYDROSEEDING SHALL BE PERFORMED AT THE COMPLETION OF ALL WORK IN EACH CONSTRUCTION AREA.
5. EROSION CONTROL ITEMS TO BE REMOVED ONCE CORRESPONDING CONSTRUCTION WORK HAS BEEN COMPLETED AND THE SITE HAS BEEN STABILIZED WITH PERMANENT EROSION CONTROL MEASURES AS DETERMINED BY THE RPR.
6. GRINDINGS SHALL BE PLACED AT A MINIMUM OF 6" DEPTH AND COMPACTED TO A FIRM, UNYIELDING CONDITION, SUBJECT TO APPROVAL BY THE RPR.
7. PUBLIC RIGHTS-OF-WAY AND AIRCRAFT OPERATIONS AREAS SHALL BE KEPT IN A CLEAN AND SERVICEABLE CONDITION AT ALL TIMES. IN THE EVENT MATERIALS ARE INADVERTENTLY DEPOSITED ON ROADWAYS, TAXIWAYS OR APRONS, THE MATERIAL SHALL BE PROMPTLY REMOVED.
8. AREAS DISTURBED BY CONSTRUCTION ACTIVITY, INCLUDING STAGING AND DISPOSAL AREAS, SHALL BE COVERED BY HYDRAULICALLY APPLIED SEED AND TACKING AGENTS, VERIFY LOCATIONS WITH RPR, SEE SPECIFICATIONS. ANY AREAS DISTURBED BY THE CONTRACTOR OUTSIDE DISPOSAL AREAS, OR OUTSIDE OTHER AREAS AS INDICATED ON THE PLANS SHALL BE RESEEDED AT THE CONTRACTOR'S EXPENSE WITH T-901 HYDROSEEDING.
9. THE CONTRACTOR SHALL REMOVE AND STOCKPILE EXISTING ROUND AGGREGATE DURING GRADING ACTIVITIES. ROUND AGGREGATE SHALL BE REAPPLIED TO THE SURFACE AT A MINIMUM 4-INCH DEPTH.

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**PRECISION APPROACH**  
ENGINEERING  
5125 Southwest Hout Street  
Corvallis, OR 97333  
541•754•0043

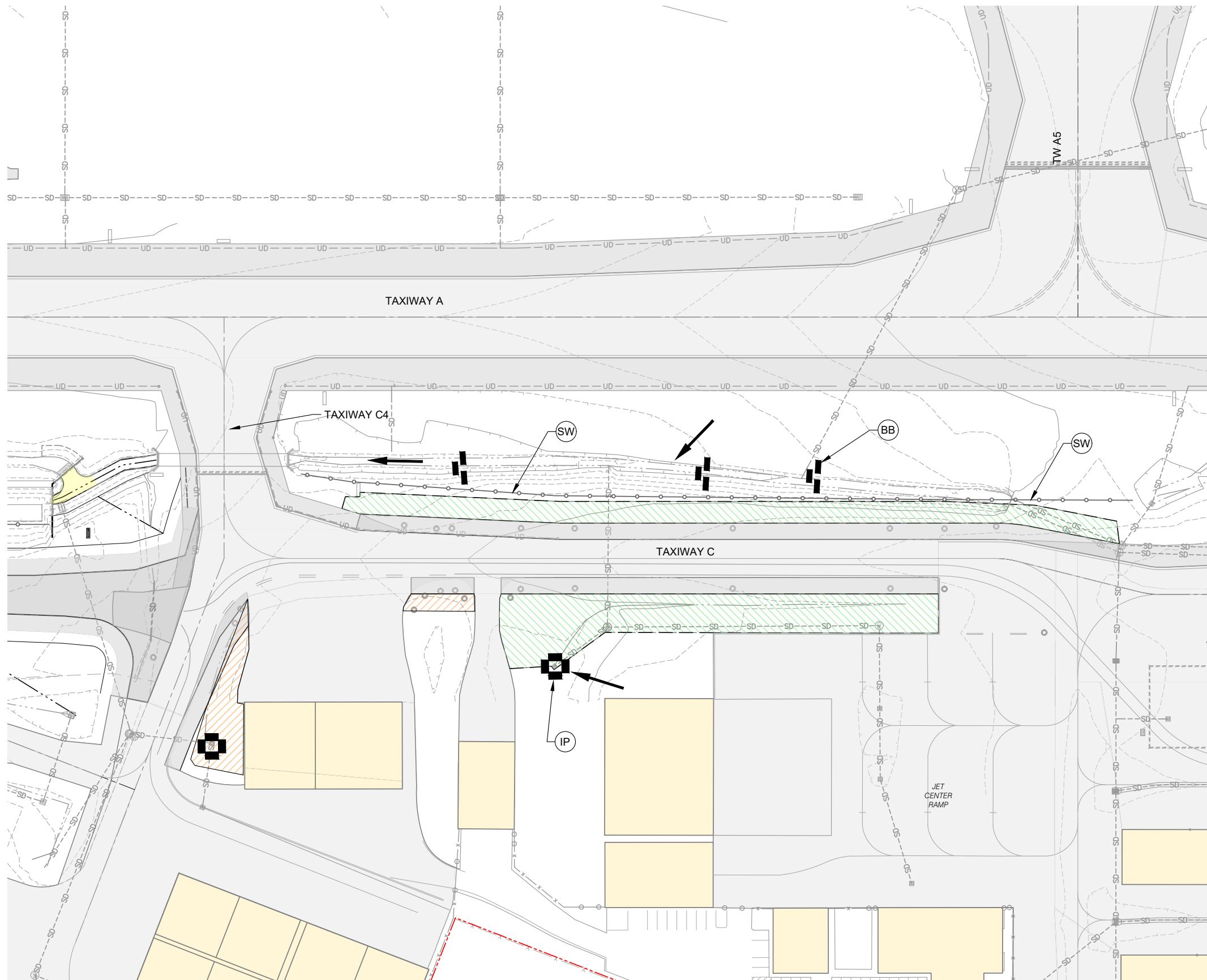


ADDENDUM NO. 1  
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**EROSION AND SEDIMENT  
CONTROL PLAN**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV\I-2020-W04**  
SHEET NO.  
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OF **71**



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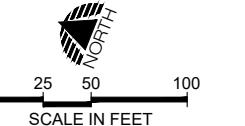
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5/24/2024 JGL  
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REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**EROSION AND SEDIMENT  
CONTROL PLAN - ADDITIVE**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**11**  
OF  
**71**



#### LEGEND - STANDARD PRACTICE CODING SYSTEM:

CODE	SYMBOL	DESCRIPTION
BB		BIOFILTER BAGS, SEE DETAIL 14
IP		STORM DRAIN INLET PROTECTION, SEE DETAIL 14
SF		SILT FENCE, SEE DETAIL 13
PS		TEMPORARY AND PERMANENT SEEDING
FM		FIBER MATTING AND PERMANENT SEEDING, SEE SPECIFICATIONS
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AR		RELOCATED AIRPORT ROADWAY
SW		STRAW WATTLE, SEE DETAIL 3 13
		DIRECTION OF FLOW
		SALVAGED EXISTING ROUND AGGREGATE, SEE NOTE 9
		RIPRAP PROTECTION, SEE PLANS

#### NOTES

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## STANDARD NOTES (DEQ 1200-CA PERMIT)

- Once known, include a list of all contractors that will engage in construction activities on site, and the areas of the site where the contractor(s) will engage in construction activities. Revise the list as appropriate until project completion (Section 15.4.c.i). In addition, include a list of all personnel (by name and position) that are responsible for the design, installation and maintenance of stormwater control measures (e.g. ESCP developer, BMP installer (see Section 15.10), as well as their individual responsibilities. (Section 15.4.c.ii)
- Visual monitoring inspection reports must be made in accordance with DEQ 1200-CA permit requirements. (Section 17.5)
- Inspection logs must be kept in accordance with DEQ's 1200-CA permit requirements. (Section 17.5.q)
- Retain a copy of the ESCP and all revisions on site and make it available on request to DEQ, Agent, or the local municipality. (Section 15.7)
- The permit registrant must implement the ESCP. Failure to implement any of the control measures or practices described in the ESCP is a violation of the permit. (Sections 15 and 15.11)
- The ESCP must be accurate and reflect site conditions. (Section 15.8)
- Submission of all ESCP revisions is not required. Submittal of the ESCP revisions is only under specific conditions. Submit all necessary revision to DEQ or Agent within 10 days. (Section 15.9)
- Sequence clearing and grading to the maximum extent practical to prevent exposed inactive areas from becoming a source of erosion. (Section 13.2.2)
- Create smooth surfaces between soil surface and erosion and sediment controls to prevent stormwater from bypassing controls and ponding. (Section 13.2.3)
- Identify, mark, and protect (by construction fencing or other means) critical riparian areas and vegetation including important trees and associated rooting zones, and vegetation areas to be preserved. Identify vegetative buffer zones between the site and sensitive areas (e.g., wetlands), and other areas to be preserved, especially in perimeter areas. (Section 13.2.1)
- Preserve existing vegetation when practical and re-vegetate open areas. Re-vegetate open areas when practicable before and after grading or construction. Identify the type of vegetative seed mix used. (Section 13.2.5)
- Maintain and delineate any existing natural buffer within the 50-feet of waters of the state. (Section 13.2.4)
- Install perimeter sediment control, including storm drain inlet protection as well as all sediment basins, traps, and barriers prior to land disturbance. (Sections 13.1.3)
- Control both peak flow rates and total stormwater volume, to minimize erosion at outlets and downstream channels and stream banks. (Sections 13.1.1. and 13.2.16)
- Control sediment as needed along the site perimeter and at all operational internal storm drain inlets at all times during construction, both internally and at the site boundary. (Sections 13.2.6 and 13.2.13)
- Establish concrete truck and other concrete equipment washout areas before beginning concrete work. (Section 13.2.14)
- Apply temporary and/or permanent soil stabilization measures immediately on all disturbed areas as grading progresses. Temporary or permanent stabilizations measures are not required for areas that are intended to be left unvegetated, such as dirt access roads or utility pole pads. (Sections 13.2.20 and 13.2.21)
- Establish material and waste storage areas, and other non-stormwater controls. (Section 13.3.7)
- Keep waste container lids closed when not in use and close lids at the end of the business day for those containers that are actively used throughout the day. For waste containers that do not have lids, provide either (1) cover (e.g., a tarp, plastic sheeting, temporary roof) to prevent exposure of wastes to precipitation, or (2) a similarly effective means designed to prevent the discharge of pollutants (e.g., secondary containment). (Section 13.3.7)
- Prevent tracking of sediment onto public or private roads using BMPs such as: construction entrance, graveled (or paved) exits and parking areas, gravel all unpaved roads located onsite, or use an exit tire wash. These BMPs must be in place prior to land-disturbing activities. (Section 13.2.7)
- When trucking saturated soils from the site, either use water-tight trucks or drain loads on site. (Section 13.2.7.f)
- Control prohibited discharges from leaving the construction site, i.e., concrete wash-out, wastewater from cleanout of stucco, paint and curing compounds. (Sections 12 and 13.3.9)
- Ensure that steep slope areas where construction activities are not occurring are not disturbed. (Section 13.2.10)
- Prevent soil compaction in areas where post-construction infiltration facilities are to be installed. (Section 13.2.12)
- Use BMPs to prevent or minimize stormwater exposure to pollutants from spills; vehicle and equipment fueling, maintenance, and storage; other cleaning and maintenance activities; and waste handling activities. These pollutants include fuel, hydraulic fluid, and other oils from vehicles and machinery, as well as debris, fertilizer, pesticides and herbicides, paints, solvents, curing compounds and adhesives from construction operations. (Sections 13.2.15 and 13.3)

## STANDARD NOTES (cont.)

- Provide plans for sedimentation basins that have been designed per Section 13.2.17 and stamped by an Oregon Professional Engineer. (See Section 13.2.17.a)
- If engineered soils are used on site, a sedimentation basin/impoundment must be installed. (See Sections 13.2.17 and 13.2.18)
- Provide a dewatering plan for accumulated water from precipitation and uncontaminated groundwater seepage due to shallow excavation activities. (See Section 13.4)
- Implement the following BMPs when applicable: written spill prevention and response procedures, employee training on spill prevention and proper disposal procedures, spill kits in all vehicles, regular maintenance schedule for vehicles and machinery, material delivery and storage controls, training and signage, and covered storage areas for waste and supplies. (Section 13.3)
- Use water, soil-binding agent or other dust control technique as needed to avoid wind-blown soil. (Section 13.2.9)
- The application rate of fertilizers used to reestablish vegetation must follow manufacturer's recommendations to minimize nutrient release to surface waters. Exercise caution when using time-release fertilizers within any waterway riparian zone. (Section 13.3.5)
- If an active treatment system (for example, electro-coagulation, flocculation, filtration, etc.) for sediment or other pollutant removal is employed, submit an operation and maintenance plan (including system schematic, location of system, location of inlet, location of discharge, discharge dispersion device design, and a sampling plan and frequency) before operating the treatment system. Obtain Environmental Management Plan approval from DEQ before operating the treatment system. Operate and maintain the treatment system according to manufacturer's specifications. (Section 6)
- Temporarily stabilize soils at the end of the shift before holidays and weekends, if needed. The registrant is responsible for ensuring that soils are stable during rain events at all times of the year. (Section 13.2)
- As needed based on weather conditions, at the end of each workday soil stockpiles must be stabilized or covered, or other BMPs must be implemented to prevent discharges to surface waters or conveyance systems leading to surface waters. (Section 13.2.8)
- Sediment fence: remove trapped sediment before it reaches one third of the above ground fence height and before fence removal. (Section 13.1.5.b)
- Other sediment barriers (such as biobags): remove sediment before it reaches two inches depth above ground height and before BMP removal. (Section 13.1.5.c)
- Catch basins: clean before retention capacity has been reduced by fifty percent. Sediment basins and sediment traps: remove trapped sediments before design capacity has been reduced by fifty percent and at completion of project. (Section 13.1.5.d)
- Within 24 hours, significant sediment that has left the construction site, must be remediated. Investigate the cause of the sediment release and implement steps to prevent a recurrence of the discharge within the same 24 hours. Any in-stream clean-up of sediment shall be performed according to the Oregon Department of State Lands required timeframe. (Section 13.2.19.a)
- The intentional washing of sediment into storm sewers or drainage ways must not occur. Vacuuming or dry sweeping and material pickup must be used to cleanup released sediments. (Section 13.2.19)
- Document any portion(s) of the site where land disturbing activities have permanently ceased or will be temporarily inactive for 14 or more calendar days. (Section 17.5.f.)
- Provide temporary stabilization for that portion of the site where construction activities cease for 14 days or more with a covering of blown straw and a tackifier, loose straw, or an adequate covering of compost mulch until work resumes on that portion of the site. (Section 13.2.20)
- Do not remove temporary sediment control practices until permanent vegetation or other cover of exposed areas is established. Once construction is complete and the site is stabilized, all temporary erosion controls and retained moved and disposed of properly, unless needed for long term use following project completion. (Section 13.2.21)

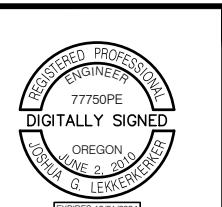
## EROSION CONTROL PROJECT NOTES

- PRIOR TO START OF WORK, THE CONTRACTOR SHALL SUBMIT SCHEDULES FOR ACCOMPLISHMENT OF TEMPORARY AND PERMANENT EROSION CONTROL WORK, INCLUDING A PROPOSED METHOD OF DUST CONTROL AND A PLAN FOR DISPOSAL OF WASTE MATERIALS. SPECIAL ATTENTION IS BROUGHT TO DUST CONTROL ON ALL CONSTRUCTION AREAS, CONTRACTOR SHALL MAINTAIN CONTROL OF AIR POLLUTION THROUGHOUT THE DURATION OF THE PROJECT, SEE SPECIFICATIONS.
- ALL BASE ESC MEASURES (PERIMETER SEDIMENT CONTROL, GRAVEL CONSTRUCTION ENTRANCES, ETC.) MUST BE IN PLACE, FUNCTIONAL, AND APPROVED IN AN INITIAL INSPECTION, PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE ENTIRE DURATION OF THE PROJECT. LOCATION OF GRAVEL CONSTRUCTION ENTRANCE SHALL BE AS DIRECTED BY THE ENGINEER. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, STREET SWEEPING AND VACUUMING MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
- DURING THE CONSTRUCTION PERIOD, THE CONTRACTOR SHALL UPGRADE AND MAINTAIN ALL EROSION CONTROL FACILITIES AS NEEDED FOR WINTERIZATION OR LONG-TERM STABILIZATION, UNEXPECTED STORM EVENTS, AND TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT LEAVE THE SITE.
- ALL EROSION CONTROL FACILITIES SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE CONTINUED FUNCTION.
- AT NO TIME SHALL MORE THAN 1 FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.
- ADDITIONAL MEASURES MUST BE TAKEN BY THE CONTRACTOR, WHEN NECESSARY, TO ENSURE THAT ALL EXISTING PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT. VERIFY WITH RPR LOCATION AND REQUIREMENTS FOR CONSTRUCTION ACCESS.
- LONG-TERM STABILIZATION MEASURES SHALL INCLUDE THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER VIA HYDROSEEDING. SEE SPECIFICATIONS.

## INSPECTION FREQUENCY

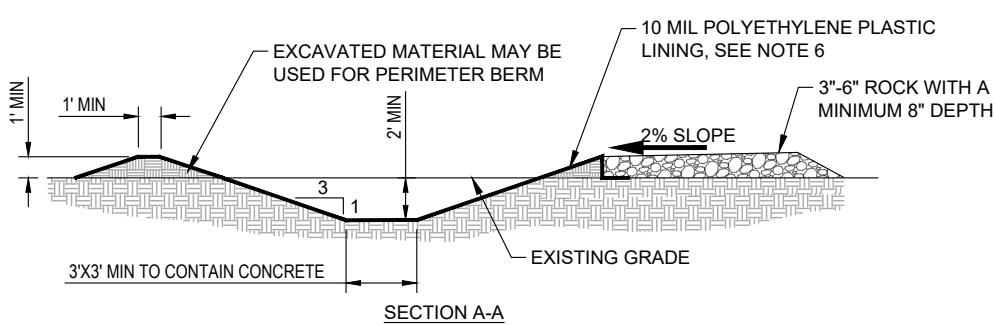
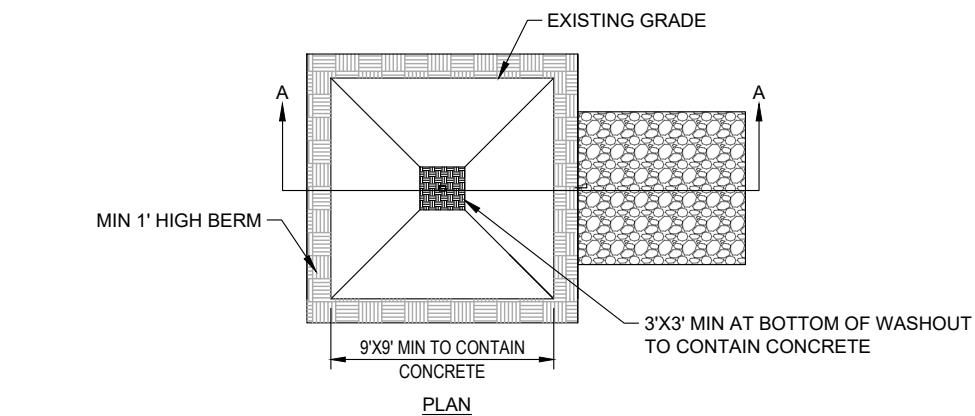
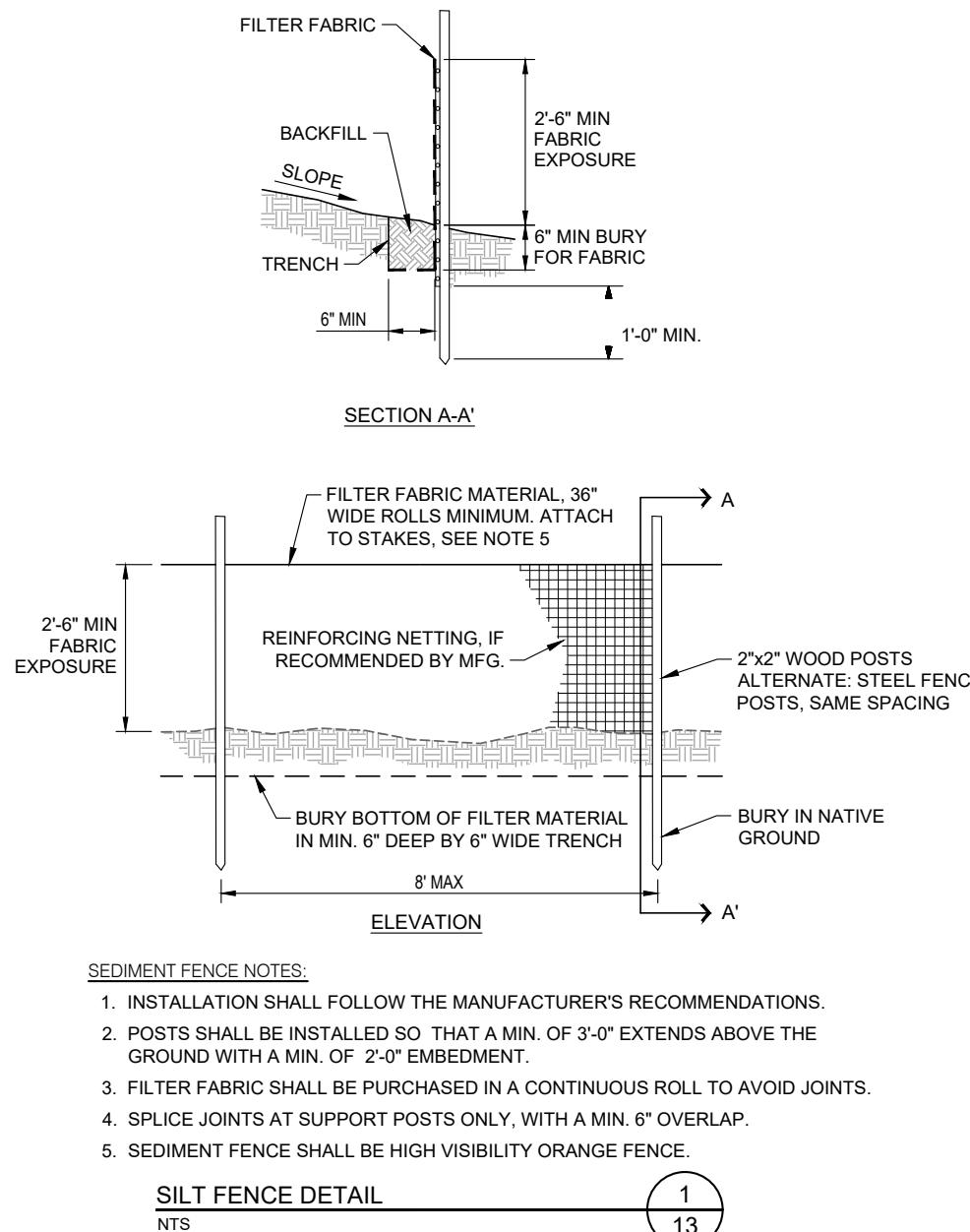
SITE CONDITION	MINIMUM FREQUENCY
1. ACTIVE PERIOD	ON INITIAL DATE THAT LAND DISTURBANCE ACTIVITIES COMMENCE.  WITHIN 24 HOURS OF ANY STORM EVENT, INCLUDING RUNOFF FROM SNOW MELT, THAT RESULTS IN DISCHARGE FROM THE SITE.  AT LEAST ONCE EVERY 14 DAYS, REGARDLESS OF WHETHER STORMWATER RUNOFF IS OCCURRING.
2. INACTIVE PERIODS GREATER THAN FOURTEEN (14) CONSECUTIVE CALENDAR DAYS.	THE INSPECTOR MAY REDUCE THE FREQUENCY OF INSPECTIONS IN ANY AREA OF THE SITE WHERE THE STABILIZATION STEPS IN SECTION 2.2.20 HAVE BEEN COMPLETED TO TWICE PER MONTH FOR THE FIRST MONTH, NO LESS THAN 14 CALENDAR DAYS APART, THEN ONCE PER MONTH.
3. PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER.	IF SAFE, ACCESSIBLE AND PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT DISCHARGE POINT OR DOWNSTREAM LOCATION OF THE RECEIVING WATERBODY.
4. PERIODS DURING WHICH CONSTRUCTION ACTIVITIES ARE SUSPENDED AND RUNOFF IS UNLIKELY DURING FROZEN CONDITIONS.	VISUAL MONITORING INSPECTIONS MAY BE TEMPORARILY SUSPENDED. IMMEDIATELY RESUME MONITORING UPON THAWING, OR WHEN WEATHER CONDITIONS MAKE DISCHARGE LIKELY.
5. PERIODS DURING WHICH CONSTRUCTION ACTIVITIES ARE CONDUCTED AND RUNOFF IS UNLIKELY DURING FROZEN CONDITIONS.	VISUAL MONITORING INSPECTIONS MAY BE TEMPORARILY REDUCED TO ONCE A MONTH. IMMEDIATELY RESUME MONITORING UPON THAWING, OR WHEN WEATHER CONDITIONS MAKE DISCHARGE LIKELY.

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1 ADDENDUM NO. 1	5/24/2024	JGL
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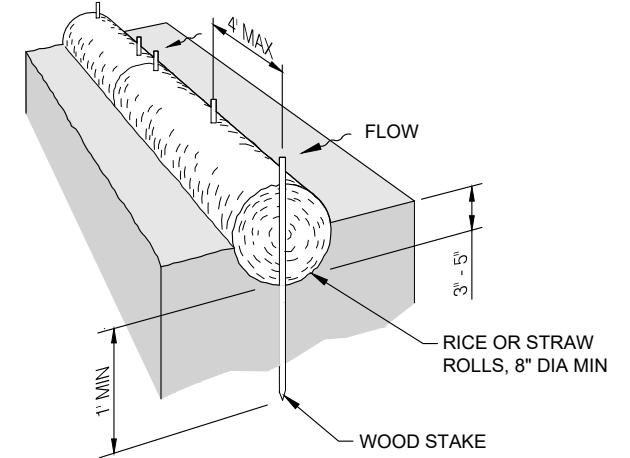
DATE: MAY 2024	DESIGN: KJK	ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES	PROJECT NUMBER: RVI-2020-W04
DRAWN: JAW	CHECKED: JGL	EROSION AND SEDIMENT CONTROL NOTES	
REVISION NUMBER: 0	SCALE: AS SHOWN	PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063	
12		SHEET NO.	71

**NOTES:**

1. WASHOUT FACILITIES SHALL BE MAINTAINED TO PROVIDE ADEQUATE HOLDING CAPACITY WITH A MINIMUM FREEBOARD OF 12 INCHES.
2. WASHOUT FACILITY MUST BE CLEANED OR NEW FACILITIES MUST BE CONSTRUCTED AND READY FOR USE ONCE THE WASHOUT IS 75% FULL.
3. IF THE WASHOUT IS NEARING CAPACITY, VACUUM AND DISPOSE OF THE WASTE MATERIAL IN AN APPROVED MANNER.
4. TEMPORARY CONCRETE WASHOUT FACILITIES SHALL BE LOCATED A MINIMUM OF 50 FT FROM SENSITIVE AREAS INCLUDING OPEN DRAINAGE FACILITIES AND WATER SOURCES.
5. CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED AND MAINTAINED IN SUFFICIENT QUANTITY AND SIZE TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS.
6. PLASTIC LINER SHALL BE SECURED TO THE TOP OF THE PERIMETER BERM, AT 2' SPACING, ON ALL SIDES, BY RPR APPROVED METHOD.
7. TEMPORARY CONCRETE WASHOUTS MAY BE A PREFABRICATED CONTAINER THAT IS PORTABLE AND REUSABLE. SUBMIT TO RPR FOR APPROVAL.
8. AT COMPLETION OF CONSTRUCTION, CONCRETE WASHOUT AND ALL WASHOUT MATERIAL SHALL BE REMOVED AND DISPOSED OF OFF SITE. WASHOUT AREA AND AREAS DIRECTLY ADJACENT SHALL MEET OR EXCEED EXISTING CONDITIONS.

**CONCRETE WASHOUT DETAIL**  
NTS

2  
13



**NOTES:**

1. WATTERS SHALL BE IN ACCORDANCE WITH OREGON DEQ SEDIMENT CONTROL BMP SC-7. INSTALL WATTERS AS SHOWN ON PLANS OR AS DIRECTED BY THE RPR.
2. SECURELY KNOT EACH END OF THE WATTER. ABUT ADJACENT WATTERS TIGHTLY, END TO END, WITHOUT OVERLAPPING THE ENDS.
3. USE A STRAIGHT BAR TO DRIVE PILOT HOLES THROUGH THE ROLL AND INTO THE SOIL.
4. DRIVE THE STAKE THROUGH THE PREPARED HOLE, AND INTO THE SOIL. LEAVE ONLY 1 OR 2 INCHES OF THE STAKE EXPOSED ABOVE THE ROLL.
5. WATTERS SHALL BE INSPECTED REGULARLY, AND IMMEDIATELY AFTER A RAINFALL THAT PRODUCES RUNOFF, TO ENSURE THAT THEY REMAIN THOROUGHLY ENTRENCHED AND IN CONTACT WITH THE SOIL.

**STRAW WATTLE DETAIL**  
NTS

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REPLACE ENTIRE SHEET



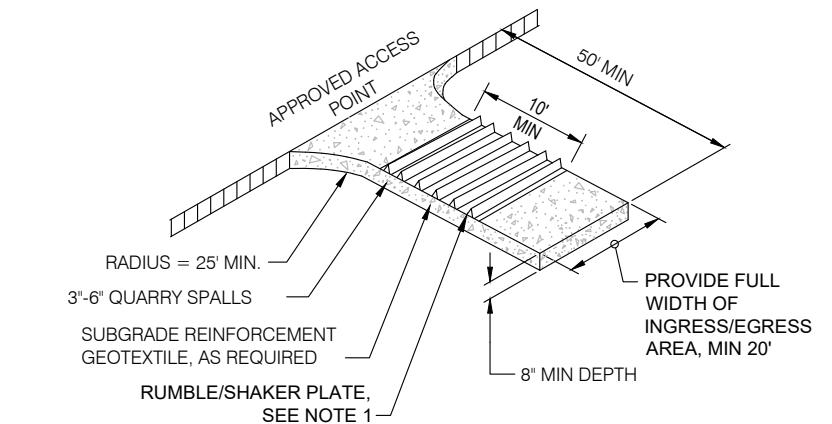
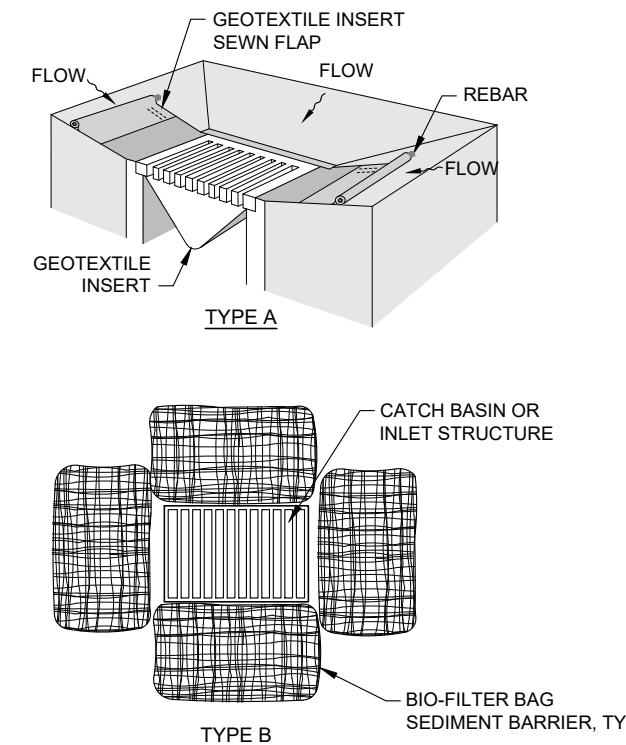
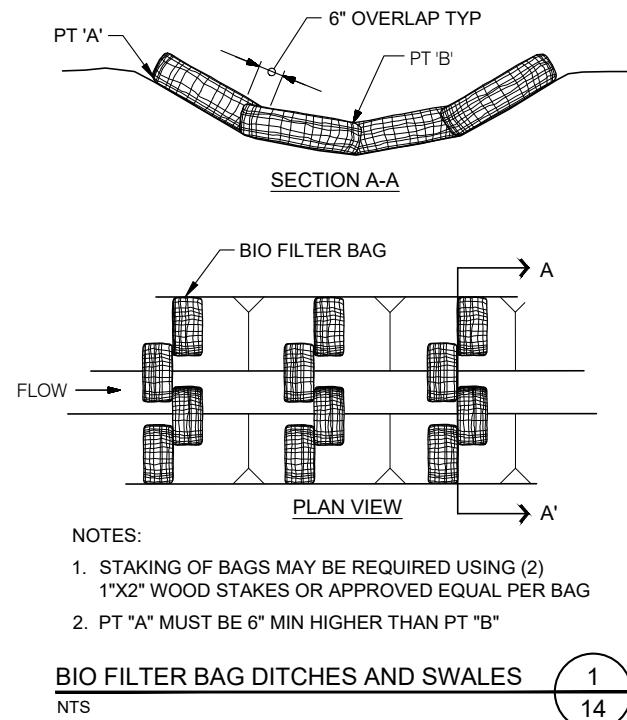
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REVISIONS:

5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

**ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES**  
**EROSION AND SEDIMENT  
CONTROL NOTES AND DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**13**  
OF **71**



REPLACE ENTIRE SHEET



1 ADDENDUM NO. 1	5/24/2024	JGL
REVISIONS:	DATE	APPD.

DATE: MAY 2024
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ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
<b>EROSION AND SEDIMENT CONTROL NOTES AND DETAILS</b>
PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063

PROJECT NUMBER: RVI-2020-W04
SHEET NO. 14
OF 71



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SCALE IN FEET

#### LEGEND

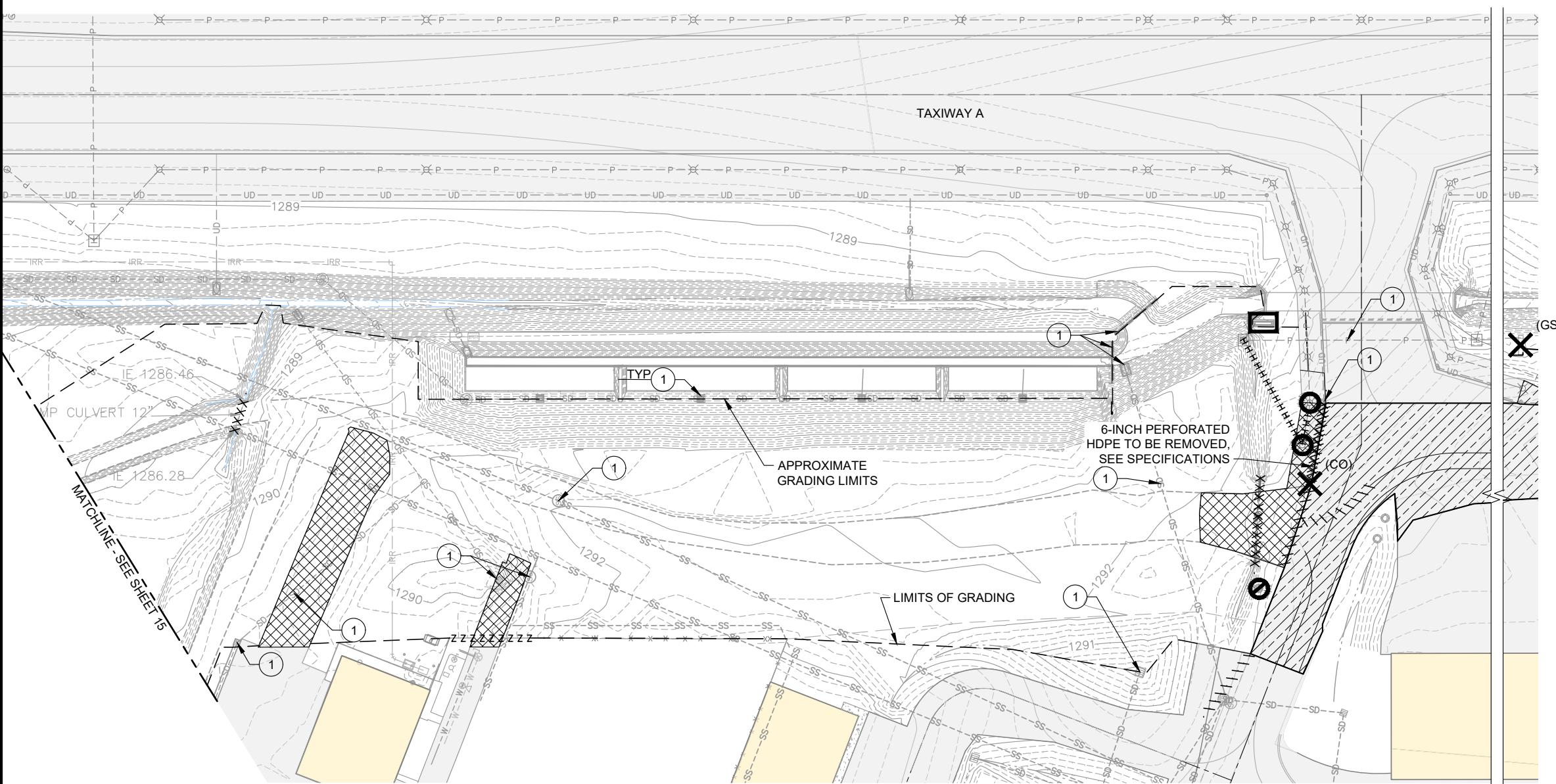
- EXISTING ASPHALT PAVEMENT
- FULL DEPTH PAVEMENT REMOVAL
- PARTIAL DEPTH PAVEMENT REMOVAL, ADDITIVE A, SEE DETAIL 4-23
- EXISTING MARKING TO BE REMOVED
- (ID) STRUCTURE TO BE REMOVED  
STRUCTURE ID LEGEND  
CO - CLEANOUT  
GS - GUIDANCE SIGN
- X LIGHT AND BASE CAN TO BE REMOVED
- O RETROREFLECTIVE MARKER REMOVAL, SEE NOTE 8
- GUIDANCE SIGN PANEL REMOVAL AND REPLACEMENT, SEE ELECTRICAL PLANS
- XXXXXX STORM DRAIN TO BE REMOVED
- HHHHHH ELECTRICAL SYSTEMS TO BE REMOVED, SEE ELECTRICAL PLANS
- ZZZZZZ FENCE TO BE REMOVED
- 1 PROTECT DURING CONSTRUCTION

#### DEMOLITION NOTES

1. LIMITS OF PAVEMENT REMOVAL ARE APPROXIMATE. CONTRACTOR SHALL LAYOUT LIMITS WITH SURVEY AND FIELD VERIFY WITH ENGINEER PRIOR TO REMOVAL. SEE LAYOUT PLAN SHEETS FOR ADDITIONAL INFORMATION.
2. ALL ASPHALT PAVEMENT TO BE REMOVED BY GRINDING UNLESS OTHERWISE APPROVED. ALL GRANULAR MATERIAL GENERATED DURING ASPHALT PAVEMENT REMOVAL SHALL BE DISPOSED OFFSITE UNLESS OTHERWISE APPROVED BY THE RPR FOR USE ON OTHER PORTIONS OF THE PROJECT. REMOVAL AND DISPOSAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF ASPHALT PAVEMENT REMOVAL, FULL OR PARTIAL DEPTH BID ITEMS.
3. ALL ELEMENTS OF DEMOLITION, UNLESS OTHERWISE NOTED, WILL BE DISPOSED OF OFFSITE AND PAID UNDER THEIR RESPECTIVE BID ITEMS.
4. DEMOLITION SHALL BE LIMITED TO APPROXIMATE WORK AREAS IDENTIFIED IN THE CONSTRUCTION PLAN SHEETS FOR EACH PHASE UNLESS OTHERWISE APPROVED BY THE RPR.
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7. PROTECT ADJACENT FEATURES TO REMAIN IN PLACE. CONTRACTOR SHALL SEAL OR PLUG ALL PIPE VOIDS WITH WATER TIGHT MATERIAL AS APPROVED BY RPR. INCIDENTAL TO DEMOLITION BID ITEM.
8. RETROREFLECTIVE MARKERS SHALL BE REMOVED AND RELOCATED, SEE MARKING AND RETROREFLECTIVE MARKER PLAN. EXCESS RETROREFLECTIVE MARKERS SHALL BE DISPOSED OF OFFSITE.
9. POROUS PAVER REMOVAL NOT SHOWN ON THIS SHEET, SEE DRAINAGE PLANS.

1 REPLACE ENTIRE SHEET





## LEGEND

EXISTING ASPHALT PAVEMENT

FULL DEPTH PAVEMENT REMOVAL

PARTIAL DEPTH PAVEMENT REMOVAL, ADDITIVE A, SEE DETAIL 4  
23

EXISTING MARKING TO BE REMOVED

 (ID) STRUCTURE TO BE REMOVED  
STRUCTURE ID LEGEND  
CO - CLEANOUT  
GS - GUIDANCE SIGN

 LIGHT AND BASE CAN TO BE REMOVED

 RETROREFLECTIVE MARKER REMOVAL, SEE NOTE 8

 GUIDANCE SIGN PANEL REMOVAL AND REPLACEMENT,  
SEE ELECTRICAL PLANS

XXXXXX · STORM DRAIN TO BE REMOVED

HHHHHH · ELECTRICAL SYSTEMS TO BE REMOVED, SEE  
ELECTRICAL PLANS

ZZZZZZ · FENCE TO BE REMOVED

 1 PROTECT DURING CONSTRUCTION

## DEMOLITION NOTES

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9. POROUS PAVER REMOVAL NOT SHOWN ON THIS SHEET, SEE DRAINAGE PLANS.



The logo for Precision Approach Engineering features the company name in a serif font. The word 'PRECISION' is on the left, 'APPROACH' is on the right, and 'ENGINEERING' is centered below them. A stylized graphic of a mountain or bridge arch is positioned between 'PRECISION' and 'APPROACH'. Below the central text is a horizontal line with three short dashes on each side of 'ENGINEERING'.



1	ADDENDUM NO. 1	5/24/2024	JGL
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DATE:	MAY 2024
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CHECKED:	JGL
REVISION NUMBER:	0
SCALE: AS SHOWN	

## ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

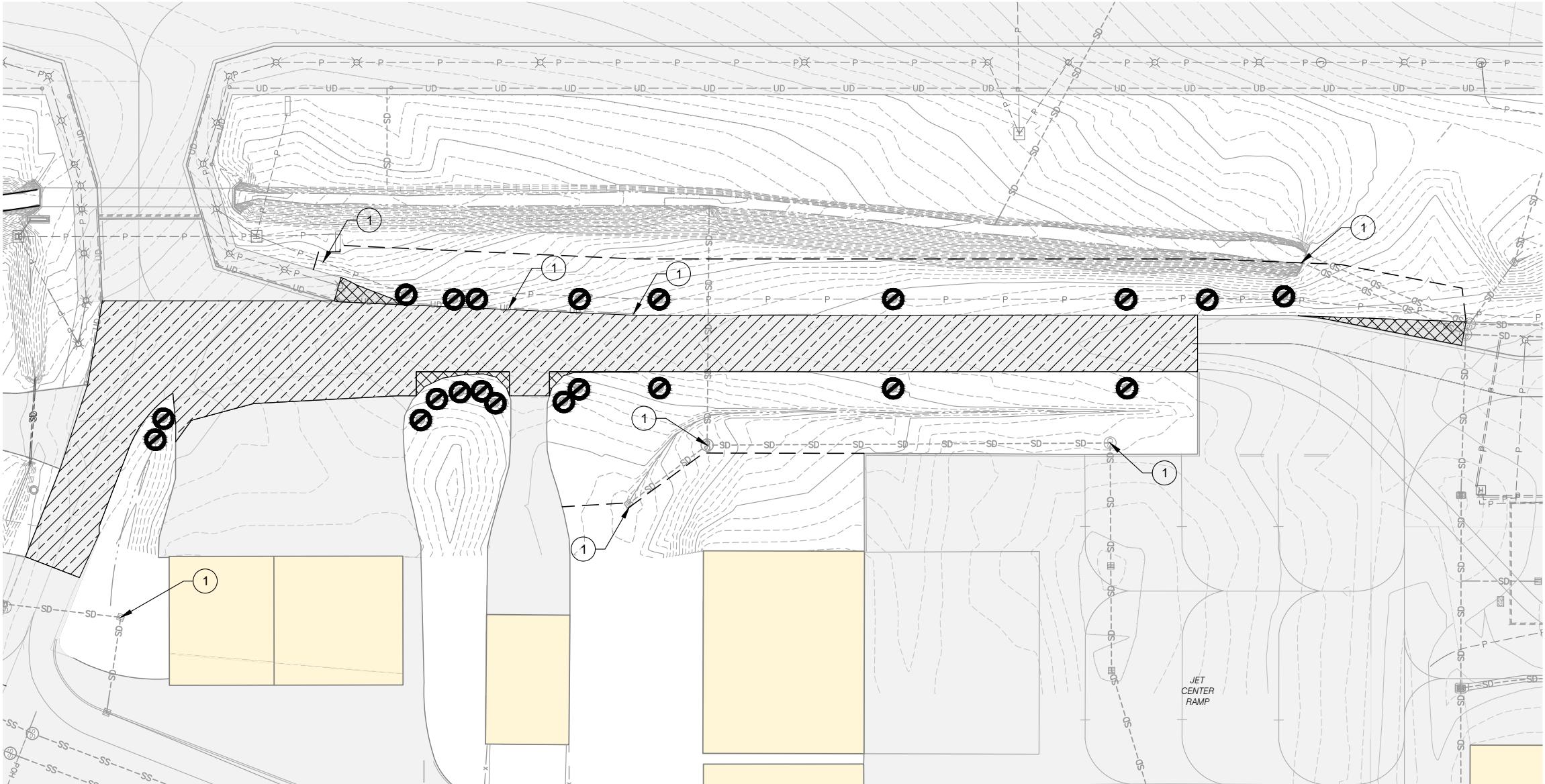
## DEMOLITION PLAN

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**

SHEET NO.  
**16**

OF **71**



SCALE IN FEET

## LEGEND

	EXISTING ASPHALT PAVEMENT
	FULL DEPTH PAVEMENT REMOVAL
	PARTIAL DEPTH PAVEMENT REMOVAL, ADDITIVE A, SEE DETAIL 
	EXISTING MARKING TO BE REMOVED
	(ID) STRUCTURE TO BE REMOVED <u>STRUCTURE ID LEGEND</u> CO - CLEANOUT GS - GUIDANCE SIGN
	LIGHT AND BASE CAN TO BE REMOVED
	RETROREFLECTIVE MARKER REMOVAL, SEE NOTE 8
	GUIDANCE SIGN PANEL REMOVAL AND REPLACEMENT, SEE ELECTRICAL PLANS
	STORM DRAIN TO BE REMOVED
	ELECTRICAL SYSTEMS TO BE REMOVED, SEE ELECTRICAL PLANS
	FENCE TO BE REMOVED
	PROTECT DURING CONSTRUCTION

## DEMOLITION NOTES

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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

## DEMOLITION PLAN - ADDITIVE

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**

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SHEET NO.

**17**

OF **71**



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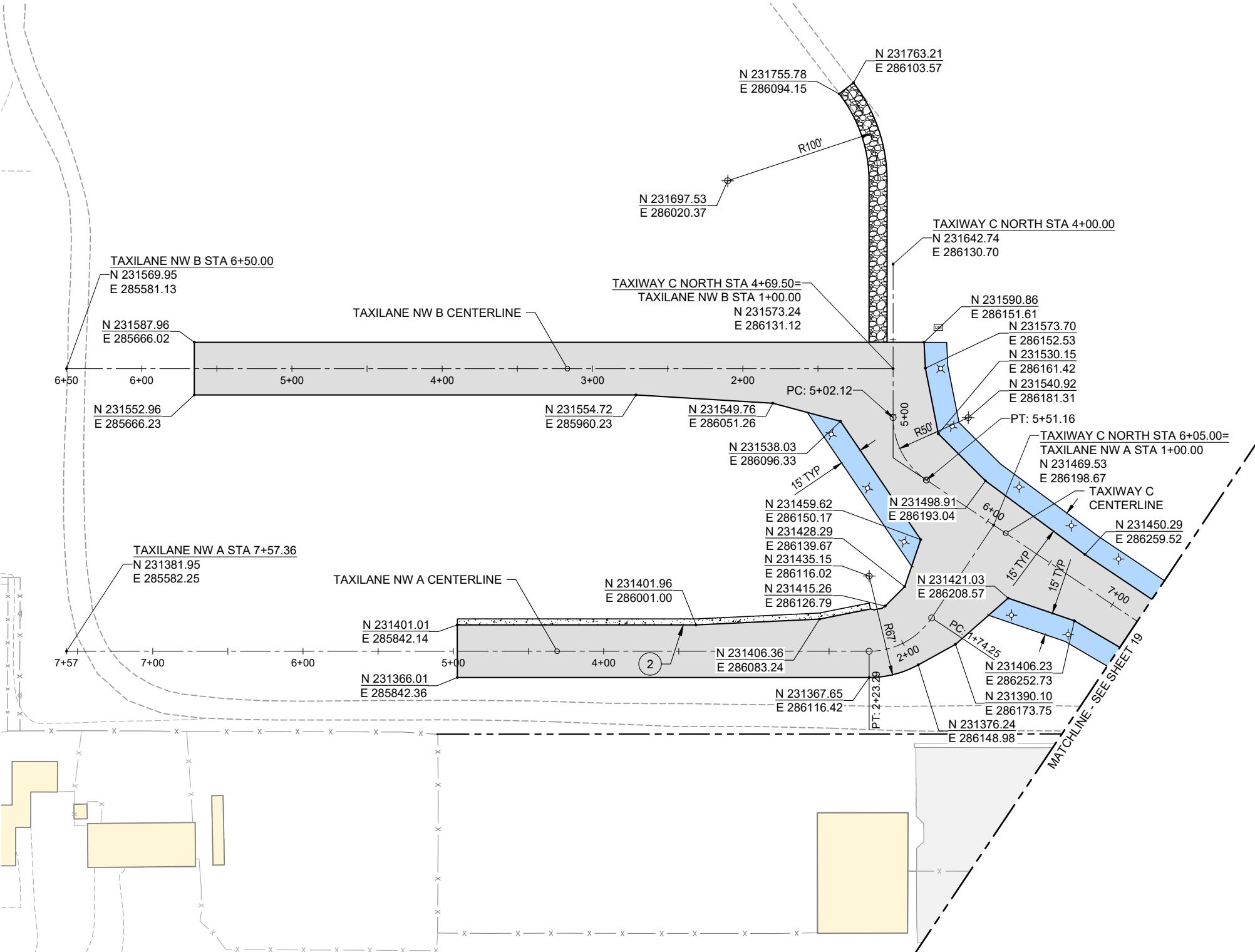
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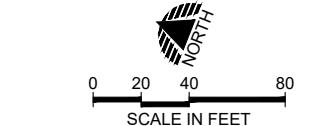
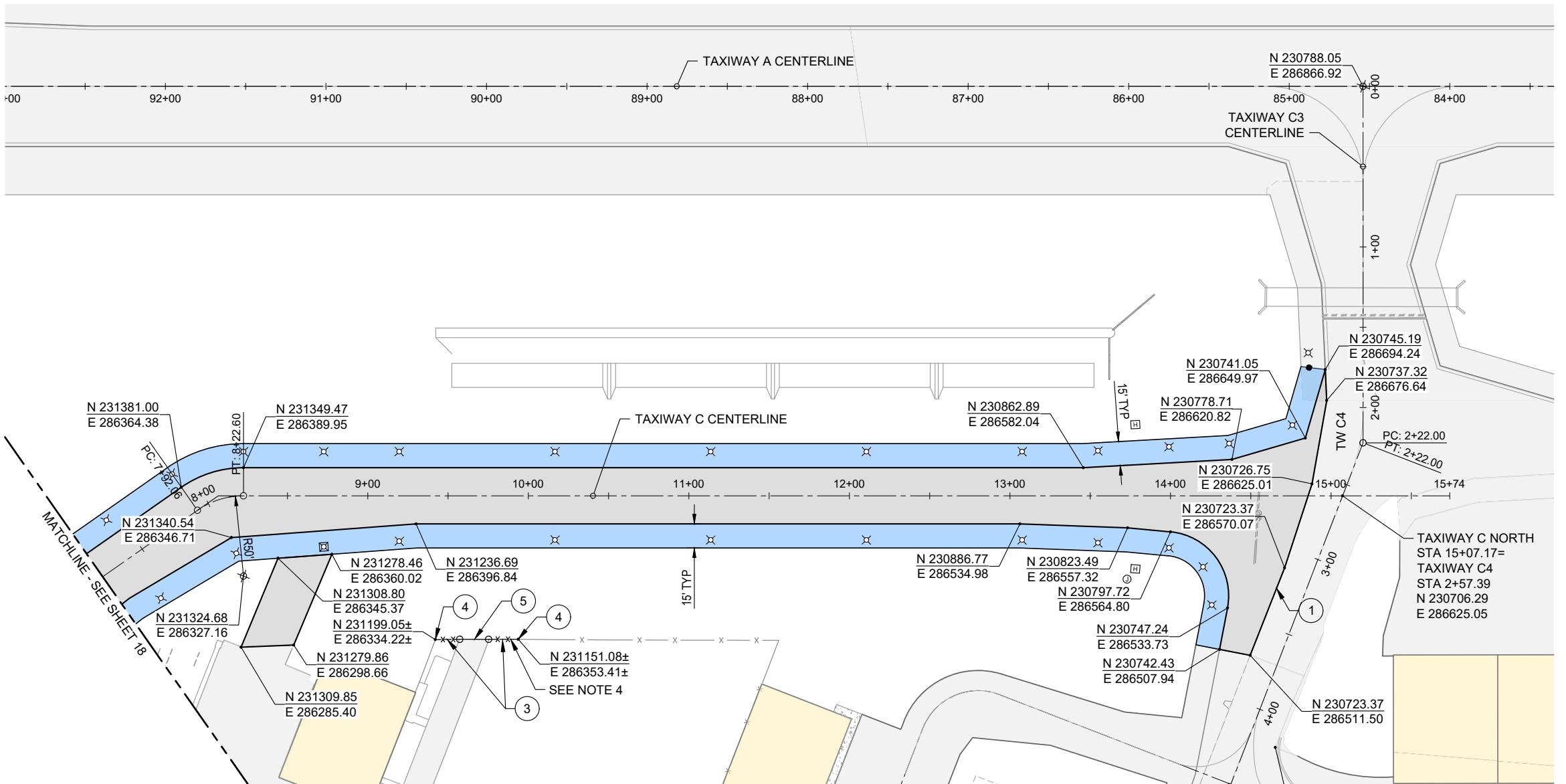
-  EXISTING ASPHALT PAVEMENT
-  FULL DEPTH ASPHALT PAVEMENT SECTION, SEE DETAIL 
-  ASPHALT OVERLAY PAVEMENT SECTION, SEE DETAIL 
-  FULL DEPTH ASPHALT SHOULDER PAVEMENT SECTION, SEE DETAIL 
-  FULL DEPTH ADDITIVE PAVEMENT SECTION, SEE DETAIL 
-  FULL DEPTH ADDITIVE SHOULDER PAVEMENT SECTION, SEE DETAIL 
-  GRAVEL ROAD SECTION, SEE DETAIL 
-  VALLEY GUTTER, SEE DETAIL 
-  1 FULL DEPTH PAVEMENT SECTION TO EXISTING PAVEMENT SECTION CONNECTION, SEE DETAIL 
-  2 FULL DEPTH PAVEMENT SECTION TO VALLEY GUTTER CONNECTION, SEE DETAIL 
-  3 INSTALL WILDLIFE FENCE, SEE DETAIL 
-  4 CONNECT TO EXISTING FENCE, SEE DETAIL 
-  5 INSTALL 18' SWING GATE, SEE DETAIL 

#### NOTES

1. SEE DEMOLITION PLAN FOR ITEMS TO BE REMOVED.
2. FOR PAVEMENT FINISH GRADE INFORMATION, SEE PAVING PLANS, PLAN AND PROFILE SHEETS, AND TAXIWAY OVERLAY TYPICAL SECTION, SHEETS 25-28 AND 31.
3. FOR NON-PAVED SHOULDER GRADING SEE GRADING AND DRAINAGE PLANS.
4. FENCE LAYOUT IS APPROXIMATE AND SHOULD INCLUDE THE 18-FOOT DOUBLE SWING GATE, CONNECTION TO THE NORTH CORNER OF THE PERIMETER FENCE, AND CONNECTION TO THE EXISTING SOUTH PERIMETER FENCE AT LEAST TWO EXISTING FENCE POSTS FROM THE NEW GATE, OR A MAXIMUM DISTANCE OF 52 LINEAR FEET FROM THE NORTH CORNER OF THE PERIMETER FENCE.

 REPLACE ENTIRE SHEET





#### LEGEND

- EXISTING ASPHALT PAVEMENT
- FULL DEPTH ASPHALT PAVEMENT SECTION, SEE DETAIL 1 22
- ASPHALT OVERLAY PAVEMENT SECTION, SEE DETAIL 4 23
- FULL DEPTH ASPHALT SHOULDER PAVEMENT SECTION, SEE DETAIL 1 22
- FULL DEPTH ADDITIVE PAVEMENT SECTION, SEE DETAIL 2 22
- FULL DEPTH ADDITIVE SHOULDER PAVEMENT SECTION, SEE DETAIL 2 22
- GRAVEL ROAD SECTION, SEE DETAIL 3 23
- VALLEY GUTTER, SEE DETAIL 2 45
- 1 FULL DEPTH PAVEMENT SECTION TO EXISTING PAVEMENT SECTION CONNECTION, SEE DETAIL 4 22
- 2 FULL DEPTH PAVEMENT SECTION TO VALLEY GUTTER CONNECTION, SEE DETAIL 2 45
- 3 INSTALL WILDLIFE FENCE, SEE DETAIL 1 69
- 4 CONNECT TO EXISTING FENCE, SEE DETAIL 1 70
- 5 INSTALL 18' SWING GATE, SEE DETAIL 1 71

#### NOTES

1. SEE DEMOLITION PLAN FOR ITEMS TO BE REMOVED.
2. FOR PAVEMENT FINISH GRADE INFORMATION, SEE PAVING PLANS, PLAN AND PROFILE SHEETS, AND TAXIWAY OVERLAY TYPICAL SECTION, SHEETS 25-28 AND 31.
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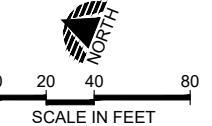
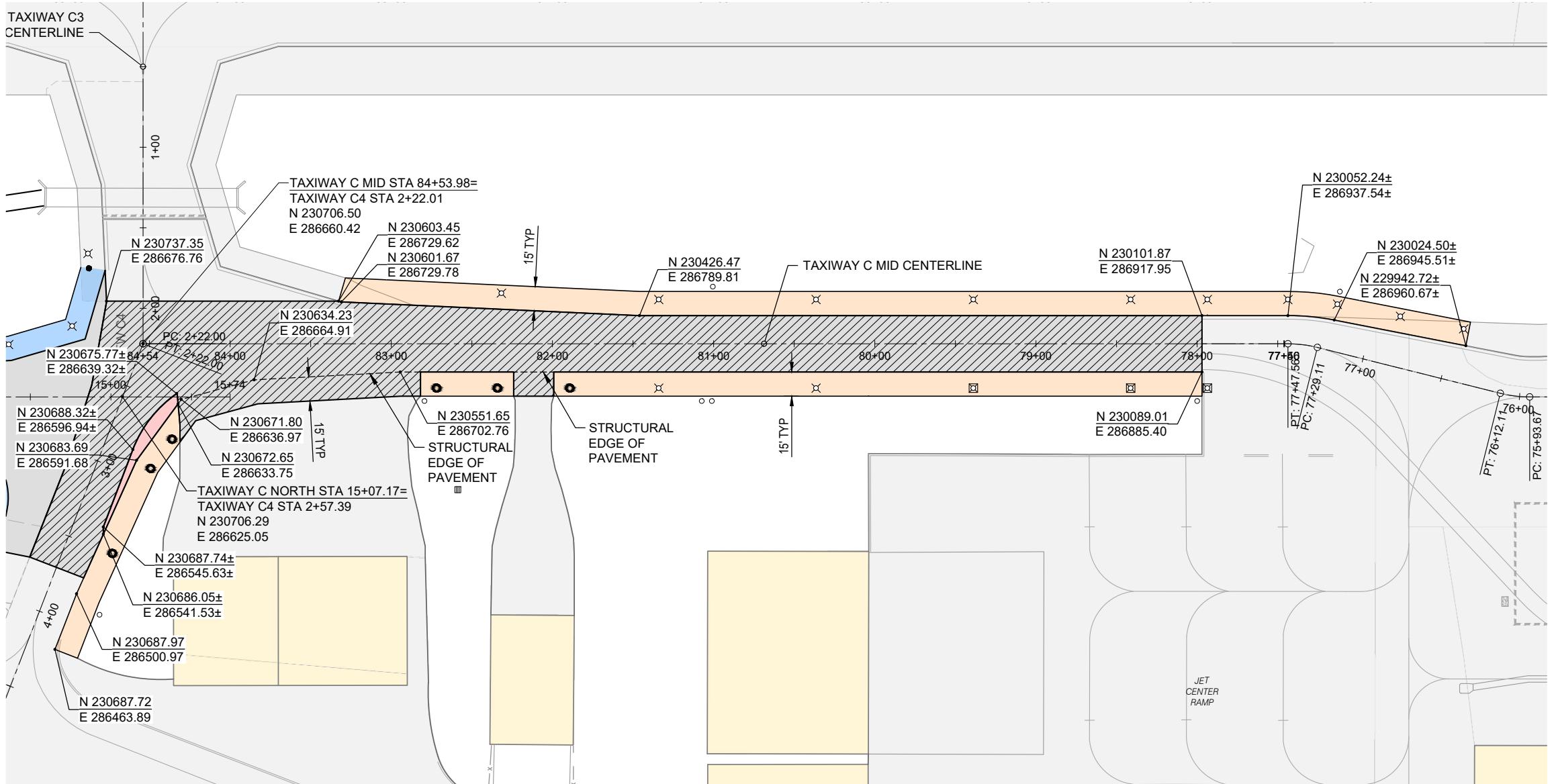


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DATE: MAY 2024  
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
 TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**LAYOUT PLAN**  
 PRECISION APPROACH ENGINEERING, INC.  
 AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
 SHEET NO.  
**19**  
 OF **71**



#### LEGEND

- EXISTING ASPHALT PAVEMENT
- FULL DEPTH ASPHALT PAVEMENT SECTION, SEE DETAIL 1  
22
- ASPHALT OVERLAY PAVEMENT SECTION, SEE DETAIL 4  
23
- FULL DEPTH ASPHALT SHOULDER PAVEMENT SECTION, SEE DETAIL 1  
22
- FULL DEPTH ADDITIVE PAVEMENT SECTION, SEE DETAIL 2  
22
- FULL DEPTH ADDITIVE SHOULDER PAVEMENT SECTION, SEE DETAIL 2  
22
- GRAVEL ROAD SECTION, SEE DETAIL 3  
23
- VALLEY GUTTER, SEE DETAIL 2  
45
- 1 FULL DEPTH PAVEMENT SECTION TO EXISTING PAVEMENT SECTION CONNECTION, SEE DETAIL 4  
22
- 2 FULL DEPTH PAVEMENT SECTION TO VALLEY GUTTER CONNECTION, SEE DETAIL 2  
45
- 3 INSTALL WILDLIFE FENCE, SEE DETAIL 1  
69
- 4 CONNECT TO EXISTING FENCE, SEE DETAIL 1  
70
- 5 INSTALL 18' SWING GATE, SEE DETAIL 1  
71

#### NOTES

1. SEE DEMOLITION PLAN FOR ITEMS TO BE REMOVED.
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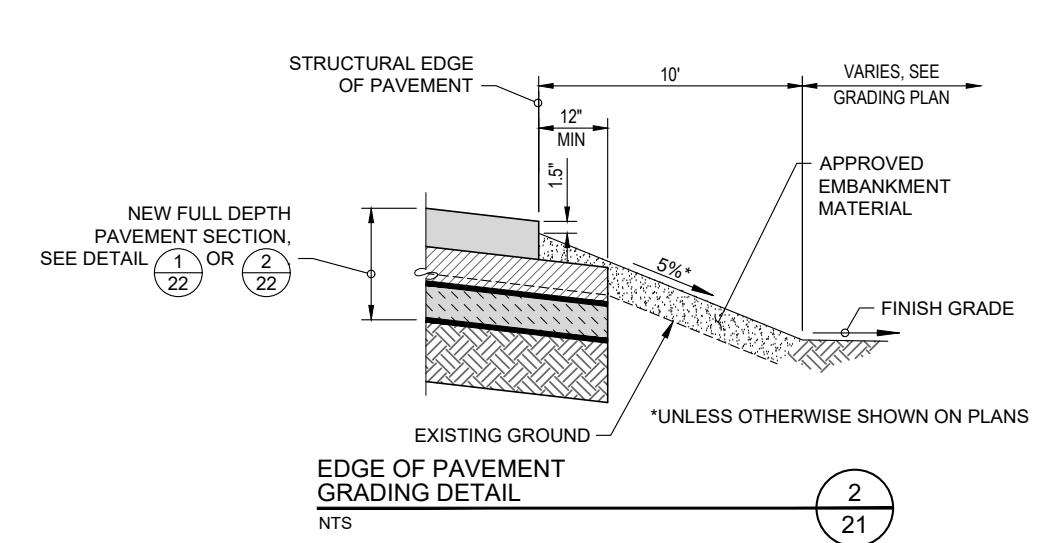
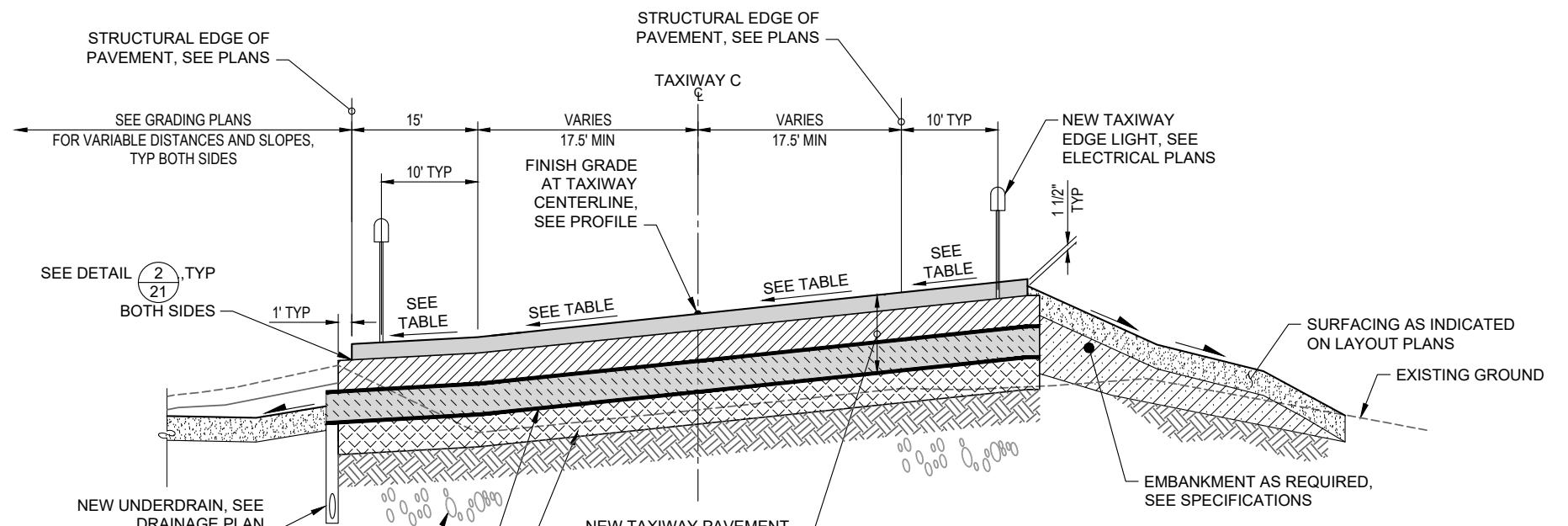


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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**LAYOUT PLAN - ADDITIVE**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**20**  
OF **71**



TAXIWAY A1 PAVEMENT SLOPE TABLE

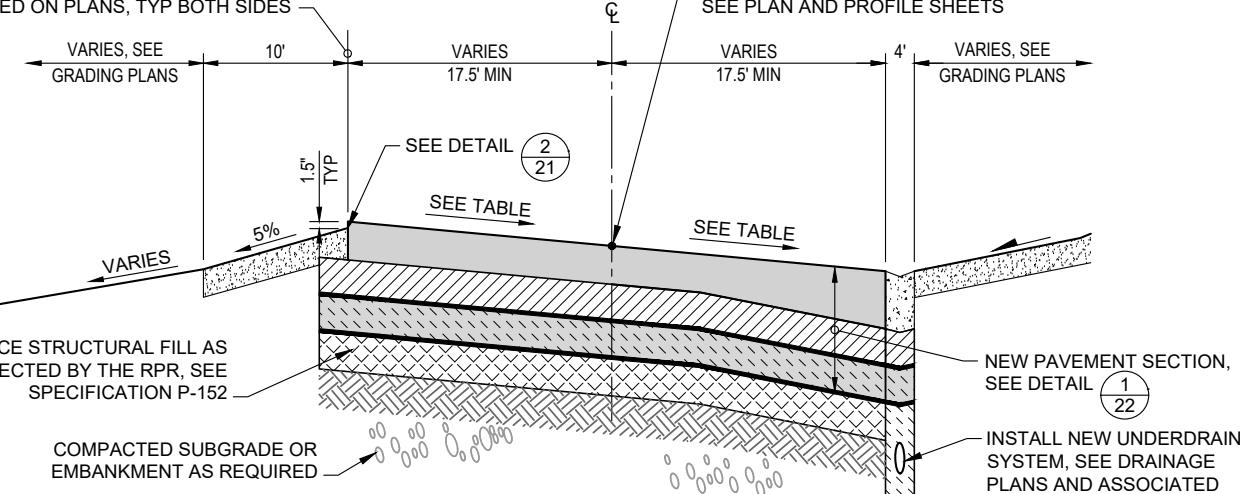
LEFT SLOPE (PERCENT GRADE)	RIGHT SLOPE (PERCENT GRADE)	SHOULDER SLOPES	BEGIN STATION	END STATION
1.0%	1.0%	1.5%	4+52	13+75
TRANSITION *	TRANSITION *	TRANSITION *	13+75	14+85.70

NOTE:  
\* SURFACE GRADES VARY, SEE PLAN AND PROFILES

TAXIWAY C FULL DEPTH PAVEMENT TYPICAL SECTION

NTS 1 21

STRUCTURAL EDGE OF PAVEMENT AS IDENTIFIED ON PLANS, TYP BOTH SIDES



TAXIWAY A1 PAVEMENT SLOPE TABLE

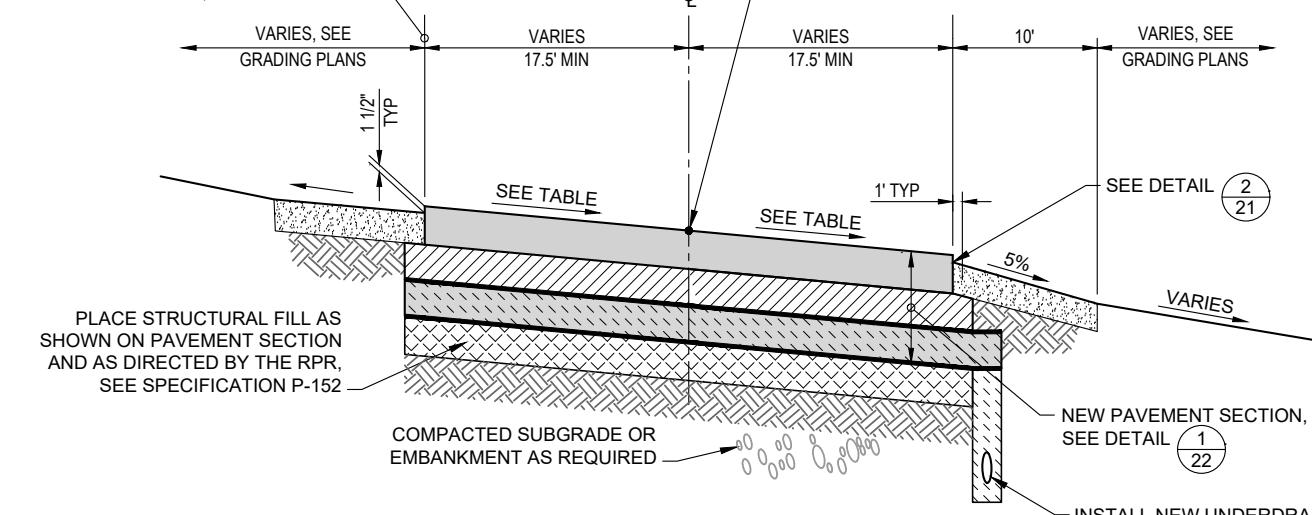
LEFT SLOPE (PERCENT GRADE)	RIGHT SLOPE (PERCENT GRADE)	BEGIN STATION	END STATION
TRANSITION *	TRANSITION *	1+35	2+23.29
1.0%	1.0%	2+23.29	4+97.36

NOTE:  
\* SURFACE GRADES VARY, SEE PLAN AND PROFILES

TAXIWAY NW A TYPICAL SECTION

NTS 3 21

STRUCTURAL EDGE OF PAVEMENT AS IDENTIFIED ON PLANS, TYP BOTH SIDES



TAXILANE NW B PAVEMENT SLOPE TABLE

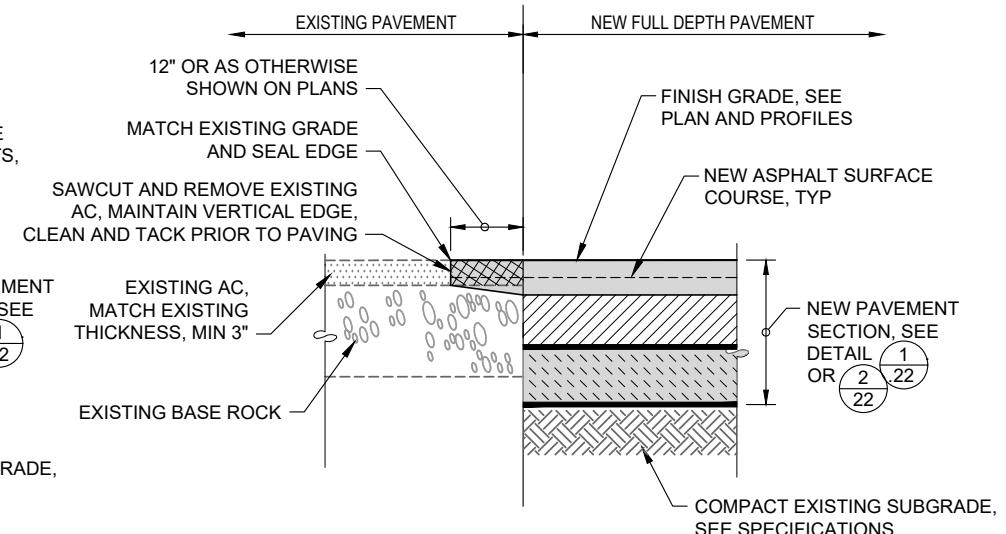
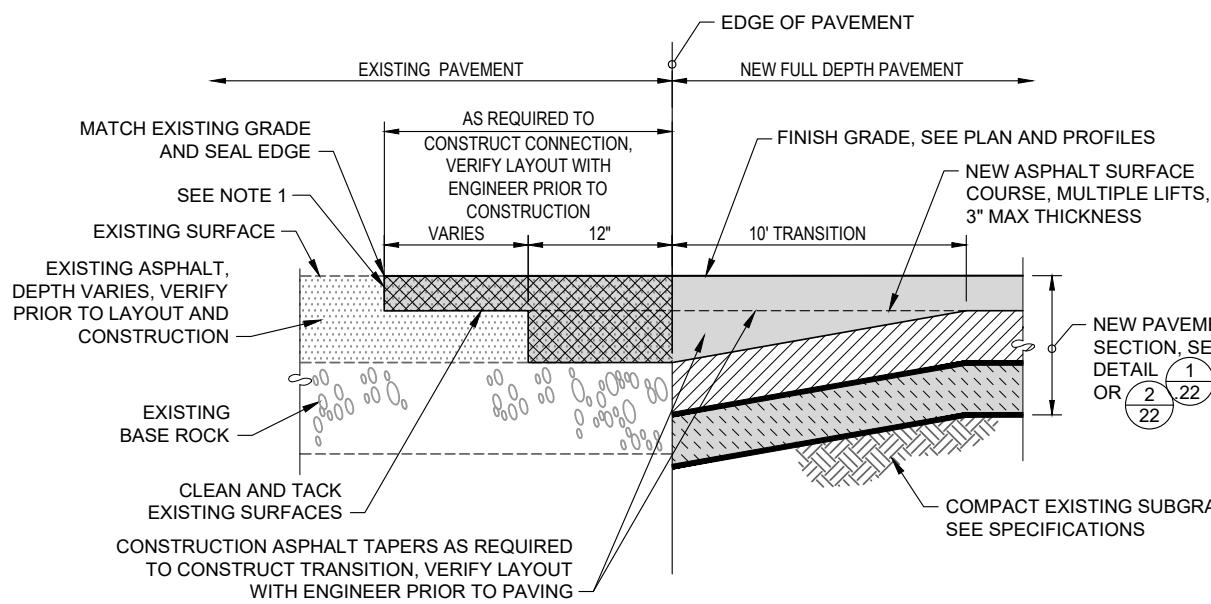
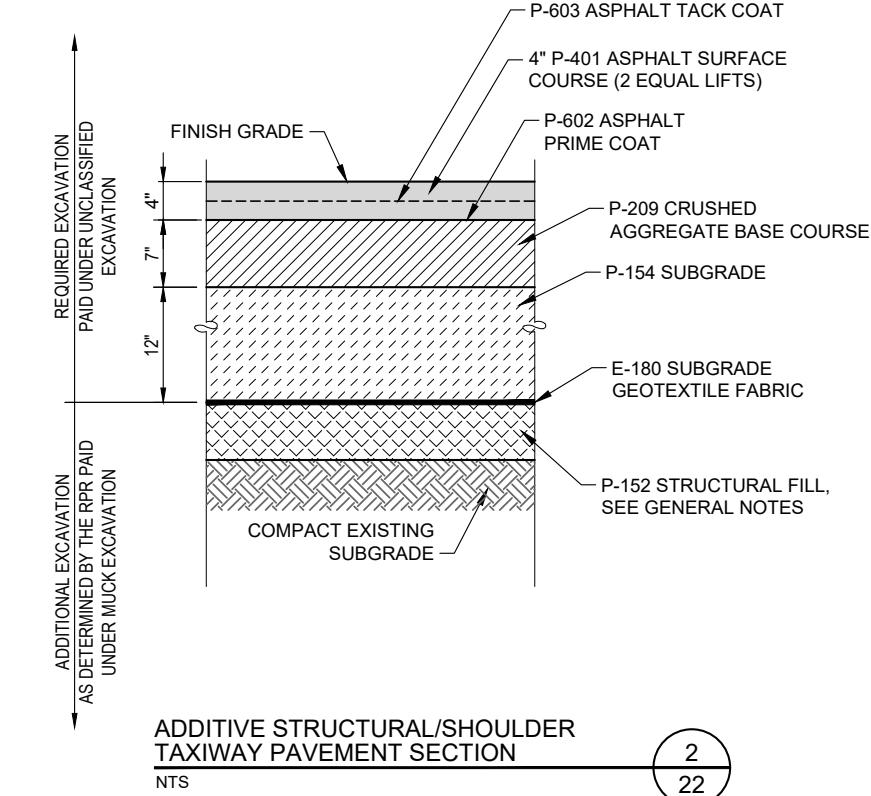
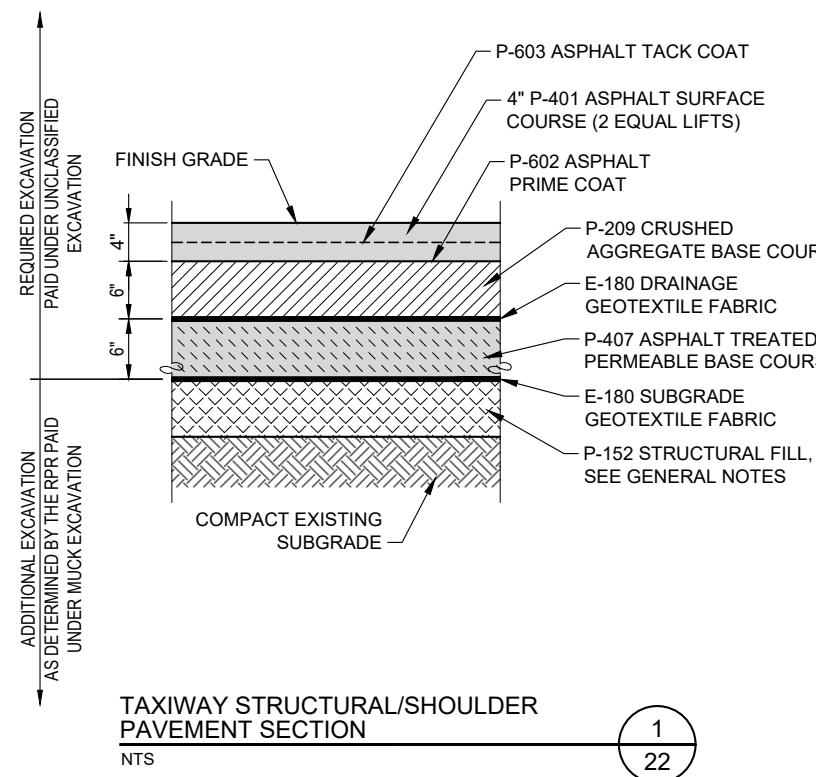
LEFT SLOPE (PERCENT GRADE)	RIGHT SLOPE (PERCENT GRADE)	BEGIN STATION	END STATION
1.0%	1.0%	1+35	3+90
*	*	3+90	5+65

NOTE:  
\* SURFACE GRADES VARY, SEE PLAN AND PROFILES

TAXIWAY NW B TYPICAL SECTION

NTS 4 21





NOTES:  
1. SAWCUT AND REMOVE EXISTING ASPHALT, MAINTAIN VERTICAL EDGE, CLEAN AND TACK PRIOR TO PAVING, TYP ALL NOTCHES.

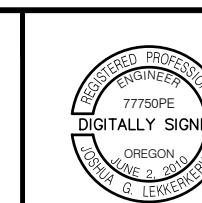
FULL DEPTH PAVEMENT SECTION TO EXISTING PAVEMENT CONNECTION DETAIL  
NTS

3  
22

GENERAL NOTES:

1. UPON REQUEST OF THE RPR, THE CONTRACTOR SHALL ALLOW FOR VISUAL INSPECTION OF THE SUBGRADE MATERIAL BY THE RPR PRIOR TO COMPACTIVE EFFORT TO DETERMINE SUBGRADE MATERIAL SUITABILITY.
2. ANY EXCAVATED MATERIAL BELOW BOTTOM COURSE OF THE PAVEMENT SECTION SHALL BE PAID FOR UNDER THE MUCK EXCAVATION BID ITEM.

REPLACE ENTIRE SHEET

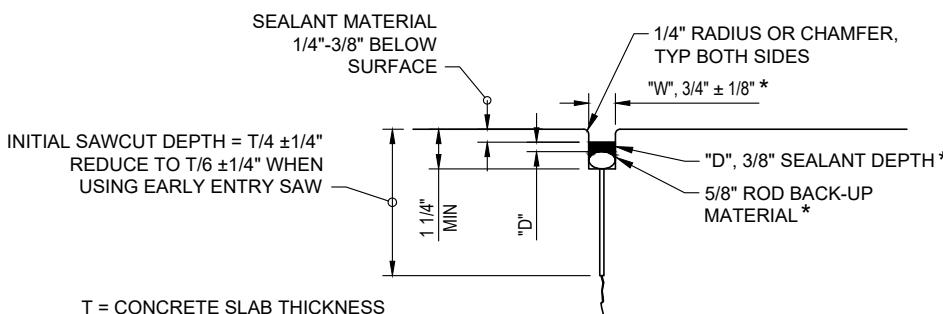


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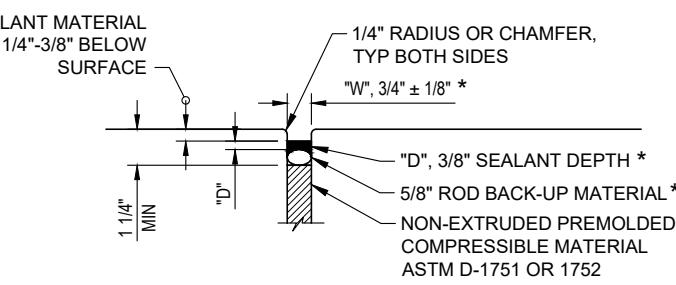
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**PAVEMENT DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**22**  
OF 71



\* SEE NOTES  
**CONTRACTION JOINT**



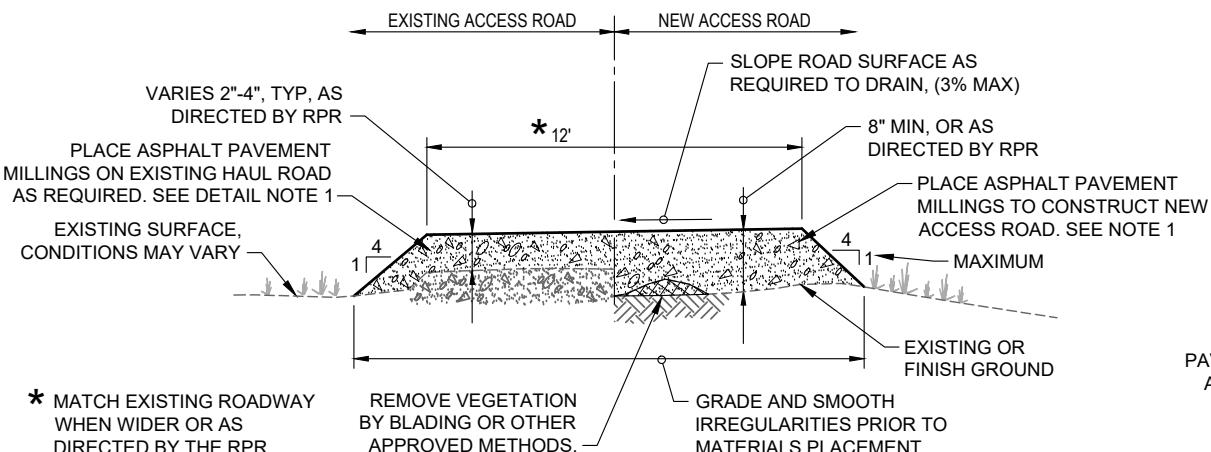
\* SEE NOTES  
**ISOLATION/ EXPANSION JOINT**

**NOTES:**

1. CONTRACTOR TO SAWCUT NEW CONCRETE TO PROVIDE RESERVOIR FOR SEALANT MEETING ASTM D-5893. CLEAN AND DRY ALL SURFACES PRIOR TO FILLING, TYP.
2. SEALANT RESERVOIR SHALL BE TO PROVIDE PROPER SHAPE FACTOR. "W" AND "D" DIMENSIONS MAY VARY PER MANUFACTURERS RECOMMENDATIONS TO PROVIDE OPTIMUM PERFORMANCE. ENGINEERS REVIEW AND APPROVAL OF SEALANT RESERVOIR REQUIRED PRIOR TO CONSTRUCTION.
3. ROD BACK-UP MATERIAL MUST BE COMPATIBLE WITH THE TYPE OF LIQUID SEALANT USED AND SIZED TO PROVIDE THE DESIRED SHAPE FACTOR. ROD BACK-UP MATERIAL SIZE MAY BE ADJUSTED TO MAINTAIN MINIMUM DEPTH, VERIFY WITH RPR.
4. ALL JOINTS AND JOINT MATERIALS SHALL BE CONSIDERED INCIDENTAL TO OTHER BID ITEMS.

**CONCRETE JOINT SEALANT  
INSTALLATION DETAIL**

NTS

1  
23

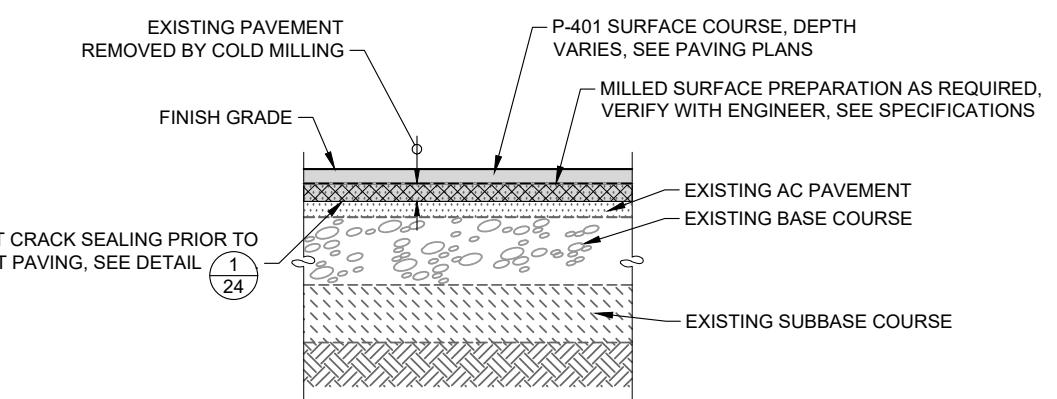
\* MATCH EXISTING ROADWAY  
WHEN WIDER OR AS  
DIRECTED BY THE RPR.

**ACCESS ROAD NOTES:**

1. GRADE SMOOTH AND COMPACT TO A DENSE UNYIELDING CONDITION AS DIRECTED BY THE RPR.
2. PRIOR TO PLACEMENT OF ANY GRINDINGS ON NEW OR EXISTING ACCESS ROADS, THE CONTRACTOR SHALL VERIFY ALL LOCATIONS WITH THE RPR.
3. UPON COMPLETION OF WORK, CONTRACTOR SHALL REGRADE ACCESS ROAD AND RECOMPACT TO DENSE UNYIELDING CONDITION, VERIFY WITH RPR.
4. ALL WORK AND MATERIALS REQUIRED TO PLACE ASPHALT GRINDINGS SHALL BE CONSIDERED INCIDENTAL TO THE ASPHALT PAVEMENT REMOVAL BID ITEMS.

**CONSTRUCTION ACCESS  
ROAD TYPICAL SECTION**

NTS

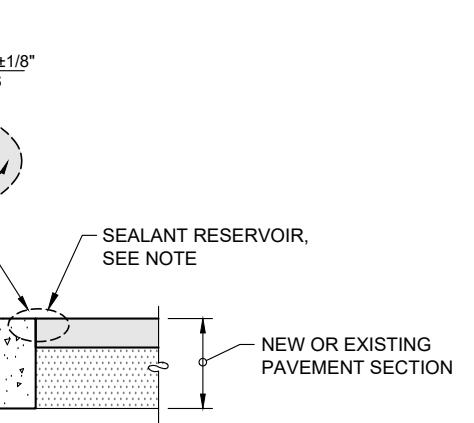
3  
23

**NOTES:**

1. MINIMUM LIFT THICKNESSES FOR P-401 SHALL BE 2 INCHES. THINNER LIFTS MAY BE REQUIRED AND SHALL BE CONSIDERED PRE-LEVEL, SEE SPECIFICATIONS.

**TAXIWAY OVERLAY PAVEMENT SECTION**

NTS

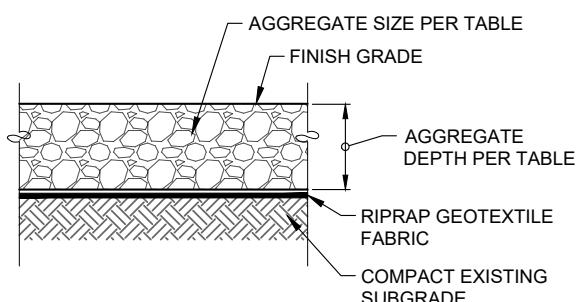
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23

**NOTES:**

1. CONTRACTOR TO SAWCUT ASPHALT PAVEMENT TO PROVIDE RESERVOIR FOR SEALANT. SEALANT SHALL BE SELF-LEVELING AND AVIATION FUEL RESISTANT. CLEAN AND DRY ALL SURFACES PRIOR TO FILLING, TYPICAL. DETAIL FOR USE WHERE NEW ASPHALT MEETS NEW OR EXISTING CONCRETE.
2. ALL JOINTS AND JOINT MATERIALS SHALL BE CONSIDERED INCIDENTAL TO OTHER BID ITEMS.

**LONGITUDINAL JOINT SEAL DETAIL**

NTS

2  
23

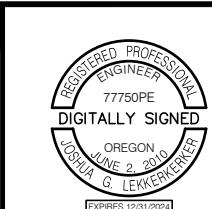
BID ITEM	AGGREGATE	DEPTH OF SECTION *
QUARRY SPALL PROTECTION	1"-4" QUARRY SPALLS	6 INCHES
CLASS 50 RIRRAP PROTECTION	ODOT CLASS 50 RIRRAP	12 INCHES
CLASS 100 RIRRAP PROTECTION	ODOT CLASS 100 RIRRAP	18 INCHES

\* UNLESS OTHERWISE SHOWN ON PLANS

**EROSION PROTECTION DETAIL**  
NTS

5  
23

REPLACE ENTIRE SHEET

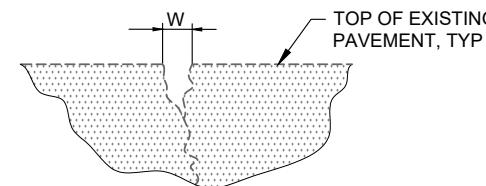


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REVISIONS:	DATE	APPD.

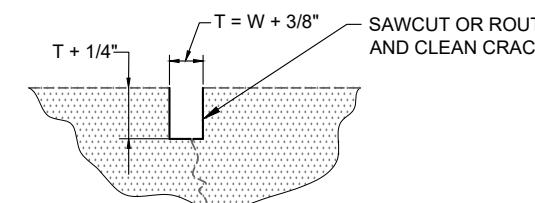
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REVISION NUMBER: 0	SCALE: AS SHOWN

**PAVEMENT DETAILS**  
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

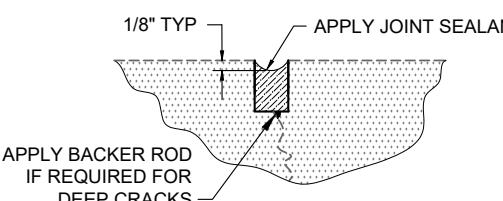
PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**23**  
OF  
**71**



EXISTING CONDITION



STEP 1



STEP 2

## NOTES:

1. CRACKS TO BE SEALED WILL BE DETERMINED BY RPR.
2. FRESHLY LAID SEALANT ABOVE SURFACE LEVEL SHALL BE REMOVED PRIOR TO OVERLAY. EXCESS SEALANT ADJACENT TO CRACK SHALL BE REMOVED BY GRINDING OR BY METHOD DEMONSTRATED TO REMOVE SEALANT WITHOUT SIGNIFICANT DISTRESS TO THE PAVEMENT.

## PAVEMENT CRACK SEALING DETAIL

NTS

1  
24

REPLACE ENTIRE SHEET

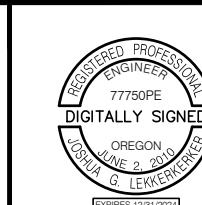
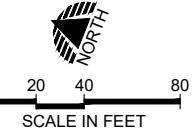
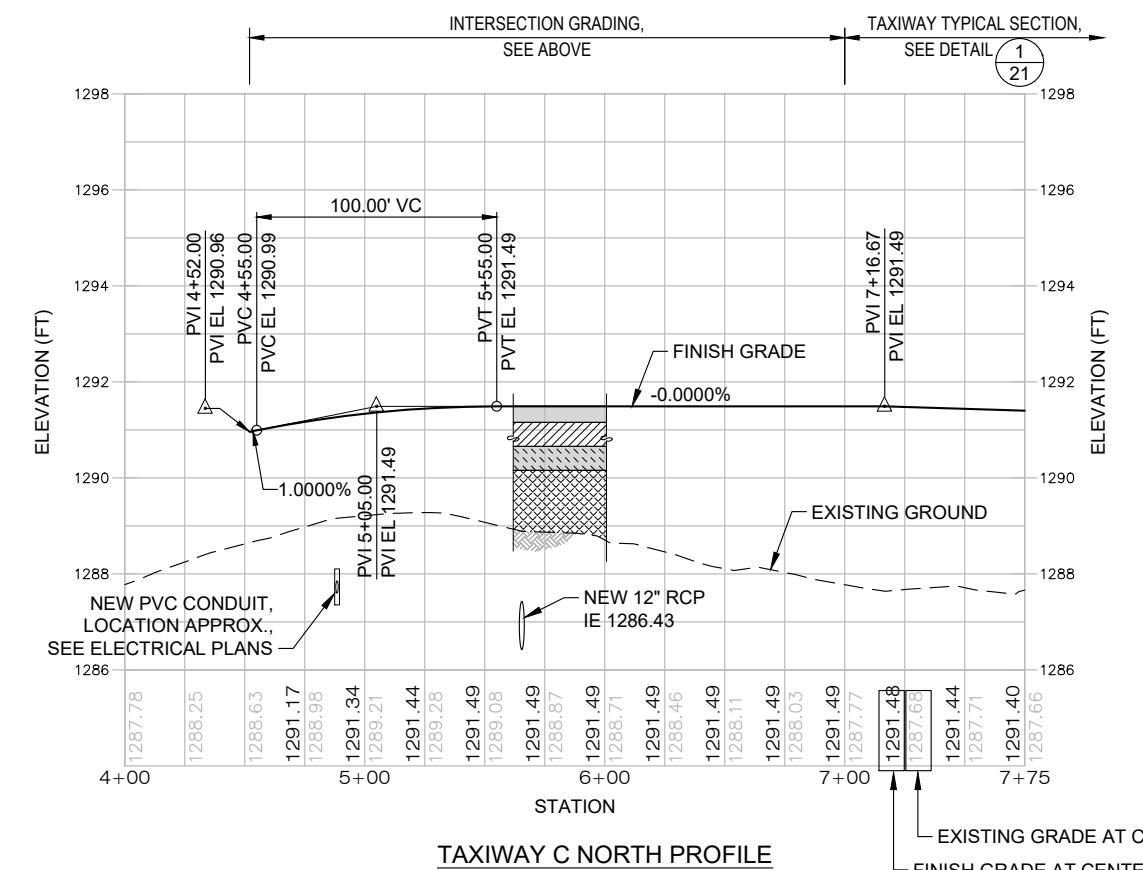
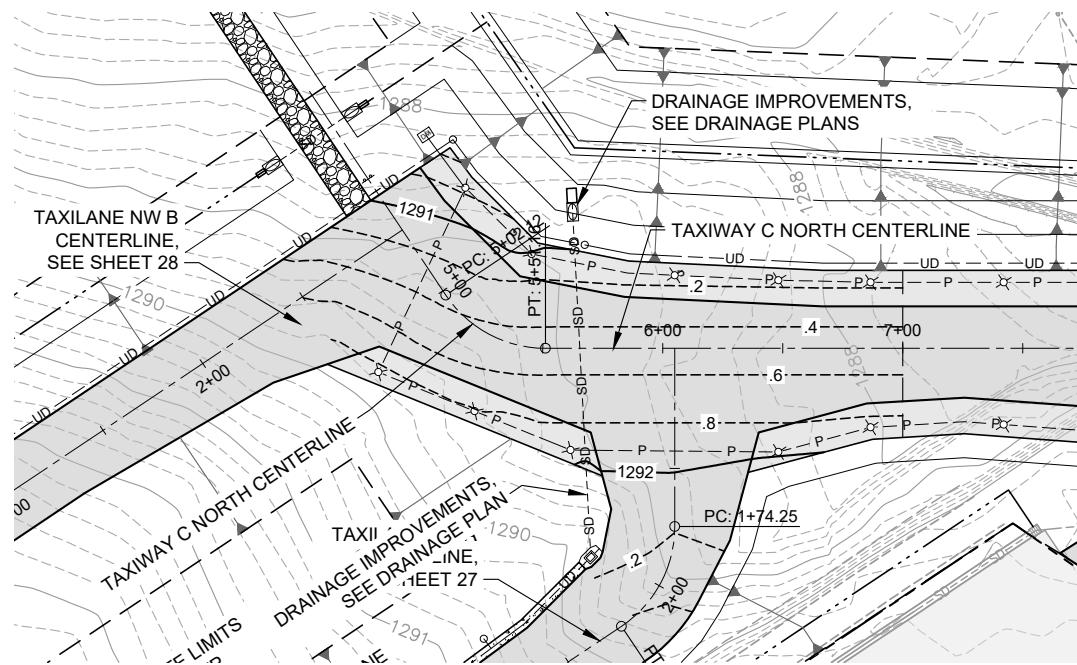


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DATE: MAY 2024	DESIGN: KJK
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SCALE: AS SHOWN	

ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES	PAVEMENT DETAILS
PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063	

PROJECT NUMBER: RVI-2020-W04	SHEET NO. 24
OF 71	



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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

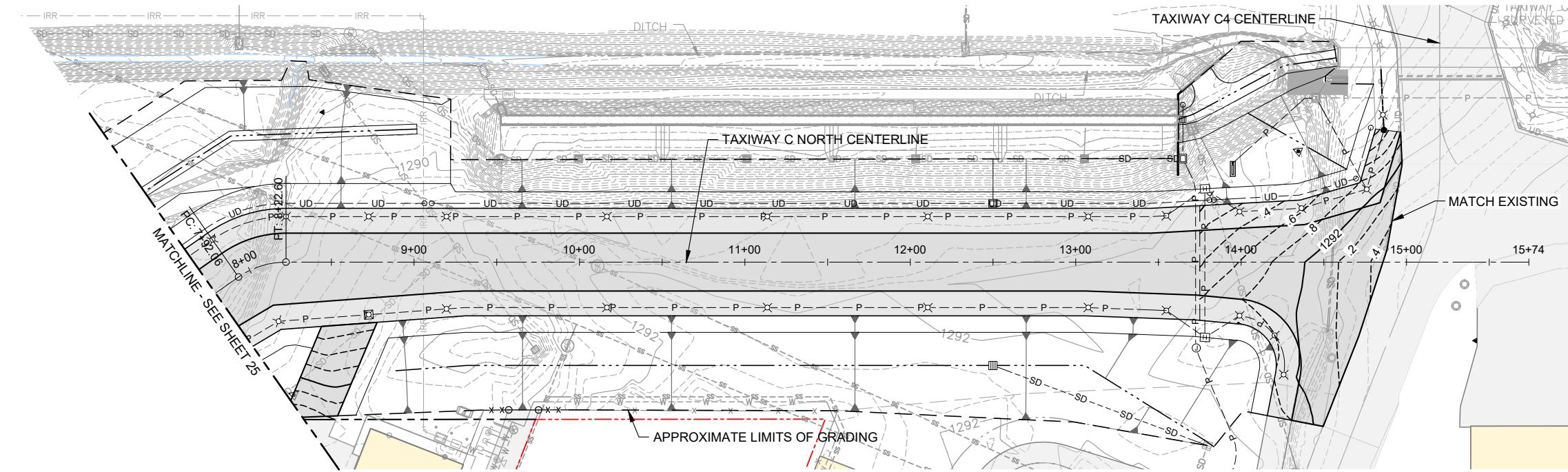
# **PLAN AND PROFILE - TAXIWAY C NORTH**

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

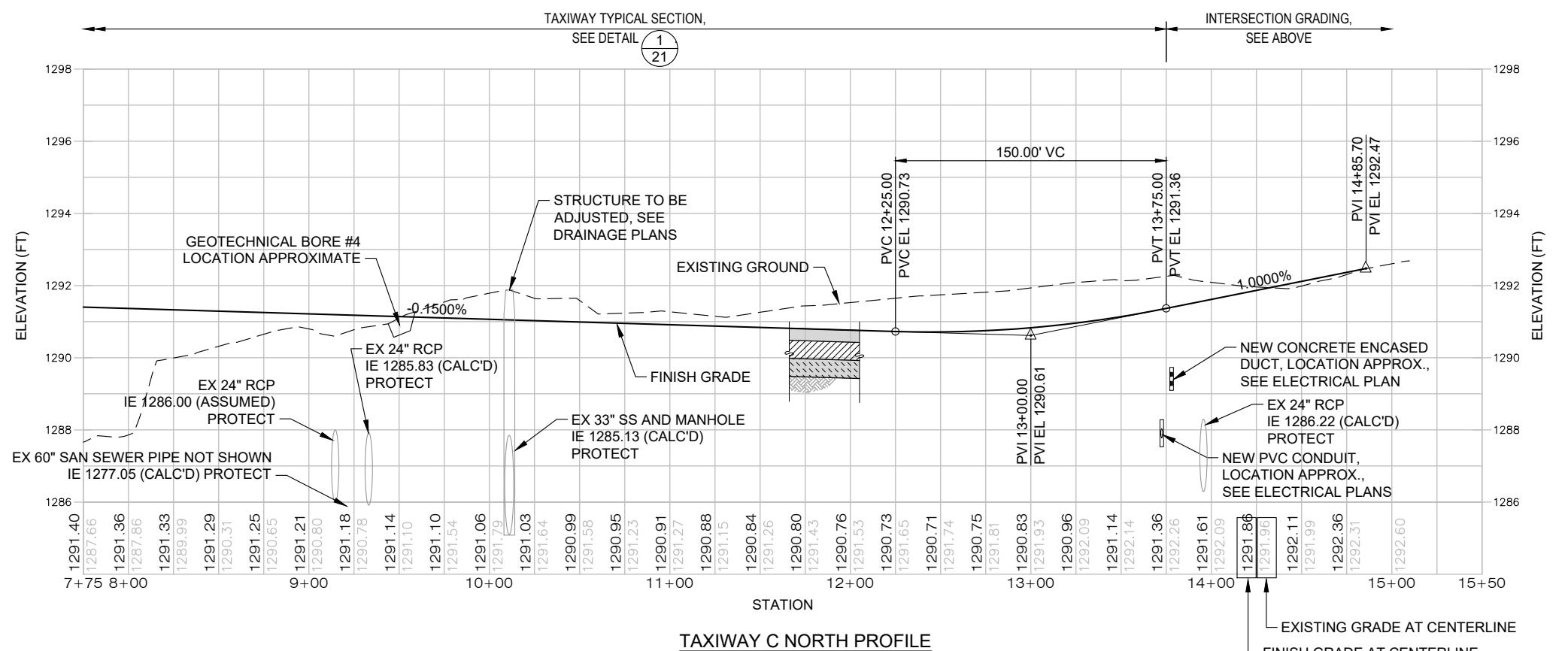
PROJECT NUMBER:  
**RFI-2020-W04**

SHEET NO.  
**25**

OF **71**



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PRECISION APPROACH  
ENGINEERING  
5125 Southwest Hout Street  
Corvallis, OR 97333  
541-754-0043



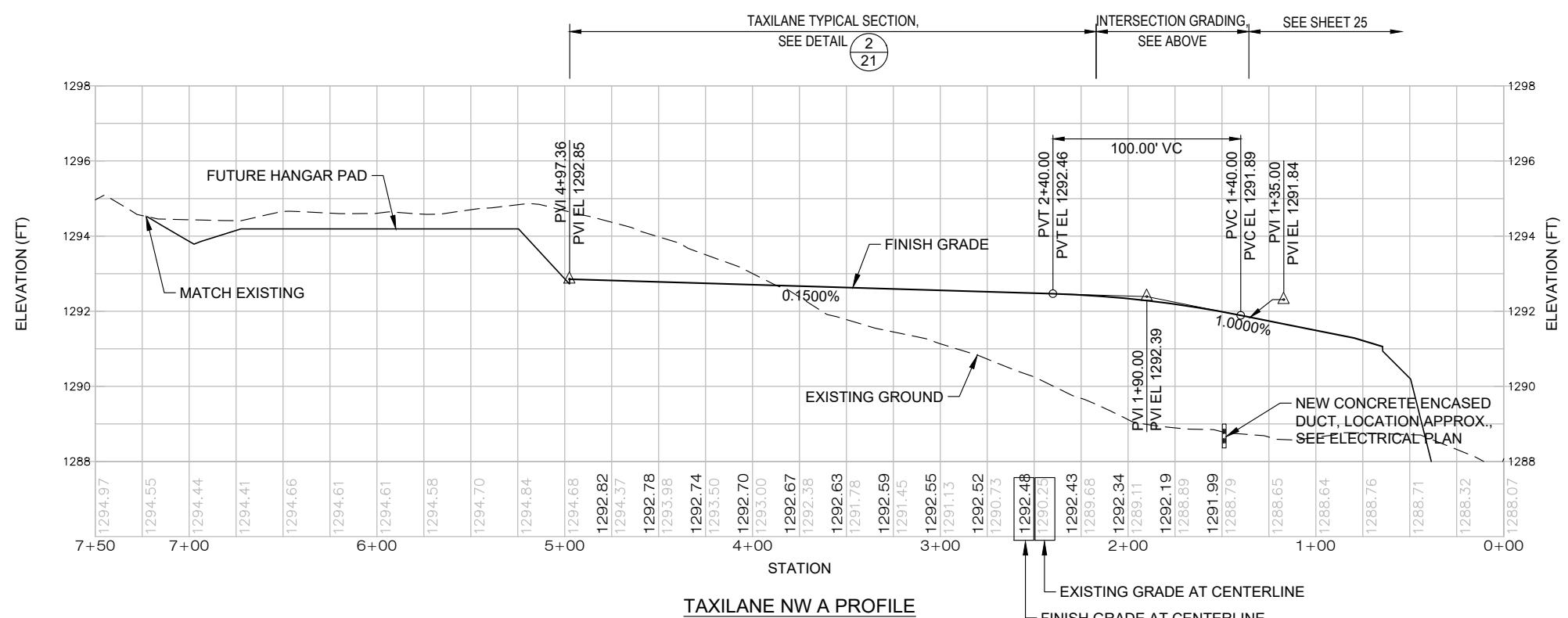
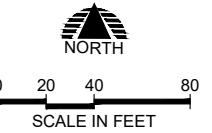
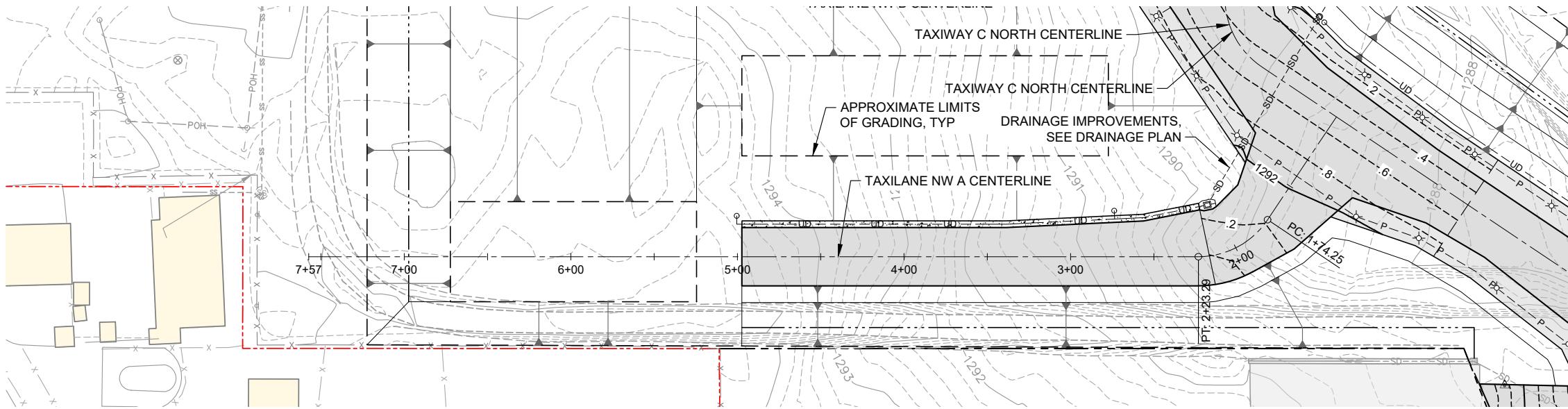
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REVISIONS:

5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
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REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
PLAN AND PROFILE - TAXIWAY C  
NORTH  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
RVI-2020-W04  
SHEET NO.  
26  
OF 71



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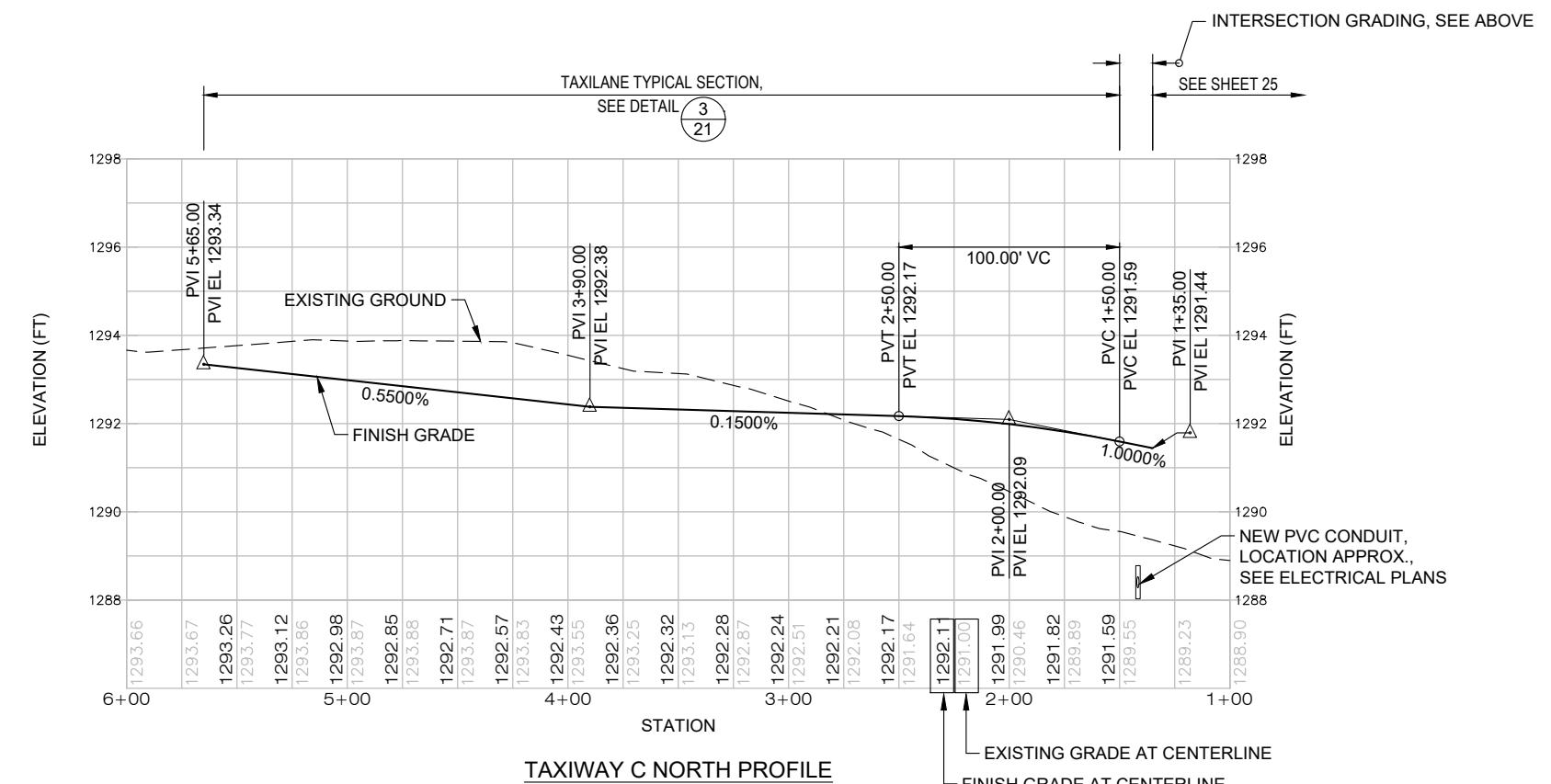
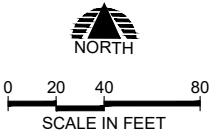
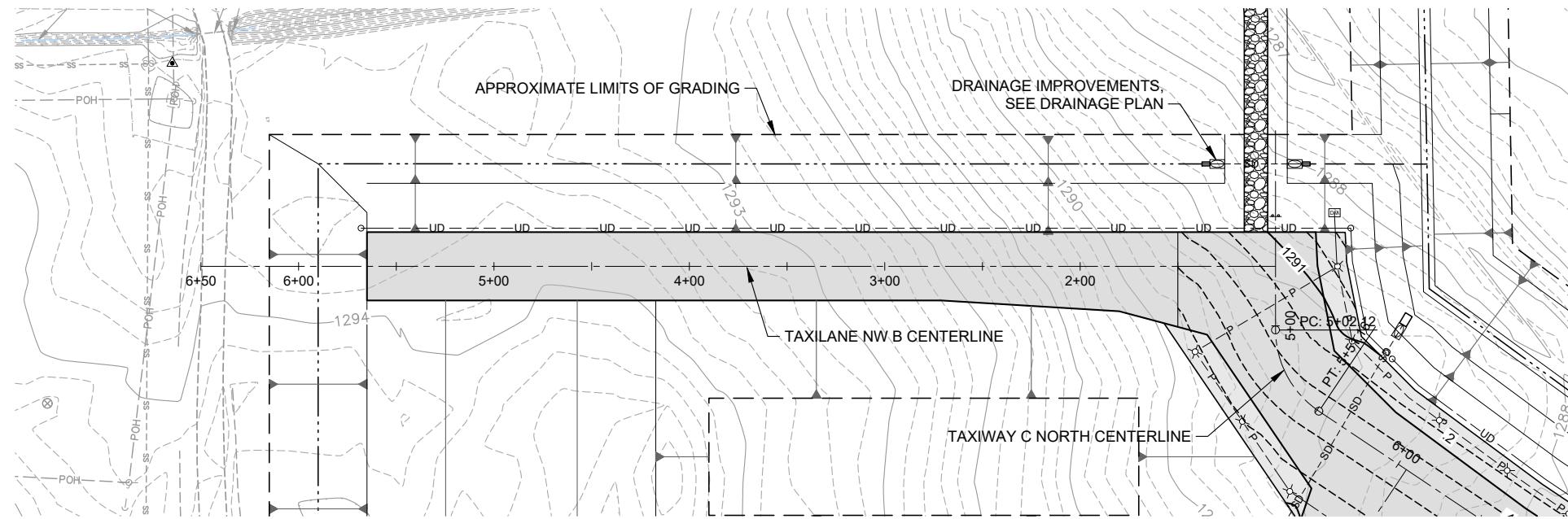


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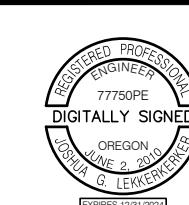
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**PLAN AND PROFILE - TAXILANE A NWA**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV\2020-W04**  
SHEET NO.  
**27**  
OF **71**



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REVISIONS:

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DATE APPD.

DATE: MAY 2024  
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**PLAN AND PROFILE - TAXILANE B NWA**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV\2020-W04**  
SHEET NO.  
**28**  
OF **71**



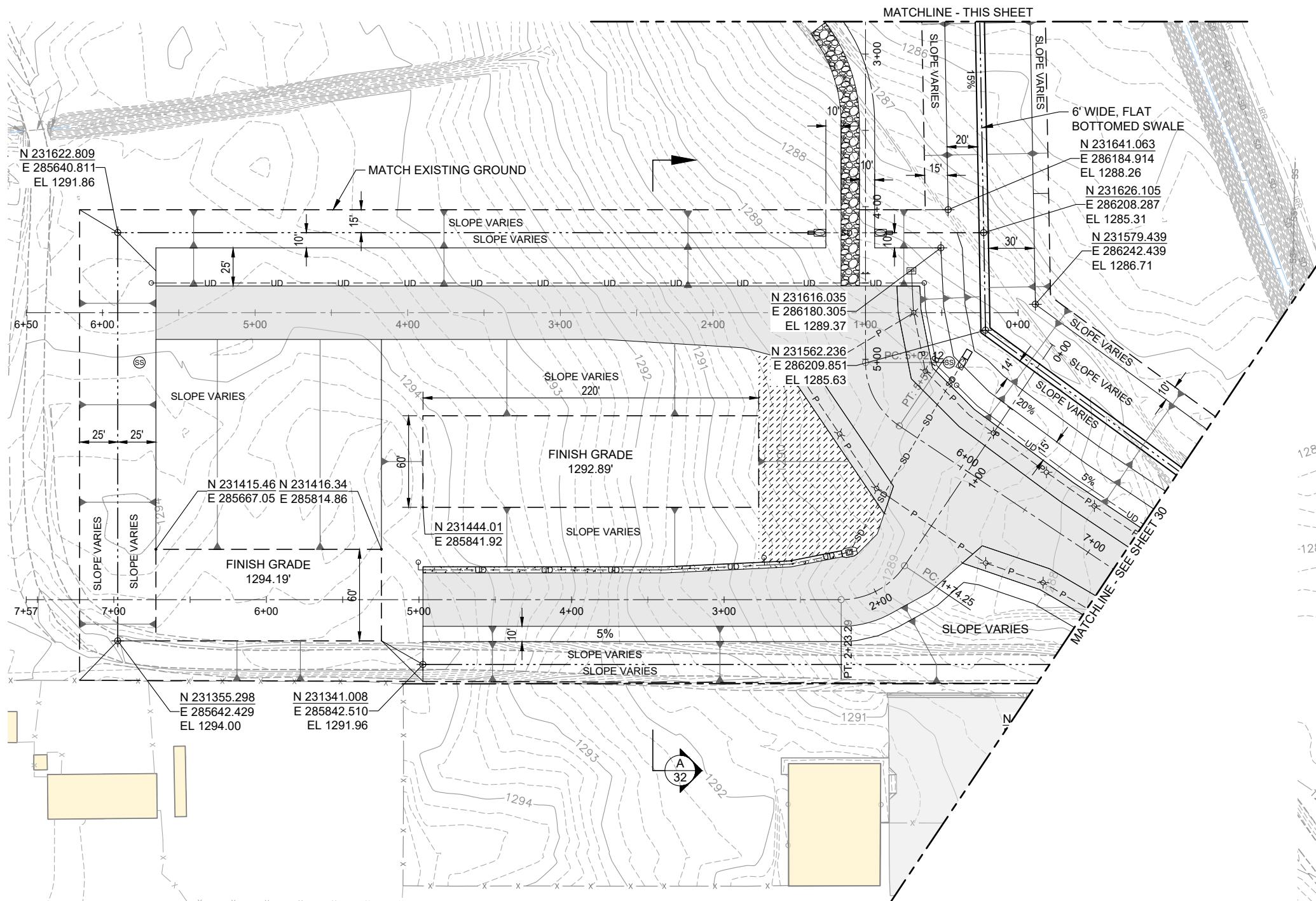
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LEGEND

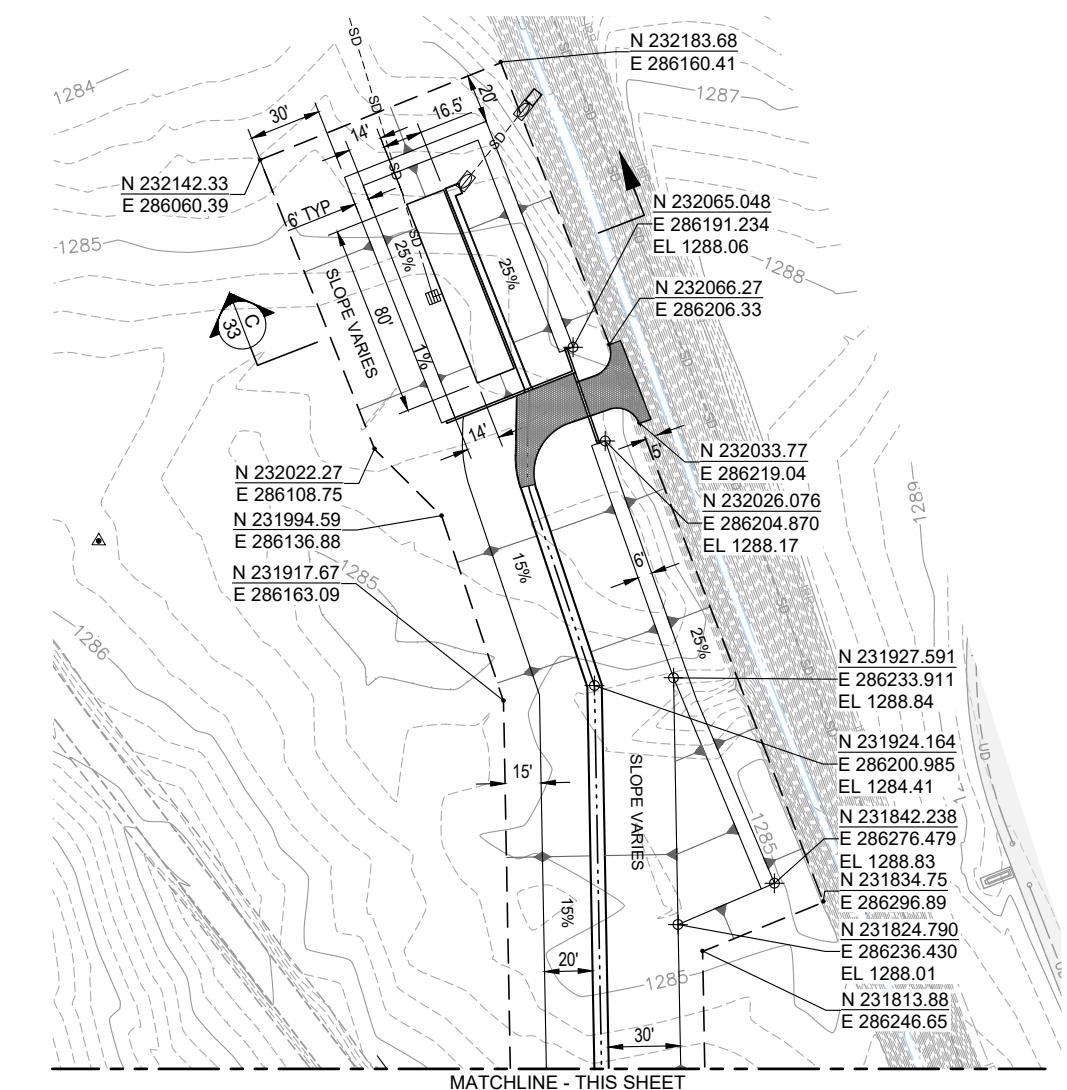
	RELOCATED AIRPORT OPERATIONS ROADWAY
	ASPHALT GRINDINGS, 6" MINIMUM DEPTH
	CLASS 100 RIPRAP, SEE DETAIL

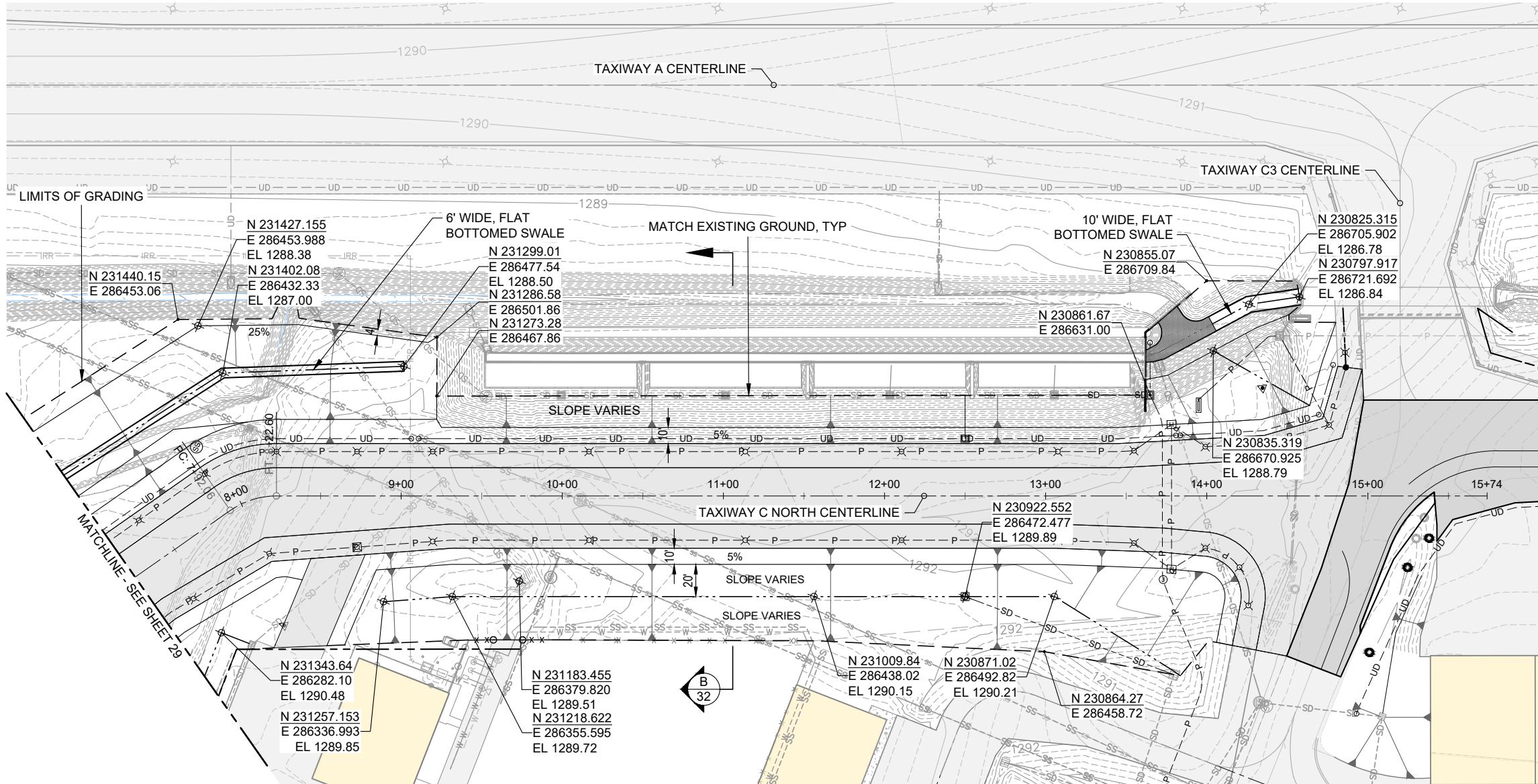
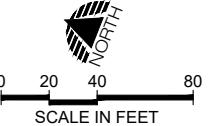
NOTES:

1. CONSTRUCT A 1.5" DROP AT THE EDGE OF PAVEMENT TO PREVENT MIGRATION OF SEDIMENT ONTO PAVED AREAS, TYPICAL.
2. AREAS WITHIN LIMITS OF GRADING WITH EXISTING TOPSOIL SHALL BE STRIPPED OF ALL VEGETATION AND TOPSOIL, MINIMUM 6" DEPTH. TOPSOIL SHALL BE STOCKPILED FOR PLACEMENT AFTER ALL EXCAVATION AND EMBANKMENT CONSTRUCTION IS FINISHED. STOCKPILED TOPSOIL SHALL BE UTILIZED ON SITE OR DISPOSED OF OFFSITE, INCIDENTAL, MEASUREMENT AND PAYMENT SHALL BE FOR NEAT LINE QUANTITY OF TOPSOIL IN ITS ORIGINAL CONDITION, SEE SPECIFICATIONS.

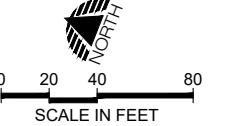
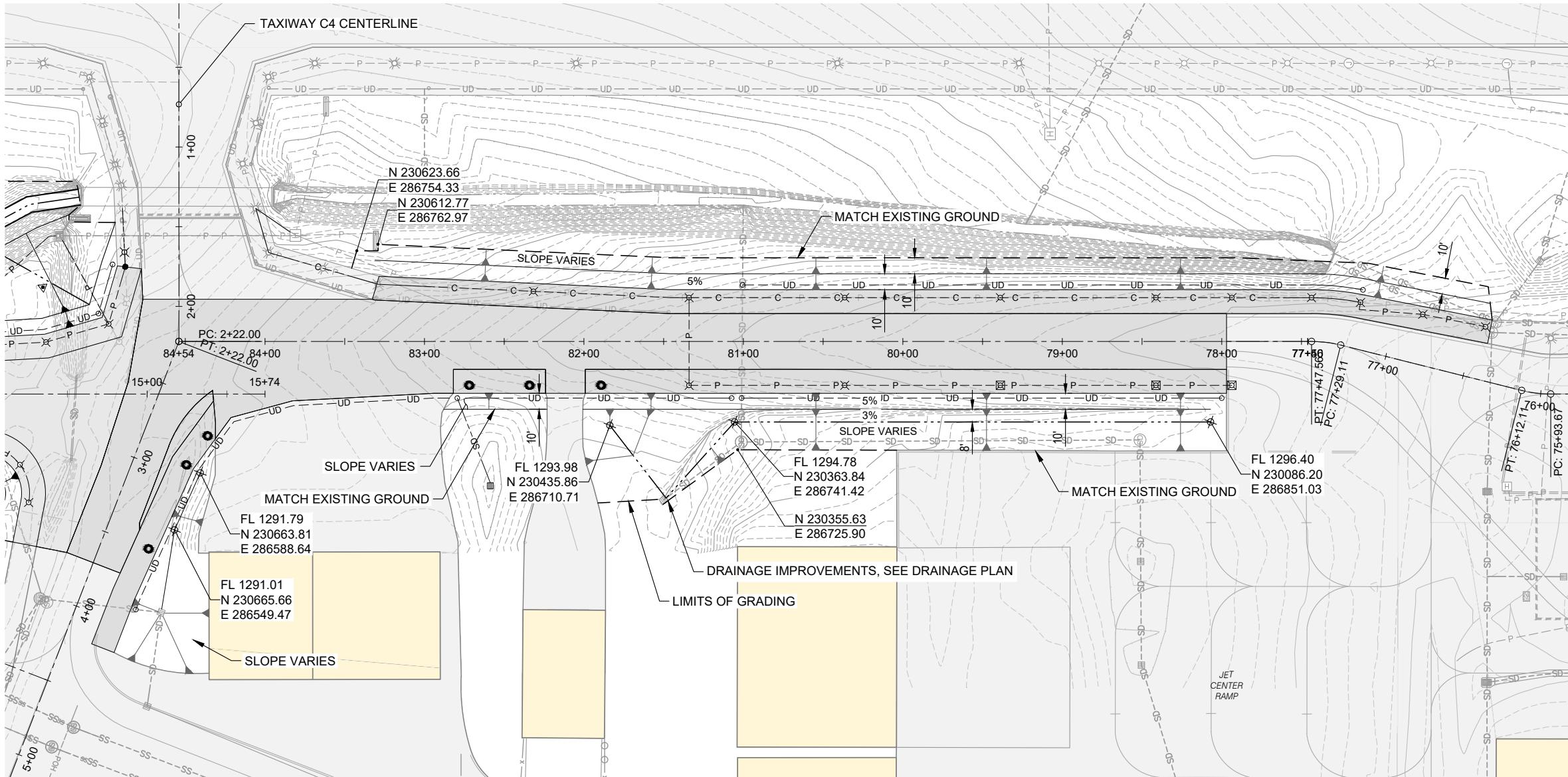


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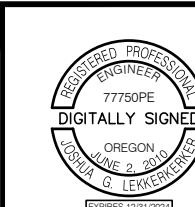
## LEGEND



RELOCATED AIRPORT OPERATIONS ROADWAY



ASPHALT GRINDINGS, 6" MINIMUM DEPTH

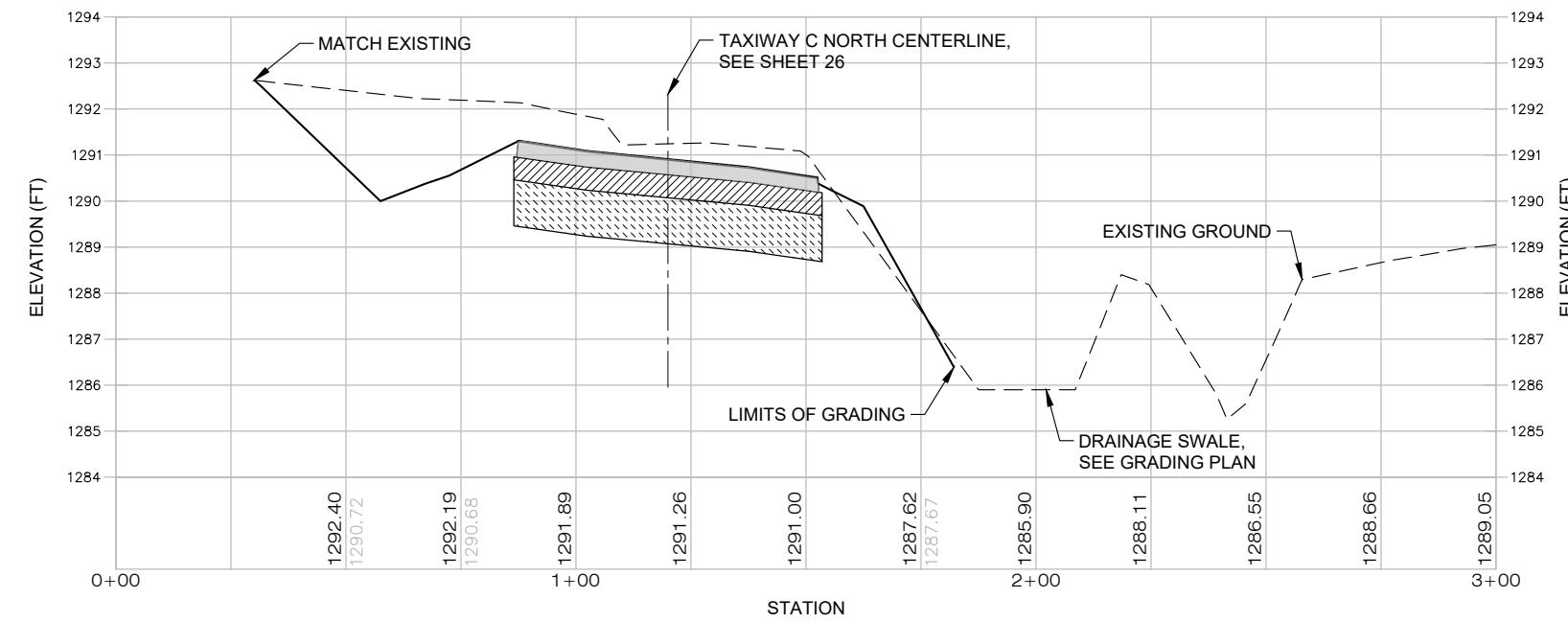
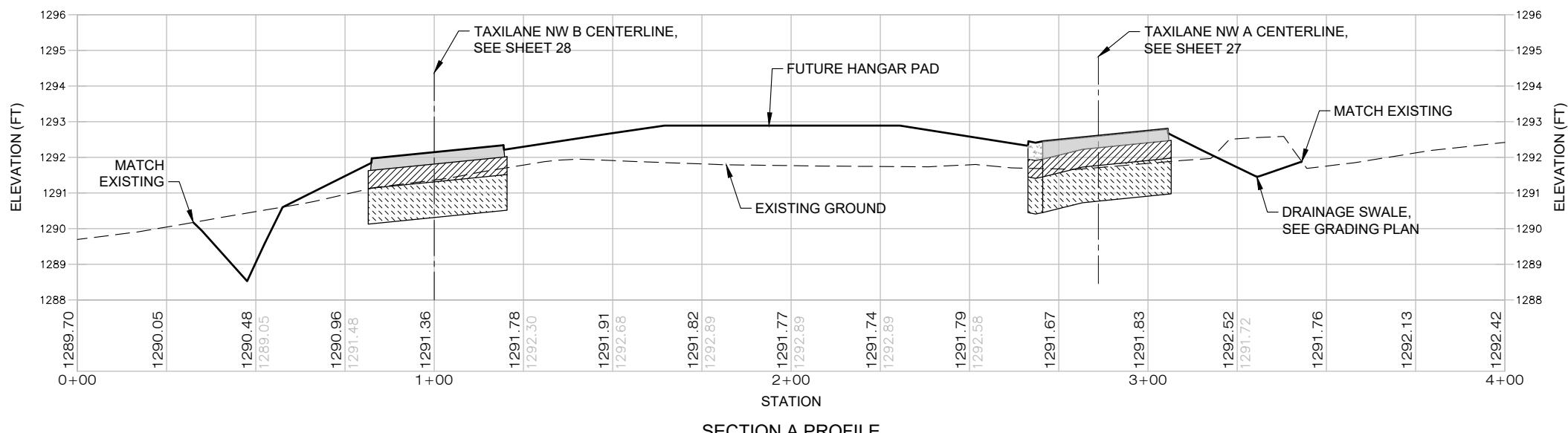
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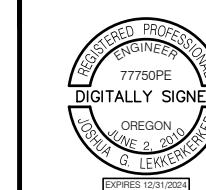
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**GRADING PLAN - ADDITIVE**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RFV-2020-W04**  
SHEET NO.  
**31**  
OF **71**



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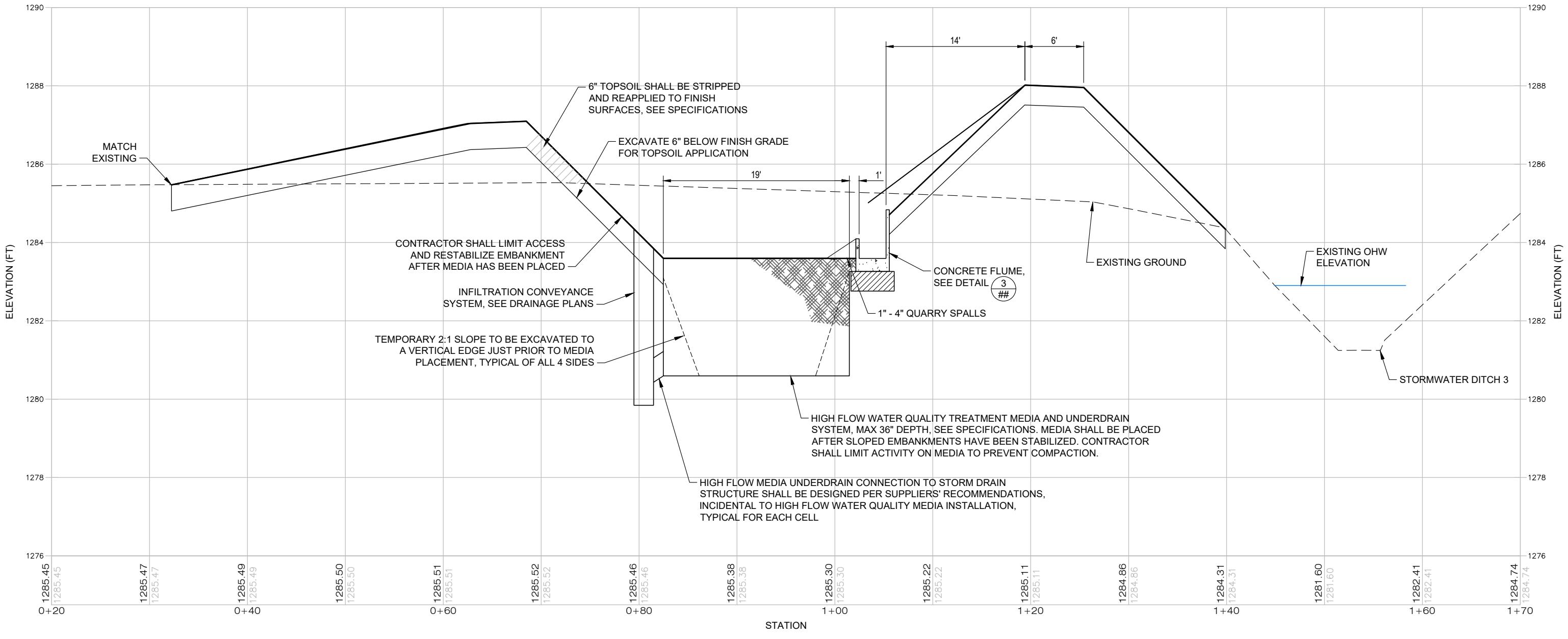


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DATE APPD.

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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**CROSS SECTIONS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
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OF **71**



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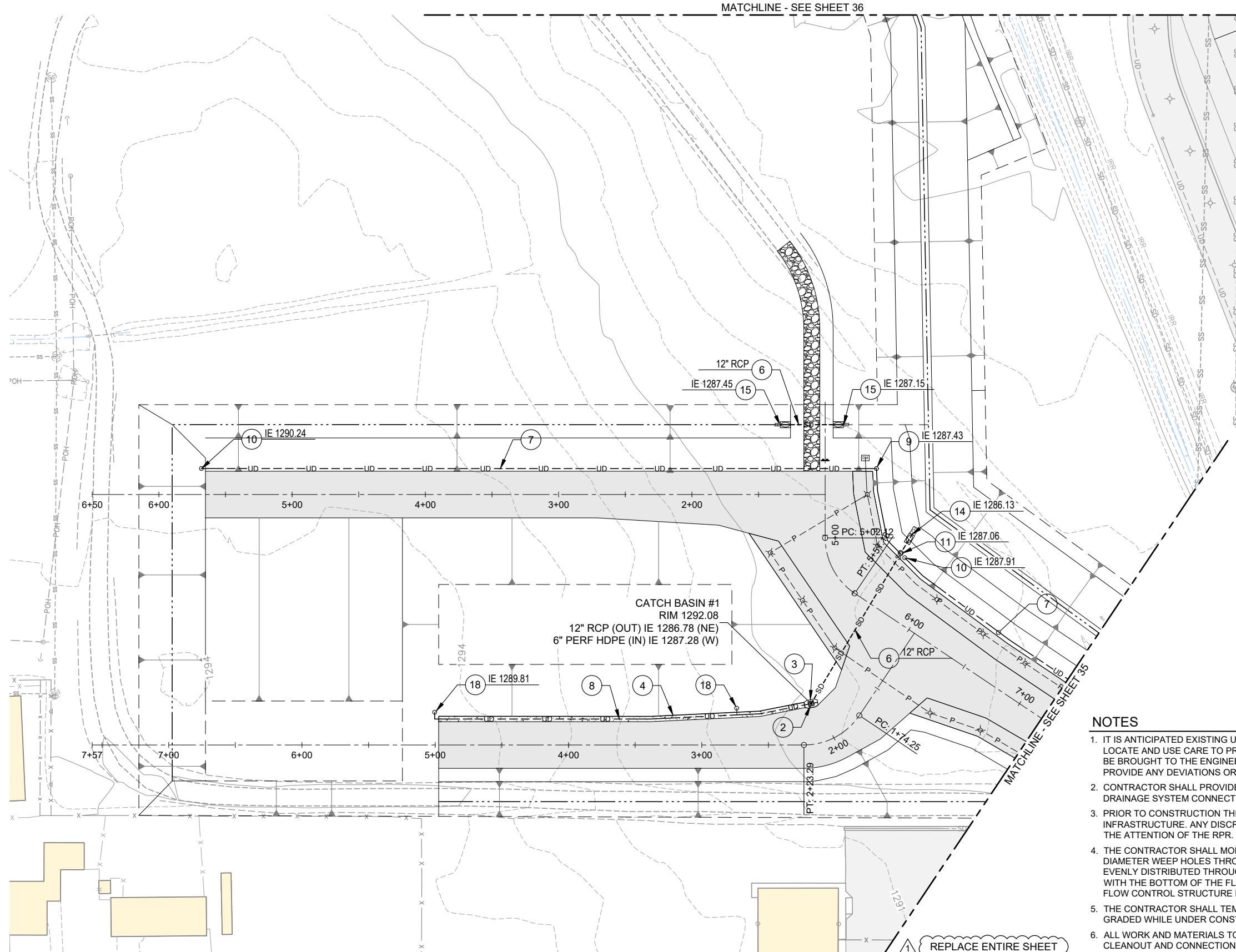
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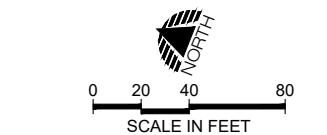
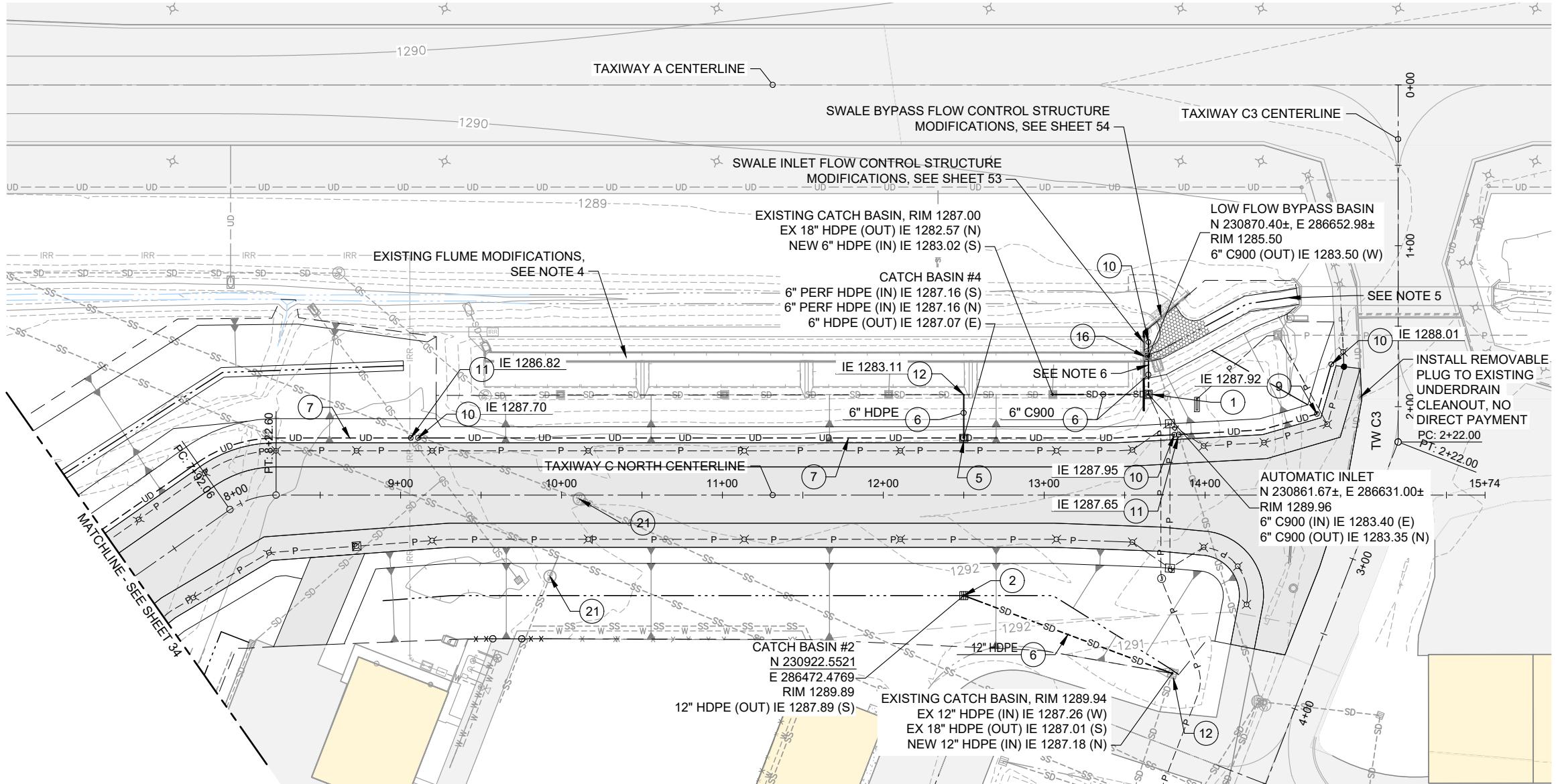
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**CROSS SECTION**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
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OF **71**





## LEGEND

1. INSTALL AUTOMATIC POND INLET SYSTEM, SEE DETAIL 1-38
2. INSTALL NEW CATCH BASIN, SEE DETAIL 2-38
3. INSTALL CATCH BASIN WITH CONCRETE DRAINAGE COLLAR, SEE DETAIL 1-45
4. INSTALL CONCRETE VALLEY GUTTER, SEE DETAIL 2-45
5. INSTALL CATCH BASIN WITH SOLID LID, SEE DETAIL 2-38
6. INSTALL SOLID PIPE AS SPECIFIED, SEE DETAIL 1-39
7. INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, NON-PAVED AREA, SEE DETAIL 1-40
8. INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, PAVED AREA, SEE DETAIL 2-40
9. INSTALL UNDERDRAIN CONNECTION WITH CLEANOUT, SEE DETAIL 1-44
10. INSTALL CLEANOUT, SEE DETAIL 2-44
11. CONNECT NEW PIPE TO NEW OR EXISTING PIPE, SEE DETAIL 3-44
12. CONNECT TO EXISTING STRUCTURE. VERIFY INVERT WITH ENGINEER PRIOR TO EXCAVATING TRENCH. ALL CONNECTIONS SHALL BE WATERTIGHT
13. INSTALL CONCRETE FLUME, SEE DETAIL 3-48
14. INSTALL CONCRETE SLOPED END, SEE DETAIL 2-46
15. STORM DRAIN INLET/OUTLET PROTECTION, SEE DETAIL 4-40
16. INSTALL LOW FLOW BYPASS BASIN, SEE DETAIL 1-47
17. INSTALL ADJUSTABLE POND INLET SYSTEM, SEE DETAIL 2-41
18. INSTALL VALLEY GUTTER CLEANOUT, SEE DETAIL 5-44
19. POND VALLEY GUTTER EXTENSION, SEE DETAIL 1-42
20. CONCRETE ENERGY DISSIPATION SYSTEM, SEE DETAIL 3-42
21. ADJUST EXISTING DRAINAGE STRUCTURE, SEE DETAIL 3-40
22. INSTALL 6" PERFORATED UNDERDRAIN PIPE IN EXISTING PAVEMENT, SEE DETAIL 1-41
23. POTENTIAL UTILITY CONFLICT, TYPICAL, POTHOLE TO VERIFY EXISTING UTILITY LOCATION AND ELEVATION, SEE NOTE 1 (NOT ALL LOCATION SHOWN)

## NOTES

1. IT IS ANTICIPATED EXISTING UTILITIES MAY CONFLICT WITH STORM PIPE CROSSINGS AT SOME LOCATIONS. CONTRACTOR SHALL LOCATE AND USE CARE TO PROTECT EXISTING UTILITIES DURING CONSTRUCTION. ANY CONFLICT FOUND SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEERS ATTENTION. CONTRACTOR SHALL VERIFY COMPATIBILITY WITH INTENDED IMPROVEMENTS AND PROVIDE ANY DEVIATIONS OR RECOMMENDED CHANGES TO ENGINEER FOR EVALUATION PRIOR TO PERFORMING ANY WORK.
2. CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE AS NECESSARY TO INTERCEPT SURFACE WATER AND MAINTAIN EXISTING DRAINAGE SYSTEM CONNECTIVITY THROUGHOUT ALL PROJECT PHASES. NO DIRECT PAYMENT.
3. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL VERIFY THE LOCATION AND INVERTS OF ALL EXISTING DRAINAGE INFRASTRUCTURE. ANY DISCREPANCIES BETWEEN THE PLANS AND FIELD CONDITIONS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE RPR.
4. THE CONTRACTOR SHALL MODIFY THE EXISTING SOUTH TREATMENT STORMWATER FLUME BY CORING OR DRILLING 12, 3-INCH DIAMETER WEEP HOLES THROUGHOUT THE FLUME. WEEP HOLES SHALL BE PLACED EQUIDISTANT IN THE FLUME AND SHALL BE EVENLY DISTRIBUTED THROUGH THE FOUR STORMWATER TREATMENT CELLS. WEEP HOLES SHALL BE CONSTRUCTED FLUSH WITH THE BOTTOM OF THE FLUME. ALL WORK AND MATERIALS SHALL BE CONSIDERED INCIDENTAL TO THE SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM. SIMILAR TO NEW CONCRETE FLUME DETAIL.
5. THE CONTRACTOR SHALL TEMPORARILY BLOCK, PUMP OR OTHERWISE DIRECT STORMWATER FLOWS AROUND THE AREAS TO BE GRADED WHILE UNDER CONSTRUCTION, COORDINATE WITH RPR. NO DIRECT PAYMENT.
6. ALL WORK AND MATERIALS TO CONSTRUCT THE SOUTH TREATMENT LOW FLOW BYPASS SYSTEM, EXCEPT FOR THE UPSTREAM CLEANOUT AND CONNECTION TO EXISTING STRUCTURE, INCLUDING THE STRUCTURE, AGGREGATE FILL, C900 PIPE, TRENCHING, EXCAVATING AND BACKFILLING THE PIPE SHALL BE PAID UNDER THE SOUTH TREATMENT LOW FLOW BYPASS BID ITEM.

1 REPLACE ENTIRE SHEET

PRECISION APPROACH  
ENGINEERING  
5125 Southwest Hout Street  
Corvallis, OR 97333  
541-754-0043



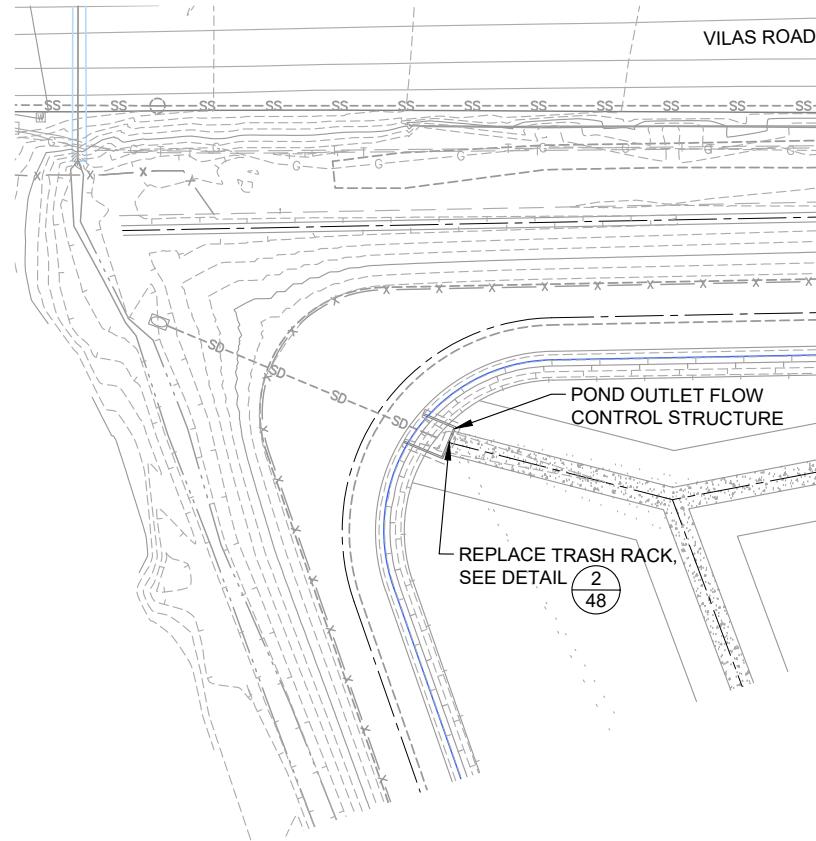
1 ADDENDUM NO. 1  
REVISIONS:

5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
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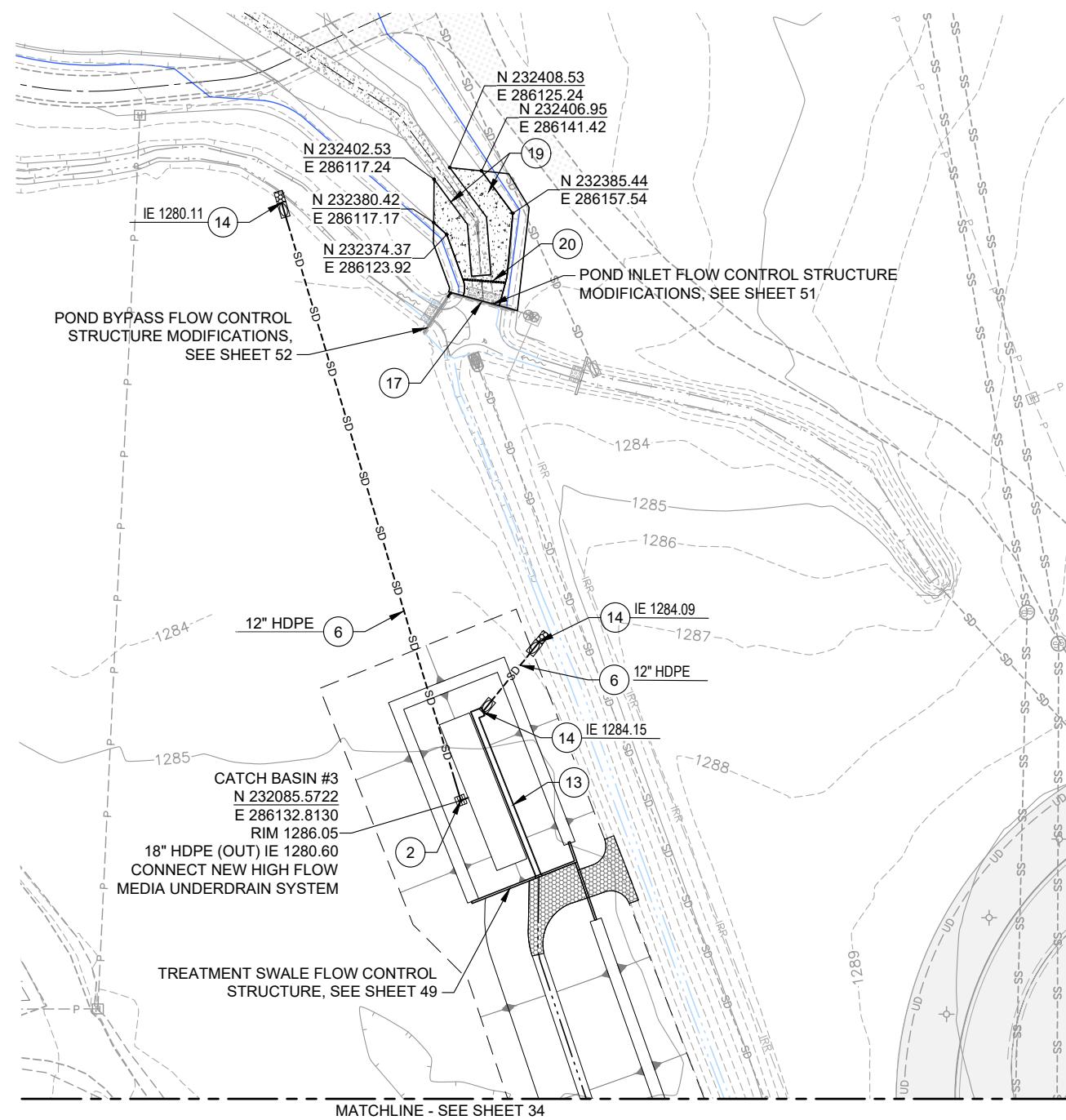
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE PLAN**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV\I-2020-W04**  
SHEET NO.  
**35**  
OF **71**



## NOTES

1. IT IS ANTICIPATED EXISTING UTILITIES MAY CONFLICT WITH STORM PIPE CROSSINGS AT SOME LOCATIONS. CONTRACTOR SHALL LOCATE AND USE CARE TO PROTECT EXISTING UTILITIES DURING CONSTRUCTION, ANY CONFLICT FOUND SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEERS ATTENTION. CONTRACTOR SHALL VERIFY COMPATIBILITY WITH INTENDED IMPROVEMENTS AND PROVIDE ANY DEVIATIONS OR RECOMMENDED CHANGES TO ENGINEER FOR EVALUATION PRIOR TO PERFORMING ANY WORK.
2. CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE AS NECESSARY TO INTERCEPT SURFACE WATER AND MAINTAIN EXISTING DRAINAGE SYSTEM CONNECTIVITY THROUGHOUT ALL PROJECT PHASES. NO DIRECT PAYMENT.
3. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL VERIFY THE LOCATION AND INVERTS OF ALL EXISTING DRAINAGE INFRASTRUCTURE. ANY DISCREPANCIES BETWEEN THE PLANS AND FIELD CONDITIONS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE RPR.
4. THE CONTRACTOR SHALL MODIFY THE EXISTING SOUTH TREATMENT STORMWATER FLUME BY CORING OR DRILLING 12, 3-INCH DIAMETER WEEP HOLES THROUGHOUT THE FLUME. WEEP HOLES SHALL BE PLACED EQUIDISTANT IN THE FLUME AND SHALL BE EVENLY DISTRIBUTED THROUGH THE FOUR STORMWATER TREATMENT CELLS. WEEP HOLES SHALL BE CONSTRUCTED FLUSH WITH THE BOTTOM OF THE FLUME. ALL WORK AND MATERIALS SHALL BE CONSIDERED INCIDENTAL TO THE SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM. SIMILAR TO NEW CONCRETE FLUME DETAIL.
5. THE CONTRACTOR SHALL TEMPORARY BLOCK, PUMP OR OTHERWISE DIRECT STORMWATER FLOWS AROUND THE AREAS TO BE GRADED WHILE UNDER CONSTRUCTION, COORDINATE WITH RPR. NO DIRECT PAYMENT.
6. ALL WORK AND MATERIALS TO CONSTRUCT THE SOUTH TREATMENT LOW FLOW BYPASS SYSTEM, EXCEPT FOR THE UPSTREAM CLEANOUT AND CONNECTION TO EXISTING STRUCTURE, INCLUDING THE STRUCTURE, AGGREGATE FILL, C900 PIPE, TRENCHING, EXCAVATING AND BACKFILLING THE PIPE SHALL BE PAID UNDER THE SOUTH TREATMENT LOW FLOW BYPASS BID ITEM.

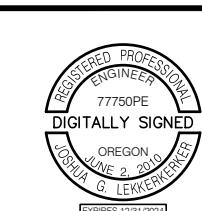


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## LEGEND

1. INSTALL AUTOMATIC POND INLET SYSTEM, SEE DETAIL 1 38
2. INSTALL NEW CATCH BASIN, SEE DETAIL 2 38
3. INSTALL CATCH BASIN WITH CONCRETE DRAINAGE COLLAR, SEE DETAIL 1 45
4. INSTALL CONCRETE VALLEY GUTTER, SEE DETAIL 2 45
5. INSTALL CATCH BASIN WITH SOLID LID, SEE DETAIL 2 38
6. INSTALL SOLID PIPE AS SPECIFIED, SEE DETAIL 1 39
7. INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, NON-PAVED AREA, SEE DETAIL 1 40
8. INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, PAVED AREA, SEE DETAIL 2 40
9. INSTALL UNDERDRAIN CONNECTION WITH CLEANOUT, SEE DETAIL 1 44
10. INSTALL CLEANOUT, SEE DETAIL 2 44
11. CONNECT NEW PIPE TO NEW OR EXISTING PIPE, SEE DETAIL 3 44
12. CONNECT TO EXISTING STRUCTURE. VERIFY INVERT WITH ENGINEER PRIOR TO EXCAVATING TRENCH. ALL CONNECTIONS SHALL BE WATERTIGHT
13. INSTALL CONCRETE FLUME, SEE DETAIL 3 48
14. INSTALL CONCRETE SLOPED END, SEE DETAIL 2 46
15. STORM DRAIN INLET/OUTLET PROTECTION, SEE DETAIL 4 40
16. INSTALL LOW FLOW BYPASS BASIN, SEE DETAIL 1 47
17. INSTALL ADJUSTABLE POND INLET SYSTEM, SEE DETAIL 2 41
18. INSTALL VALLEY GUTTER CLEANOUT, SEE DETAIL 5 44
19. POND VALLEY GUTTER EXTENSION, SEE DETAIL 1 42
20. CONCRETE ENERGY DISSIPATION SYSTEM, SEE DETAIL 3 42
21. ADJUST EXISTING DRAINAGE STRUCTURE, SEE DETAIL 3 40
22. INSTALL 6" PERFORATED UNDERDRAIN PIPE IN EXISTING PAVEMENT, SEE DETAIL 1 41
23. POTENTIAL UTILITY CONFLICT, TYPICAL, POTHOLE TO VERIFY EXISTING UTILITY LOCATION AND ELEVATION, SEE NOTE 1 (NOT ALL LOCATION SHOWN)

REPLACE ENTIRE SHEET

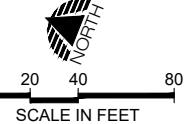
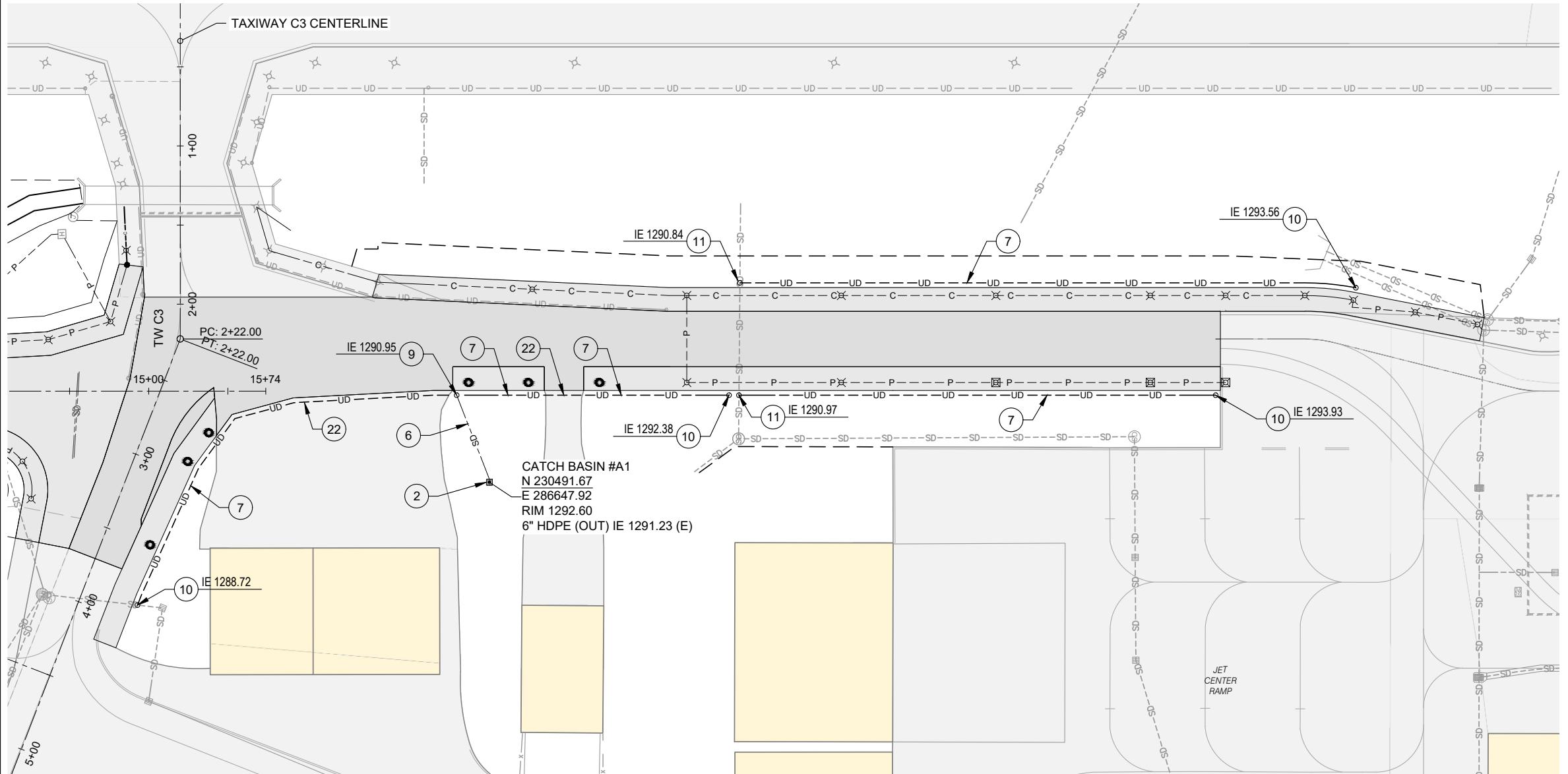


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REVISIONS:	DATE	APPD.

DATE: MAY 2024  
DESIGN: KJK  
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CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE PLAN**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV\I-2020-W04**  
SHEET NO.  
**36**  
OF **71**



### LEGEND

1. INSTALL AUTOMATIC POND INLET SYSTEM, SEE DETAIL 1 (38)
2. INSTALL NEW CATCH BASIN, SEE DETAIL 2 (38)
3. INSTALL CATCH BASIN WITH CONCRETE DRAINAGE COLLAR, SEE DETAIL 1 (45)
4. INSTALL CONCRETE VALLEY GUTTER, SEE DETAIL 2 (45)
5. INSTALL UNDERDRAIN ACCESS STRUCTURE, SEE DETAIL 1 (39)
6. INSTALL SOLID PIPE AS SPECIFIED, SEE DETAIL 2 (39)
7. INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, NON-PAVED AREA, SEE DETAIL 1 (40)
8. INSTALL 6" PERFORATED HDPE UNDERDRAIN PIPE, PAVED AREA, SEE DETAIL 2 (40)
9. INSTALL UNDERDRAIN CONNECTION WITH CLEANOUT, SEE DETAIL 1 (44)
10. INSTALL CLEANOUT, SEE DETAIL 2 (44)
11. CONNECT NEW PIPE TO NEW OR EXISTING PIPE, SEE DETAIL 3 (44)
12. CONNECT TO EXISTING STRUCTURE. VERIFY INVERT WITH ENGINEER PRIOR TO EXCAVATING TRENCH. ALL CONNECTIONS SHALL BE WATERTIGHT
13. INSTALL CONCRETE FLUME, SEE DETAIL 3 (48)
14. INSTALL CONCRETE SLOPED END, SEE DETAIL 2 (46)
15. STORM DRAIN INLET/OUTLET PROTECTION, SEE DETAIL 4 (40)
16. INSTALL LOW FLOW BYPASS BASIN, SEE DETAIL 1 (47)
17. INSTALL ADJUSTABLE POND INLET SYSTEM, SEE DETAIL 2 (41)
18. INSTALL VALLEY GUTTER CLEANOUT, SEE DETAIL 5 (44)
19. POND VALLEY GUTTER EXTENSION, SEE DETAIL 1 (42)
20. CONCRETE ENERGY DISSIPATION SYSTEM, SEE DETAIL 3 (42)
21. ADJUST EXISTING DRAINAGE STRUCTURE, SEE DETAIL 3 (40)
22. INSTALL 6" PERFORATED UNDERDRAIN PIPE IN EXISTING PAVEMENT, SEE DETAIL 1 (41)
23. POTENTIAL UTILITY CONFLICT, TYPICAL, POTHOLE TO VERIFY EXISTING UTILITY LOCATION AND ELEVATION, SEE NOTE 1 (NOT ALL LOCATION SHOWN)

### NOTES

1. IT IS ANTICIPATED EXISTING UTILITIES MAY CONFLICT WITH STORM PIPE CROSSINGS AT SOME LOCATIONS. CONTRACTOR SHALL LOCATE AND USE CARE TO PROTECT EXISTING UTILITIES DURING CONSTRUCTION. ANY CONFLICT FOUND SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEERS ATTENTION. CONTRACTOR SHALL VERIFY COMPATIBILITY WITH INTENDED IMPROVEMENTS AND PROVIDE ANY DEVIATIONS OR RECOMMENDED CHANGES TO ENGINEER FOR EVALUATION PRIOR TO PERFORMING ANY WORK.
2. CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE AS NECESSARY TO INTERCEPT SURFACE WATER AND MAINTAIN EXISTING DRAINAGE SYSTEM CONNECTIVITY THROUGHOUT ALL PROJECT PHASES. NO DIRECT PAYMENT.
3. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL VERIFY THE LOCATION AND INVERTS OF ALL EXISTING DRAINAGE INFRASTRUCTURE. ANY DISCREPANCIES BETWEEN THE PLANS AND FIELD CONDITIONS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE RPR.
4. THE CONTRACTOR SHALL MODIFY THE EXISTING SOUTH TREATMENT STORMWATER FLUME BY CORING OR DRILLING 12, 3-INCH DIAMETER WEEP HOLES THROUGHOUT THE FLUME. WEEP HOLES SHALL BE PLACED EQUIDISTANT IN THE FLUME AND SHALL BE EVENLY DISTRIBUTED THROUGH THE FOUR STORMWATER TREATMENT CELLS. WEEP HOLES SHALL BE CONSTRUCTED FLUSH WITH THE BOTTOM OF THE FLUME. ALL WORK AND MATERIALS SHALL BE CONSIDERED INCIDENTAL TO THE SOUTH TREATMENT FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM. SIMILAR TO NEW CONCRETE FLUME DETAIL.
5. THE CONTRACTOR SHALL TEMPORARILY BLOCK, PUMP OR OTHERWISE DIRECT STORMWATER FLOWS AROUND THE AREAS TO BE GRADED WHILE UNDER CONSTRUCTION, COORDINATE WITH RPR. NO DIRECT PAYMENT.
6. ALL WORK AND MATERIALS TO CONSTRUCT THE SOUTH TREATMENT LOW FLOW BYPASS SYSTEM, EXCEPT FOR THE UPSTREAM CLEANOUT AND CONNECTION TO EXISTING STRUCTURE, INCLUDING THE STRUCTURE, AGGREGATE FILL, C900 PIPE, TRENCHING, EXCAVATING AND BACKFILLING THE PIPE SHALL BE PAID UNDER THE SOUTH TREATMENT LOW FLOW BYPASS BID ITEM.

REPLACE ENTIRE SHEET

**PRECISION APPROACH**  
ENGINEERING  
5125 Southwest Hout Street  
Corvallis, OR 97333  
541•754•0043



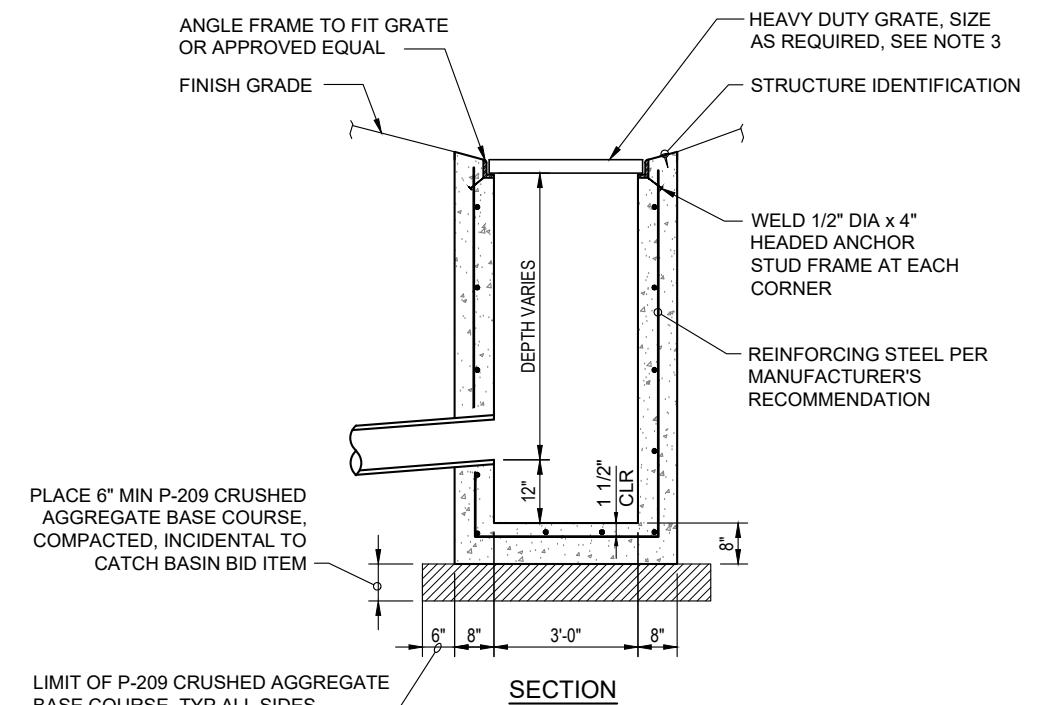
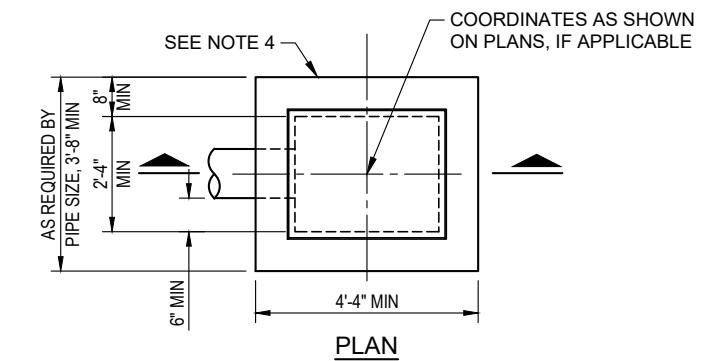
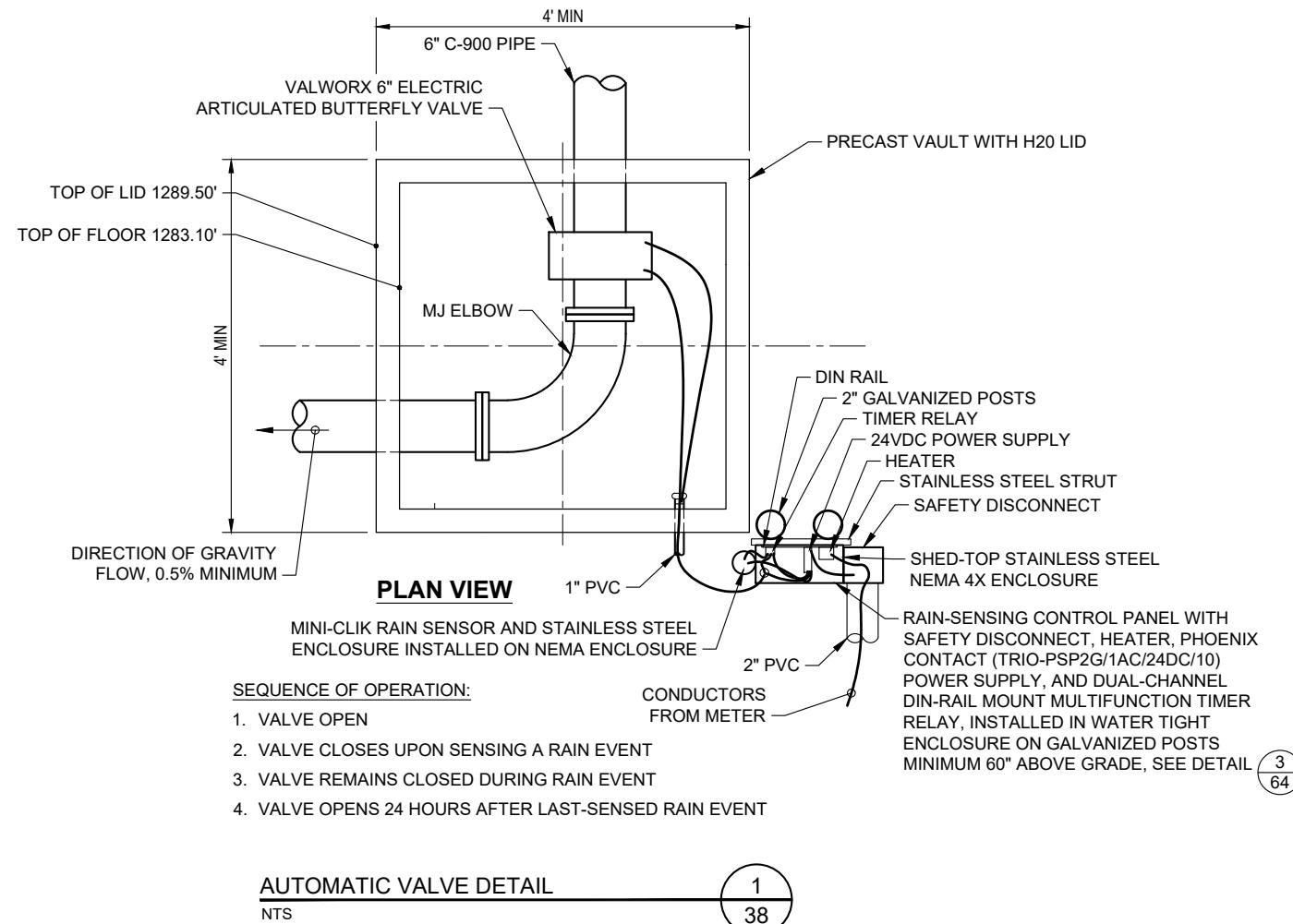
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5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE PLAN - ADDITIVE**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**37**  
OF **71**



## NOTES:

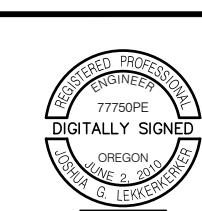
1. DIMENSIONS OF CATCH BASIN MAY CHANGE, WITH ENGINEER'S APPROVAL, IF REQUIRED FOR COMPATIBILITY WITH FRAME AND GRATE OR DUE TO PIPE SIZE.
2. GRATE SHALL BE BOLTED TO FRAME PER MANUFACTURER'S RECOMMENDATION.
3. ALL COMPONENTS SHALL BE CAPABLE OF SUPPORTING 50 KIP MINIMUM POINT LOAD.
4. CONSTRUCT LONGITUDINAL JOINT SEAL ALL SIDES. WHEN ADJACENT TO ASPHALT PAVEMENT, SEE DETAIL 2 23

**CATCH BASIN DETAIL**

NTS

2 38

REPLACE ENTIRE SHEET



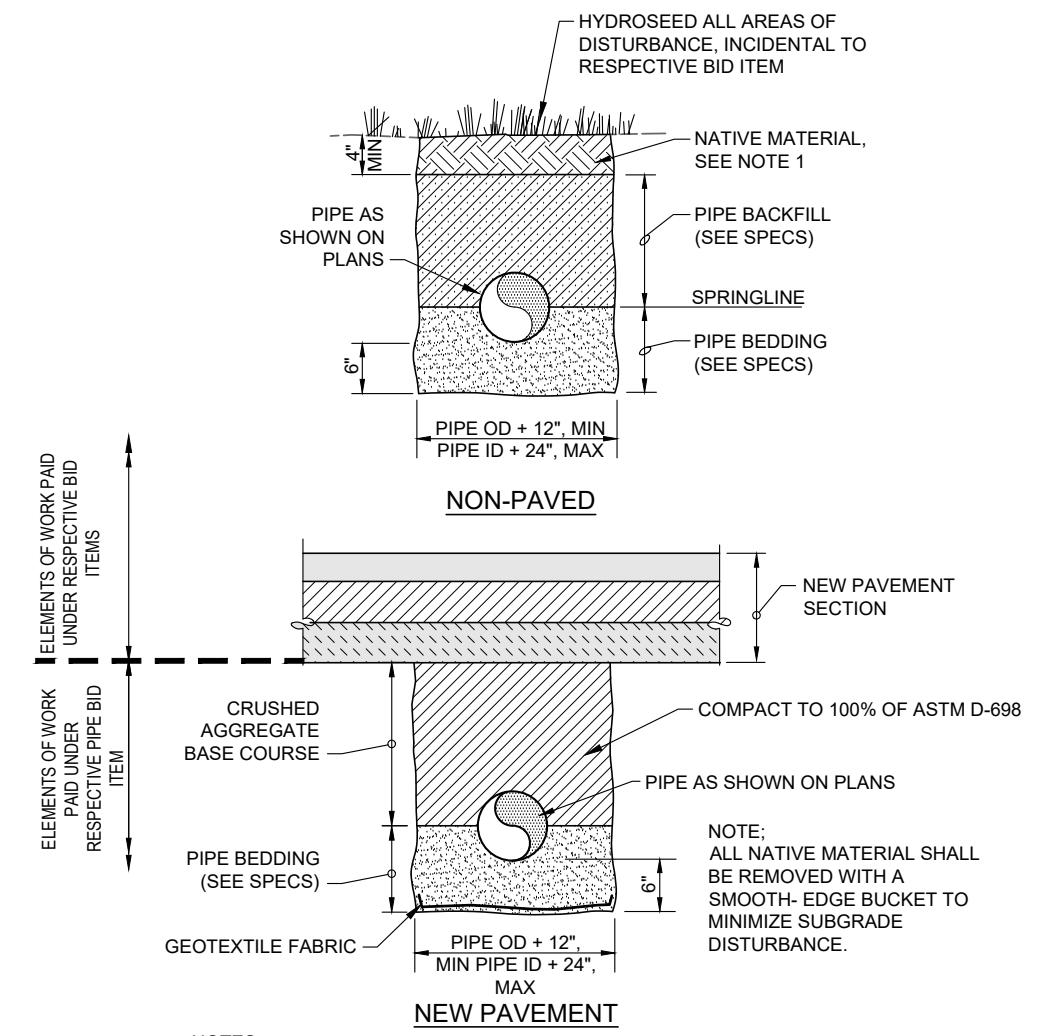
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5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
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REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**38**  
OF **71**



## NOTES:

1. STRIP AND STOCKPILE NATIVE TOP SOIL PRIOR TO TRENCHING, PLACE AND COMPACT TO A DENSE AND UNYIELDING CONDITION AS APPROVED BY THE ENGINEER PRIOR TO HYDROSEEDING.

SOLID STORM DRAIN PIPE DETAIL  
NTS1  
39

REPLACE ENTIRE SHEET



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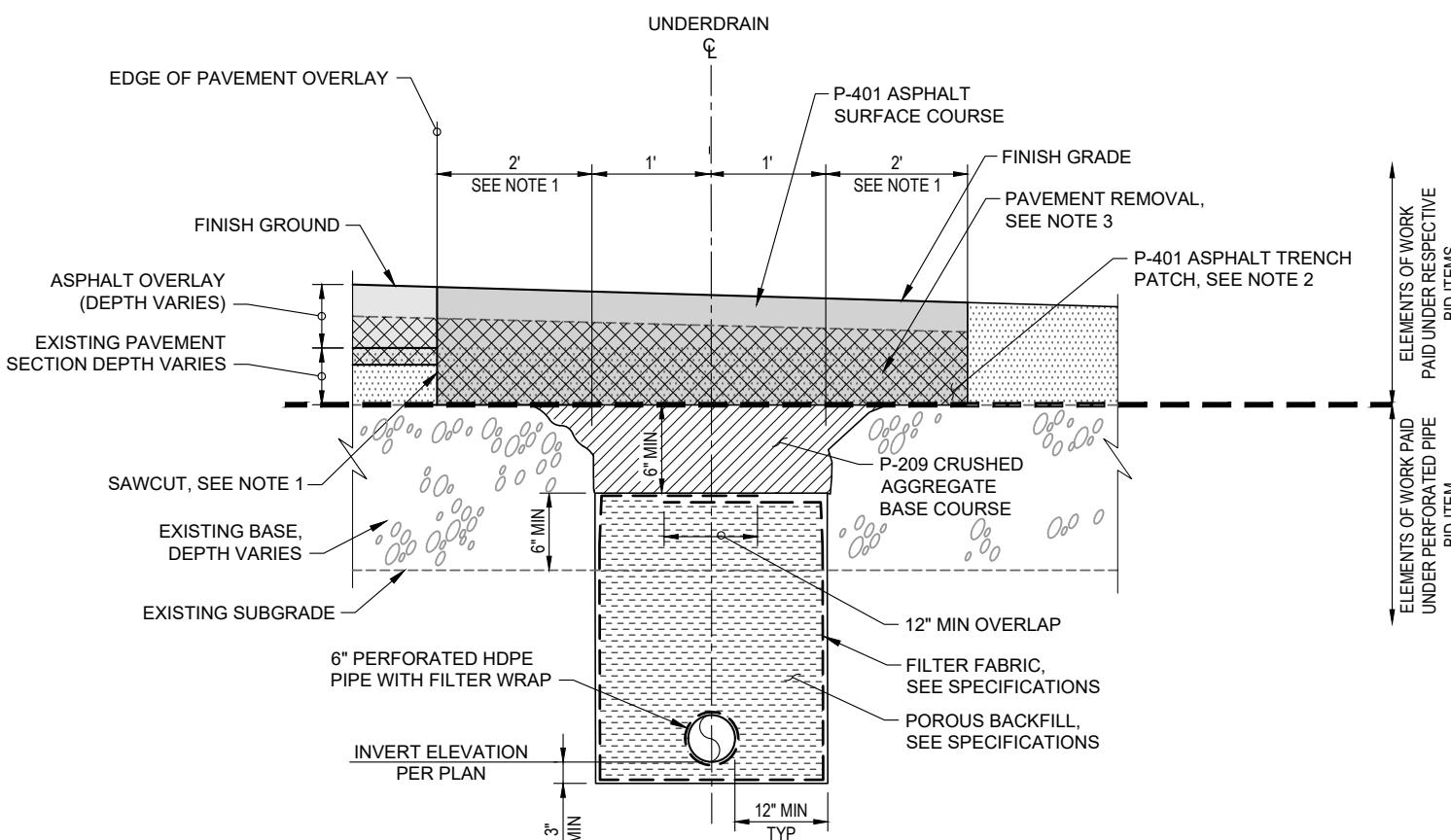
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

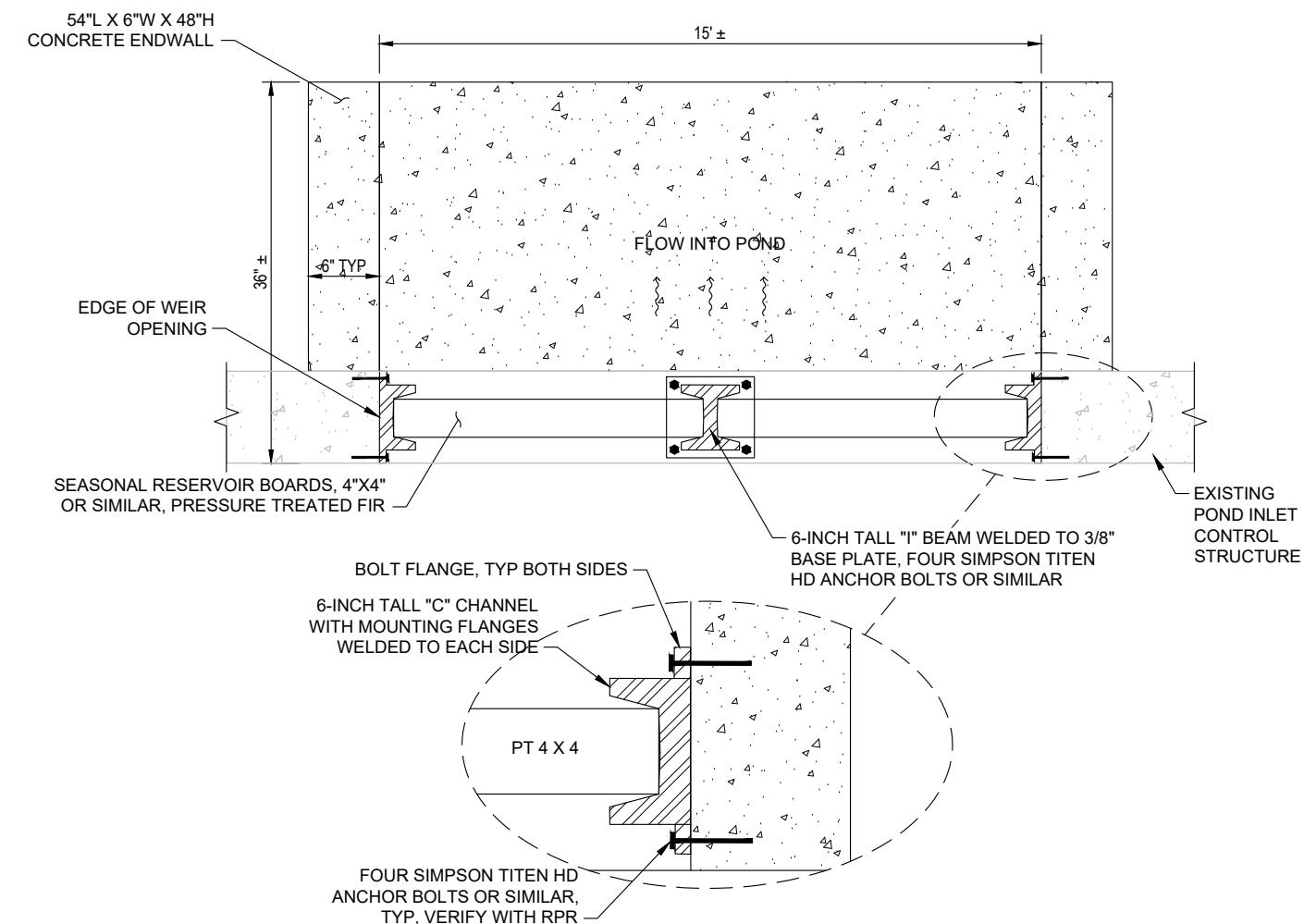
PROJECT NUMBER:  
**RVI-2020-W04**

SHEET NO.  
**39**  
OF **71**





1  
41



NOTES:

1. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS TO RPR FOR REVIEW PRIOR TO CONSTRUCTION
2. PLACE EXISTING ~8" COBBLES ADJACENT TO STRUCTURE. VERIFY COBBLE SIZE AND LIMITS WITH RPR PRIOR TO CONSTRUCTION.

ADJUSTABLE POND INLET SYSTEM DETAIL  
2  
41

REPLACE ENTIRE SHEET

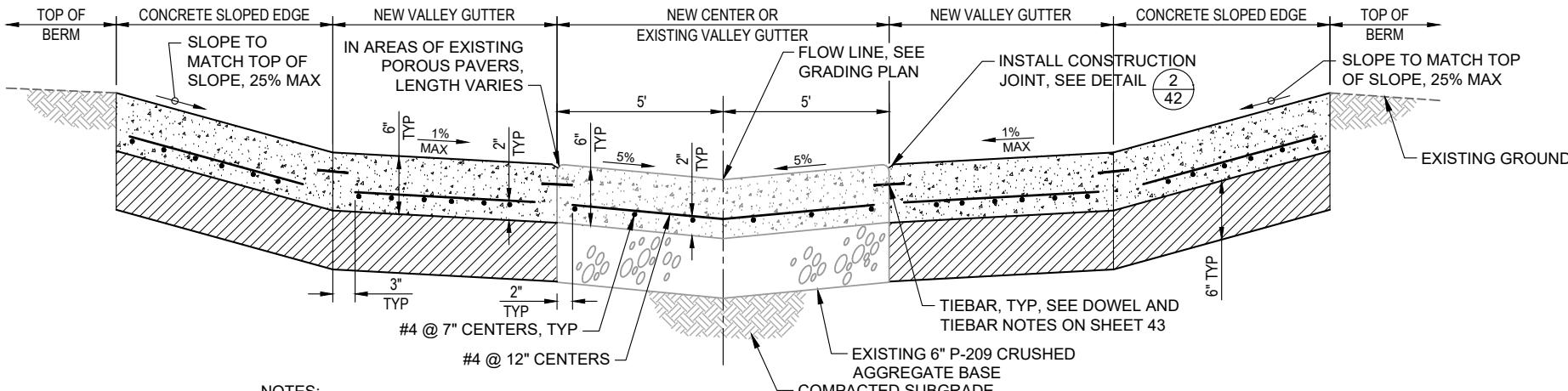


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DATE APPD.

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REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER: RVI-2020-W04  
SHEET NO. 41 OF 71



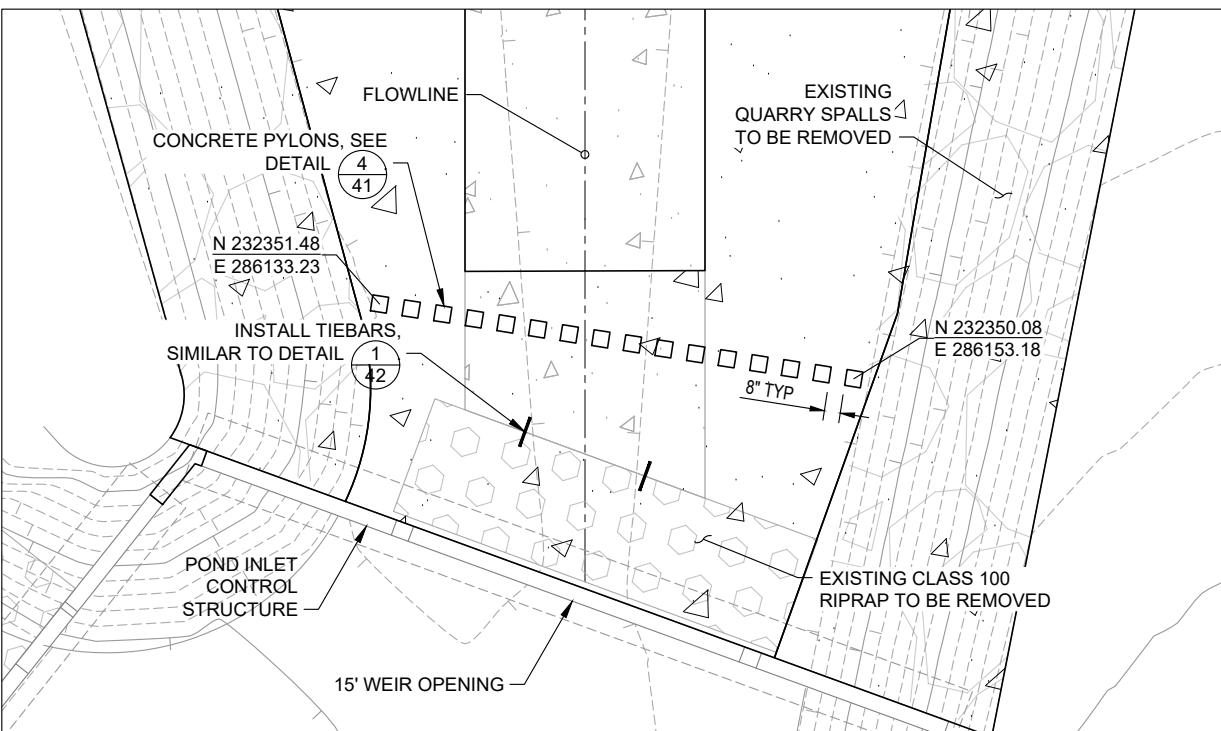
NOTES:

1. TRANSVERSE CONTRACTION JOINTS SHALL BE INSTALLED IN LINE WITH EXISTING JOINTS, VERIFY WITH RPR.
2. ALL WORK AND MATERIALS, EXCLUDING REQUIRED EXCAVATION AND POROUS PAVER REMOVAL, TO CONSTRUCT AND COMPLETE THE POND VALLEY GUTTER EXTENSION AS SHOWN SHALL BE PAID UNDER THE POND VALLEY GUTTER EXTENSION BID ITEM.
3. INSTALL NO. 4, 20" LONG, DEFORMED STEEL TIEBARS 36" O.C. BETWEEN NEW AND EXISTING VALLEY GUTTER, AND BETWEEN THE NEW SHALLOW SLOPED VALLEY GUTTER AND STEEP SLOPED CONCRETE EDGE.
4. THE CONTRACTOR SHALL CONSTRUCT ADDITIONAL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS FOR AREAS OF THE NEW VALLEY GUTTER WHERE THE LENGTH TO WIDTH RATIO EXCEEDS 1.25. THE CONTRACTOR SHALL PROVIDE A JOINT LAYOUT PLAN TO THE RPR FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION.
5. EXISTING VALLEY GUTTER REINFORCEMENT SHOWN FOR INFORMATIONAL PURPOSES ONLY.

CONCRETE POND VALLEY  
GUTTER EXTENSION DETAIL

NTS

1  
42



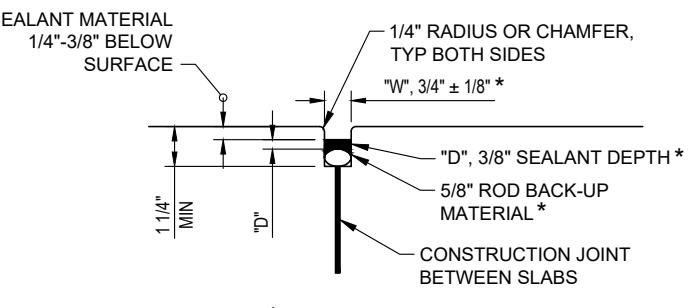
CONCRETE ENERGY  
DISSIPATION SYSTEM DETAIL

NTS

3  
42

NOTES:

1. THE CONTRACTOR SHALL SALVAGE EXISTING CLASS 100 RIPRAP AND 1"-4" QUARRY SPALLS WITHIN THE AREA OF CONCRETE ENERGY DISSIPATION SYSTEM AND PLACE IN AREAS AROUND THE EXISTING STORMWATER DETENTION POND AS DIRECTED BY THE RPR.



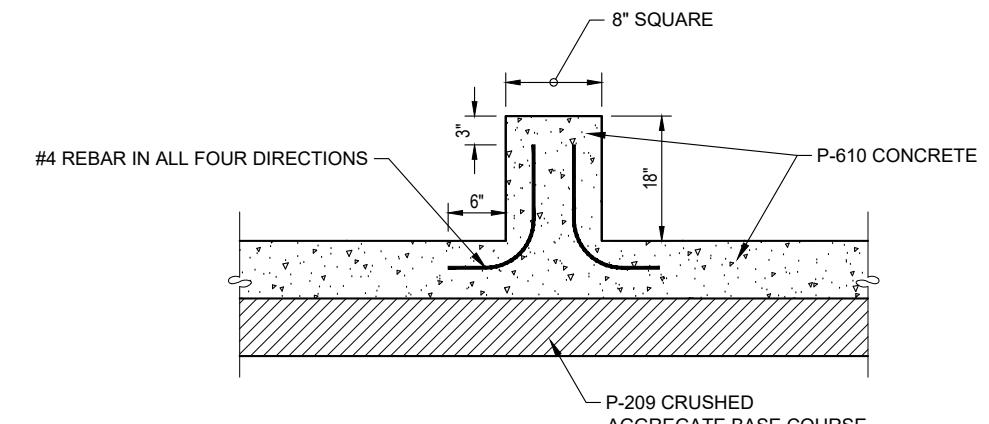
NOTES:

1. THE CONTRACTOR SHALL INSTALL 12" LONG, #4 REBAR DOWELS, 36" O.C. WITH 6" CLEAR SPACE. REBAR DOWELS SHALL BE CENTERED ON THE EXISTING VALLEY GUTTER.

PCC JOINT SEALANT  
INSTALLATION DETAIL

NTS

2  
42



NOTES:

1. CONCRETE PYLONS SHALL BE POURED IN PLACE IN AREAS OF NEW CONCRETE. IN AREAS OF EXISTING CONCRETE THE EXISTING PCC SHALL BE CORED OR OTHERWISE REMOVED TO CONSTRUCT PYLONS.

CONCRETE PYLON DETAIL

NTS

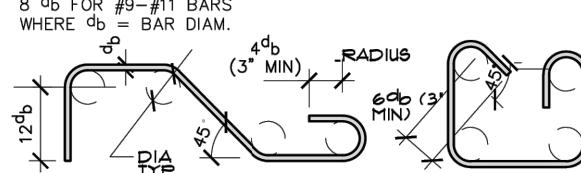
4  
42



f'c	3000 psi		3500 psi		4000 psi	
Bar Size	Top bar "Ls" [2] (inches)	Other "Ls" [3] (inches)	Top bar "Ls" [2] (inches)	Other "Ls" [3] (inches)	Top bar "Ls" [2] (inches)	Other "Ls" [3] (inches)
#3	28	22	26	20	25	20
#4	38	30	35	27	33	26
#5	47	37	43	34	41	32
#6	56	43	52	40	49	38
#7	61	63	75	58	71	55
#8	93	72	86	67	81	63

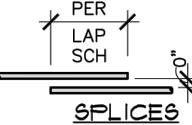
1. Top bars are horizontal bars w/ more than 12" of concrete below splice.
2. Where different bar sizes are lap spliced, the required lap length is the Ls for the smaller bar and the required development length is the Ls for the bigger bar and clearance based on the larger bar diameter.
3. Increase lap splice length by 20% for epoxy-coated bars.

GRADE 60 BEND DIAM: SPLICES FOR SUCCESSIVE TIES TO BE PLACED  
6 db FOR #3-#8 BARS  
8 db FOR #9-#11 BARS  
WHERE db = BAR DIAM.



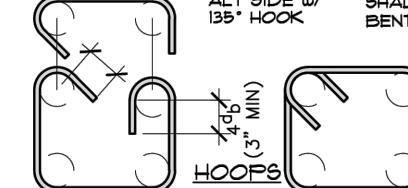
#### HOOKS & BENDS

ALL BARS SHALL BE TIED TOGETHER



#### BEAM STIRRUPS

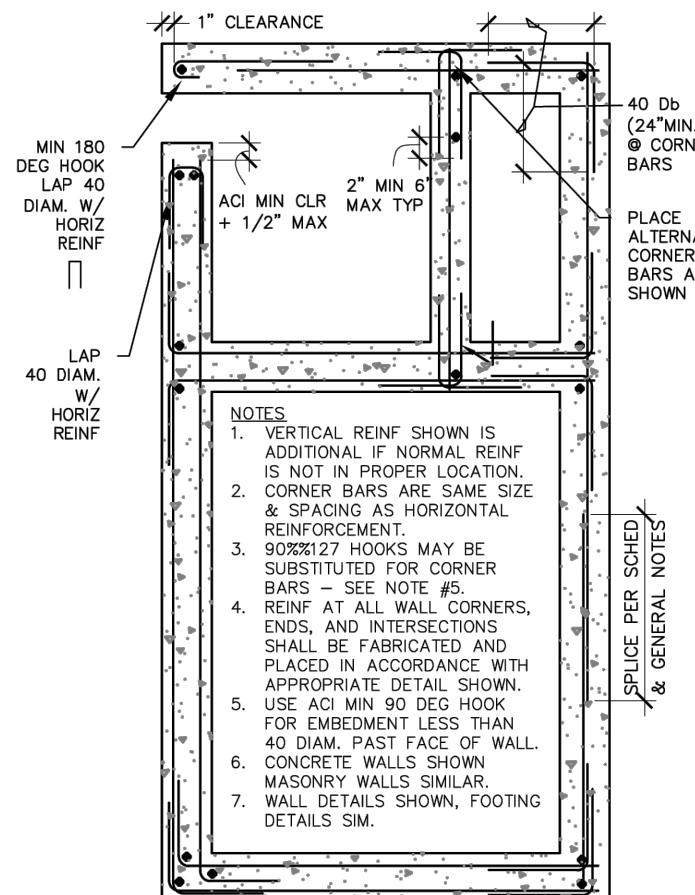
NOTE: ALL REINFORCEMENT SHALL BE BENT COLD



#### REBAR BEND DETAILS

NTS

1  
43

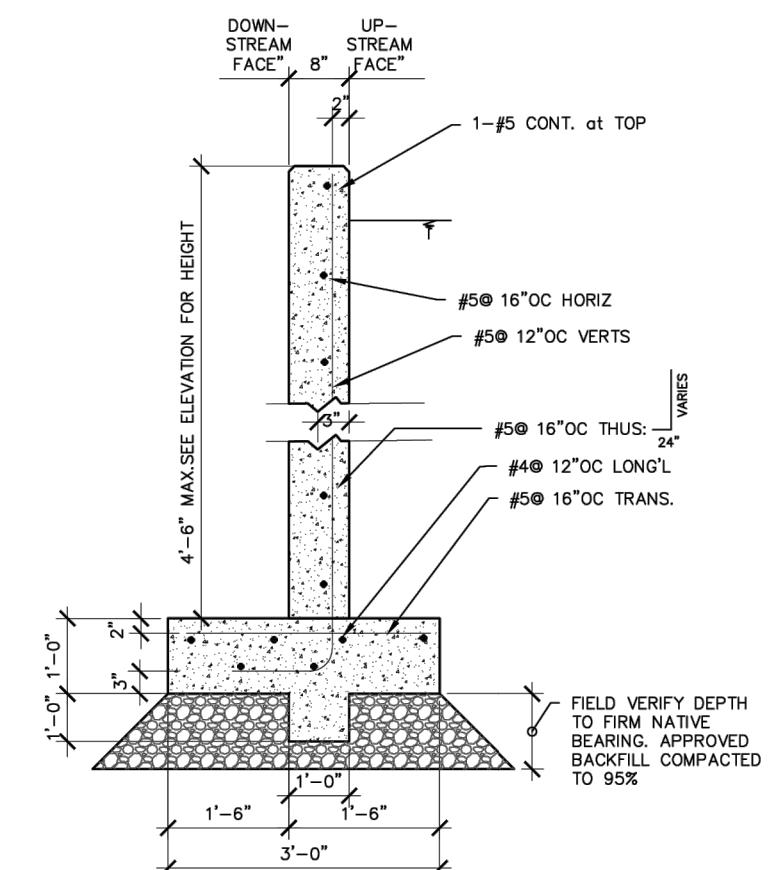


SPLICE PER SCHED & GENERAL NOTES

#### REBAR CORNER BENDS

NTS

2  
43



#### NEW OR MODIFIED CONTROL STRUCTURE

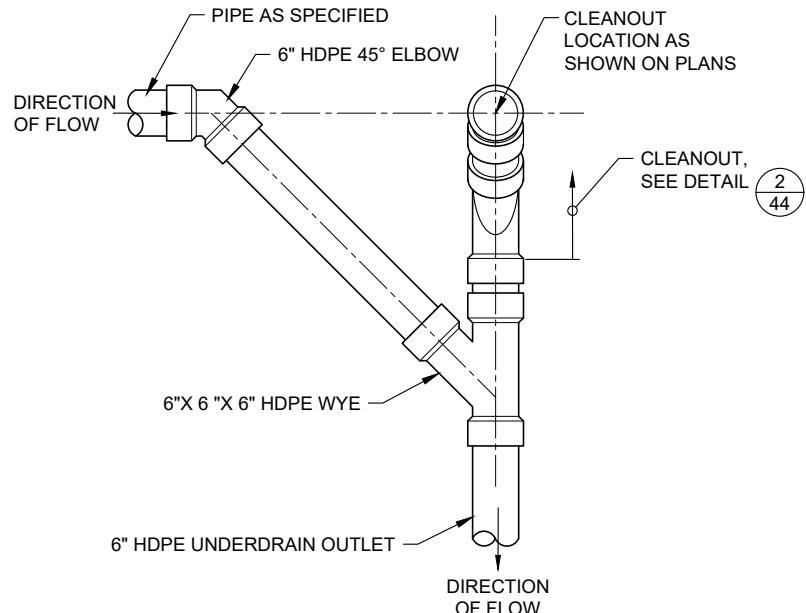
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3  
43

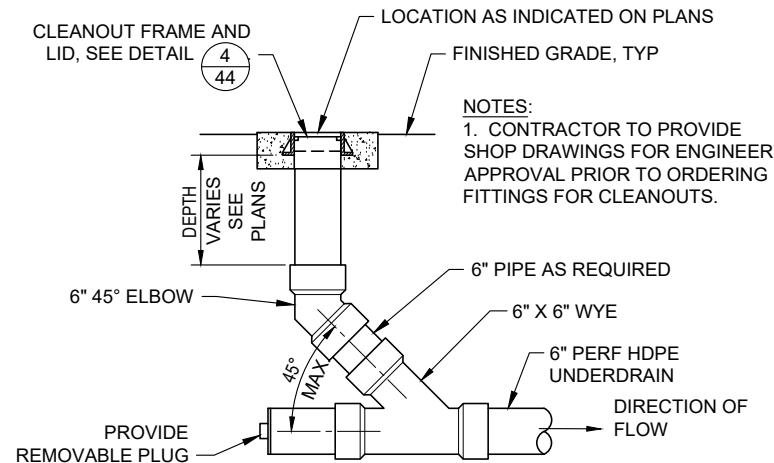
#### DOWEL AND TIEBAR GENERAL NOTES:

1. ALL DOWELS AND TIEBARS SHALL MEET THE REQUIREMENTS OF SPECIFICATION P-610, CONCRETE FOR MISCELLANEOUS STRUCTURES.
2. REBAR DOWELS SHALL BE DRILLED AND EPOXY GROUTED INTO EXISTING CONCRETE. REBAR DOWELS SHALL BE COATED WITH A BOND-BREAKER AS RECOMMENDED BY THE MANUFACTURER.
3. WHERE THE LOCATION OF THE REBAR DOWEL CONFLICTS WITH THE LOCATION OF OTHER REBAR, ADJUST REBAR SPACING TO ACCOMMODATE DOWEL. VERIFY ADJUSTED LAYOUT WITH RPR.
4. ON ALL CONTROL STRUCTURES TO BE EXPANDED, HEIGHTENED OR OTHERWISE MODIFIED, INSTALL 1-INCH DIAMETER, 18 INCH LONG, 12" O.C. ON ALL HORIZONTAL AND VERTICAL JOINTS BETWEEN NEW AND EXISTING CONCRETE. ALL WORK AND MATERIALS TO INSTALL DOWELS SHALL BE PAID UNDER THE RESPECTIVE CONTROL STRUCTURE MODIFICATION BID ITEMS.

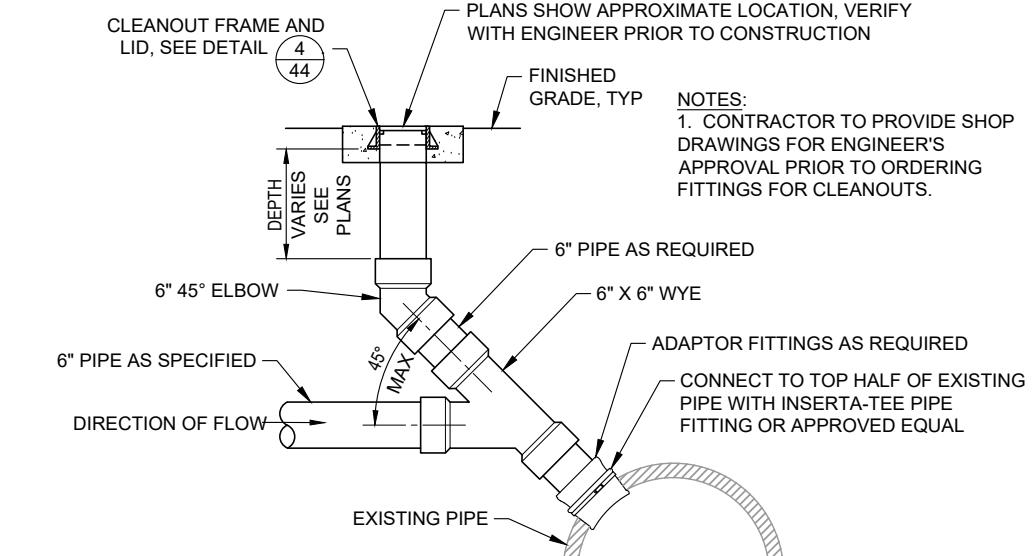
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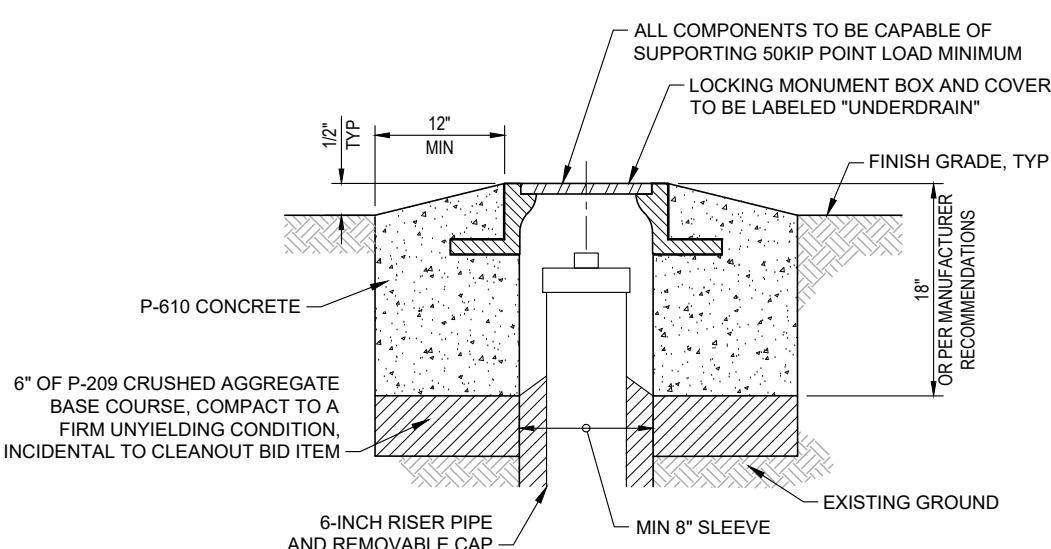
UNDERDRAIN  
CONNECTION WITH CLEANOUT DETAIL  
NTS



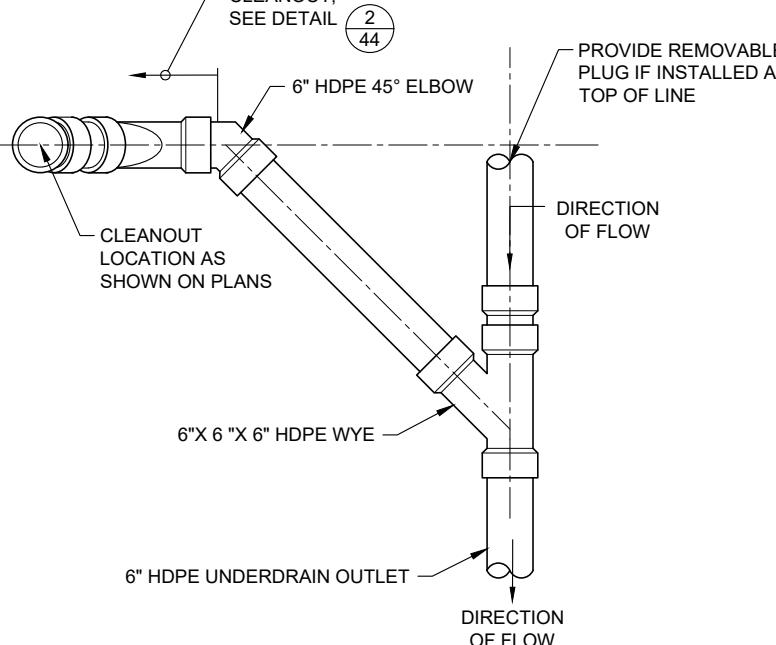
CLEANOUT DETAIL  
NTS



PIPE CONNECTION TO PIPE DETAIL  
NTS



CLEAN-OUT FRAME AND LID DETAIL  
NTS



VALLEY GUTTER  
UNDERDRAIN CLEANOUT DETAIL  
NTS

REPLACE ENTIRE SHEET



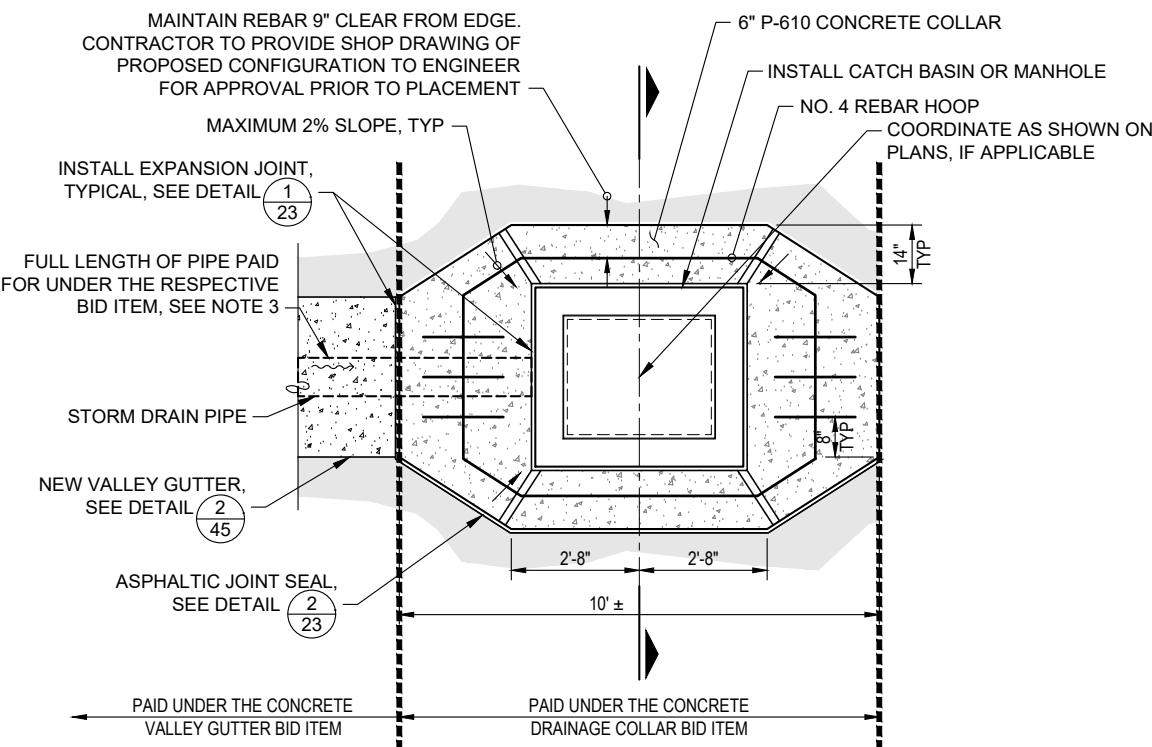
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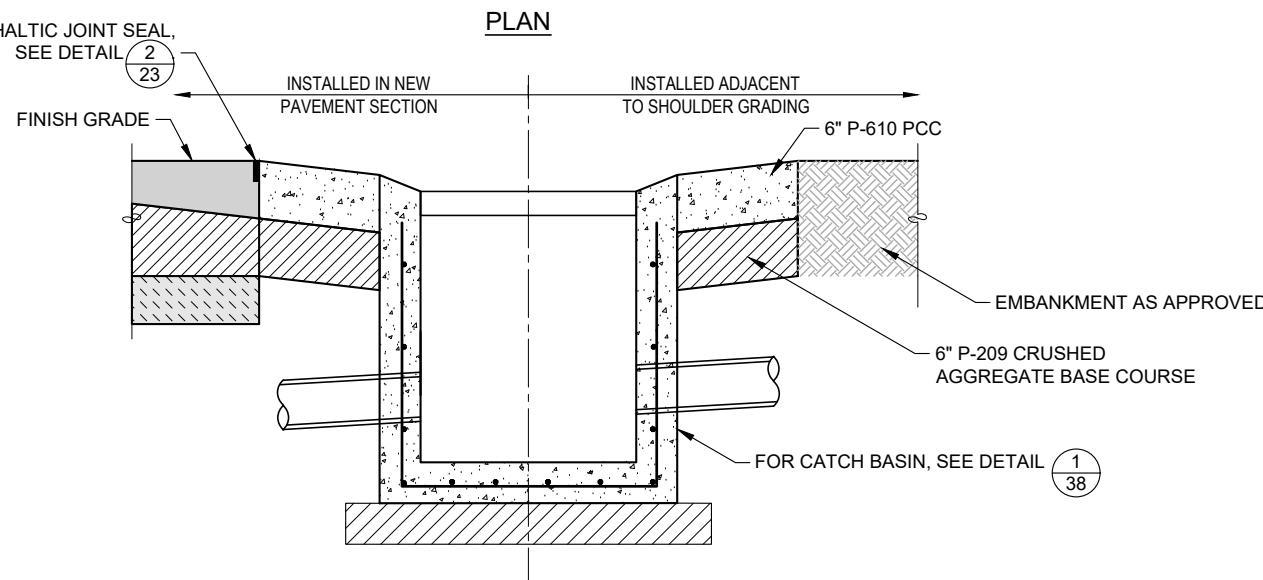
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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**44**  
OF **71**



PLAN



SECTION

## NOTES:

1. MODIFICATIONS TO CONCRETE DRAINAGE COLLAR DIMENSIONS SHOWN MAY BE NECESSARY TO FIT SPECIFIC FIELD CONDITIONS, CONTRACTOR TO PROVIDE SHOP DRAWING FOR ENGINEER APPROVAL PRIOR TO CONSTRUCTION OF CONCRETE DRAINAGE COLLAR.
2. ALL WORK AND MATERIALS INCLUDING FORMING, REBAR, JOINT SEALANT AND BASE ROCK REQUIRED FOR CONSTRUCTION OF THE CONCRETE DRAINAGE COLLAR SHALL BE PAID UNDER THE CONCRETE DRAINAGE COLLAR BID ITEM.
3. INSTALLATION OF THE CATCH BASIN, EACH CONNECTING PIPES, AND ABUTTING ASPHALT PAVEMENT WILL BE PAID UNDER EACH RESPECTIVE BID ITEM.

## CONCRETE COLLAR DETAIL

NTS

1

45

REPLACE ENTIRE SHEET

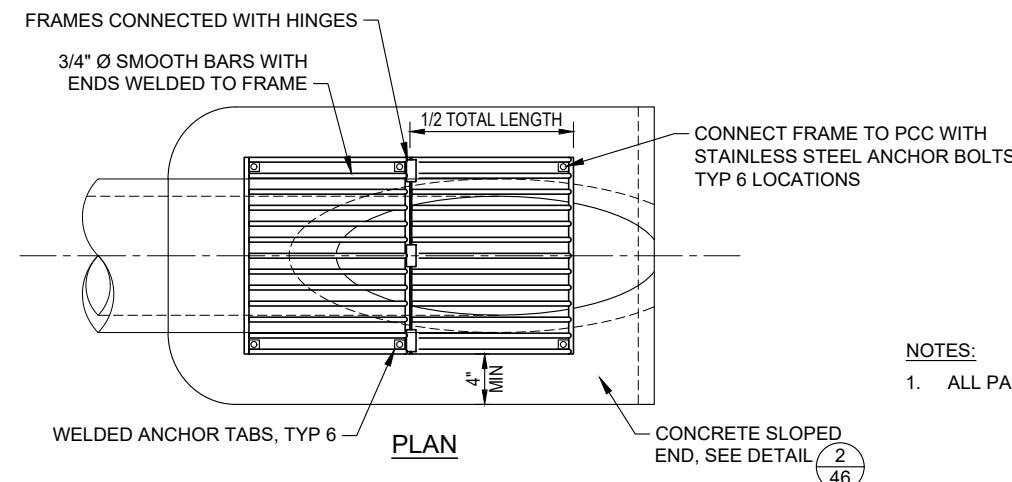
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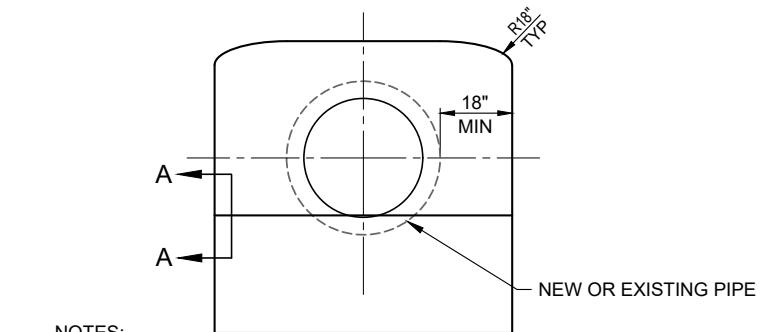
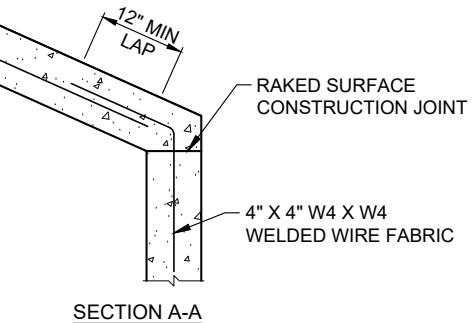
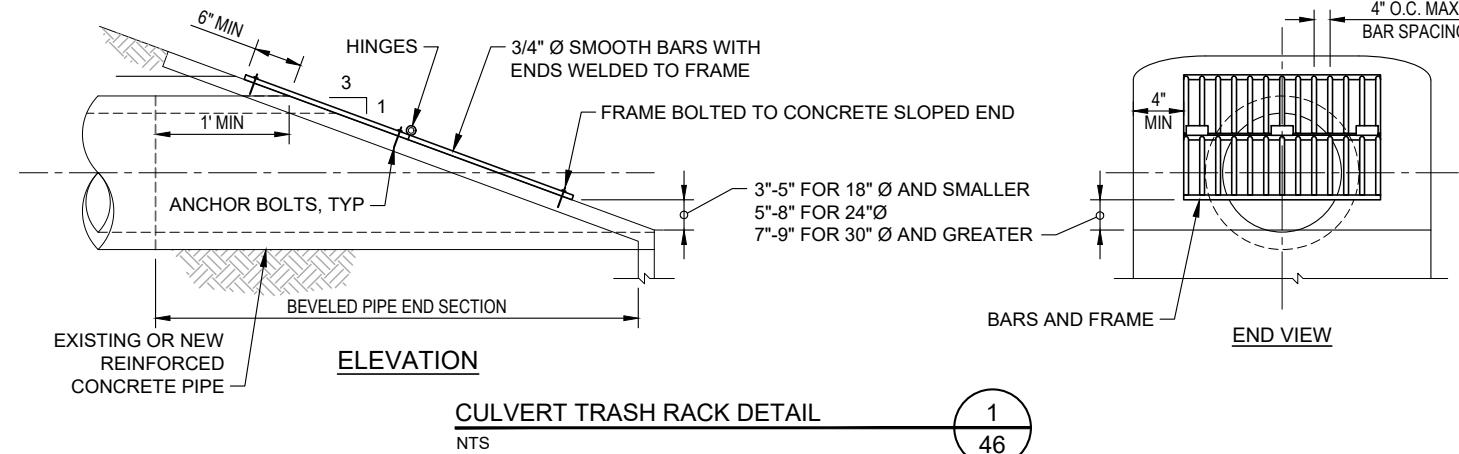
ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES
<b>DRAINAGE DETAILS</b>
PRECISION APPROACH ENGINEERING, INC. AIP NO. 3-41-0037-063

PROJECT NUMBER: <b>RV-2020-W04</b>
SHEET NO. <b>45</b>
OF <b>71</b>

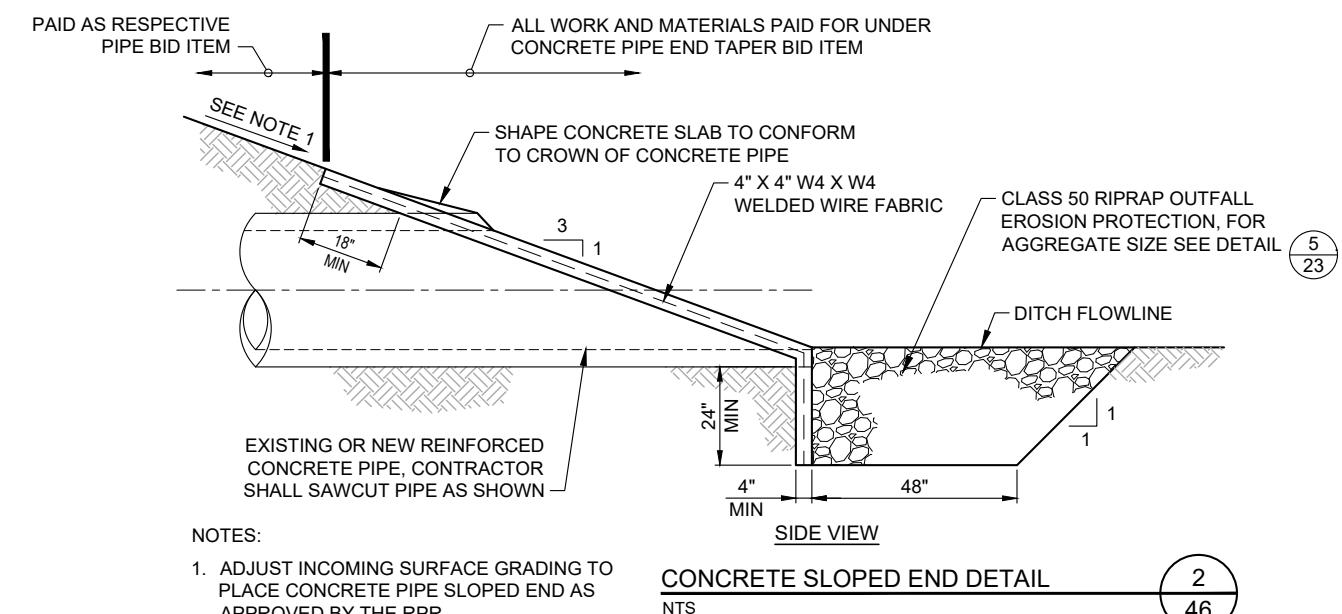




NOTES:  
1. ALL PARTS SHALL BE ALUMINUM OR STAINLESS STEEL

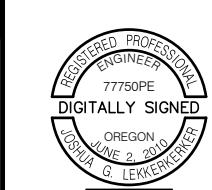


NOTES:  
1. ALL CONCRETE PIPE TAPERS TO BE ORIENTED PERPENDICULAR TO PIPE INSTALLED.



NOTES:  
1. ADJUST INCOMING SURFACE GRADING TO PLACE CONCRETE PIPE SLOPED END AS APPROVED BY THE RPR.

REPLACE ENTIRE SHEET

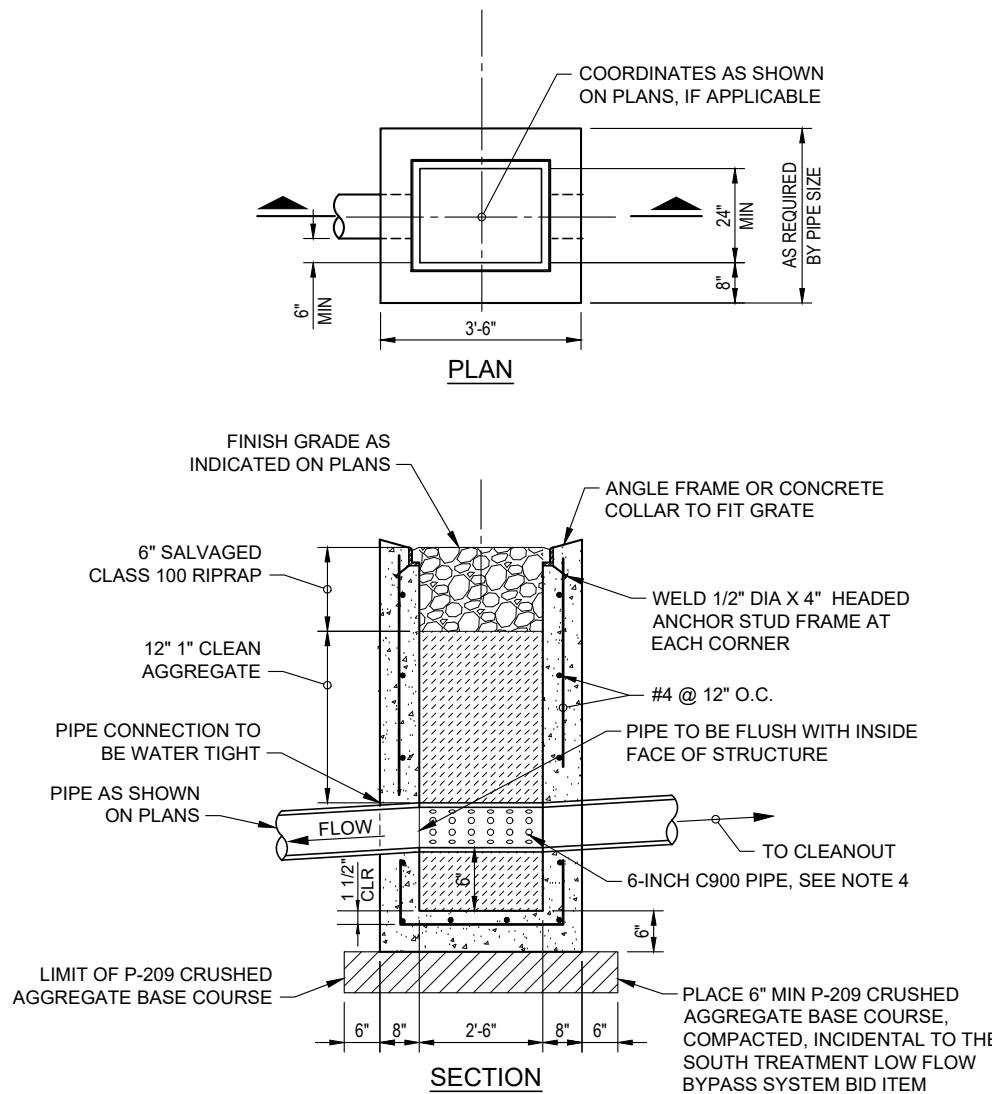


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REVISIONS:	DATE	APPD.

DATE: MAY 2024	DESIGN: KJK
DRAWN: JAW	CHECKED: JGL
REVISION NUMBER: 0	
SCALE: AS SHOWN	

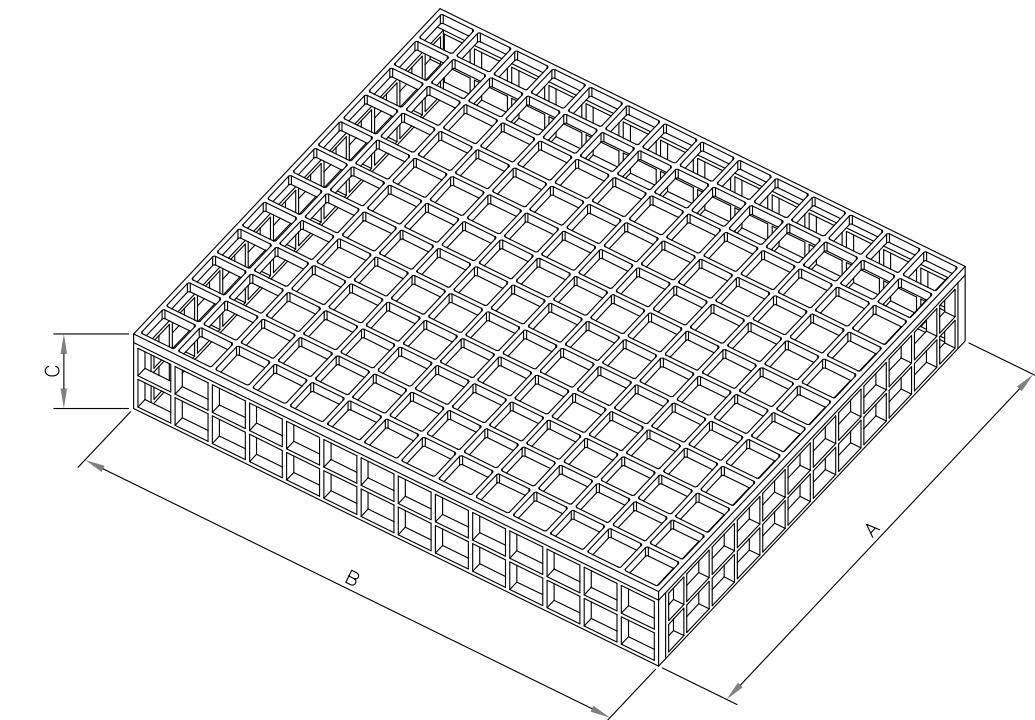
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**46**  
OF **71**



## NOTES:

1. DIMENSIONS OF CATCH BASIN MAY CHANGE, WITH ENGINEER'S APPROVAL, IF REQUIRED FOR COMPATIBILITY WITH FRAME AND GRATE OR DUE TO PIPE SIZE.
2. CONTRACTOR TO PROVIDE SHOP DRAWINGS FOR ENGINEERS APPROVAL PRIOR TO ORDERING STRUCTURES.
3. ALL COMPONENTS SHALL BE CAPABLE OF SUPPORTING H20 POINT LOAD MINIMUM.
4. C900 PIPE WITHIN THE STRUCTURE SHALL HAVE 1/2" DRILLED HOLES ON A 2-INCH GRID.
5. THE UPSTREAM CLEANOUT SHALL BE LOCATED 7 FEET AWAY FROM THE EDGE OF THE STRUCTURE AT A MINIMUM.

SOUTH TREATMENT  
LOW FLOW BYPASS SYSTEM DETAIL1  
47

DIMENSION	MINIMUM DIMENSIONS (INCHES)	
	POUND OUTLET STRUCTURE	POUND BYPASS STRUCTURE
"A"	16	18
"B"	27	72
"C"	24	24

## NOTES:

1. TRASHRACKS SHALL BE CONSTRUCTED OF CONSTRUCTED USING HIGH DENSITY POLYETHYLENE STRUCTURAL PLASTIC OR GALVANIZED STEEL.
2. FOR MINIMUM DIMENSIONS OF TRASHRACK, SEE TABLE.
3. TRASH RACK SHALL BE ATTACHED TO THE CONCRETE CONTROL STRUCTURES AS RECOMMENDED BY THE MFG.
4. THE CONTRACTOR SHALL PROVIDE TRASHRACKS WITH DIMENSIONS SIMILAR TO THOSE SHOWN ABOVE OR AS OTHERWISE APPROVED BY THE RPR.
5. TRASHRACK GAPS SHALL BE 2" MINIMUM SIZE OR AS OTHERWISE RECOMMENDED BY THE MFG AND APPROVED BY THE RPR.

CONTROL STRUCTURE  
TRASHRACK DETAIL

NTS

2  
47

REPLACE ENTIRE SHEET

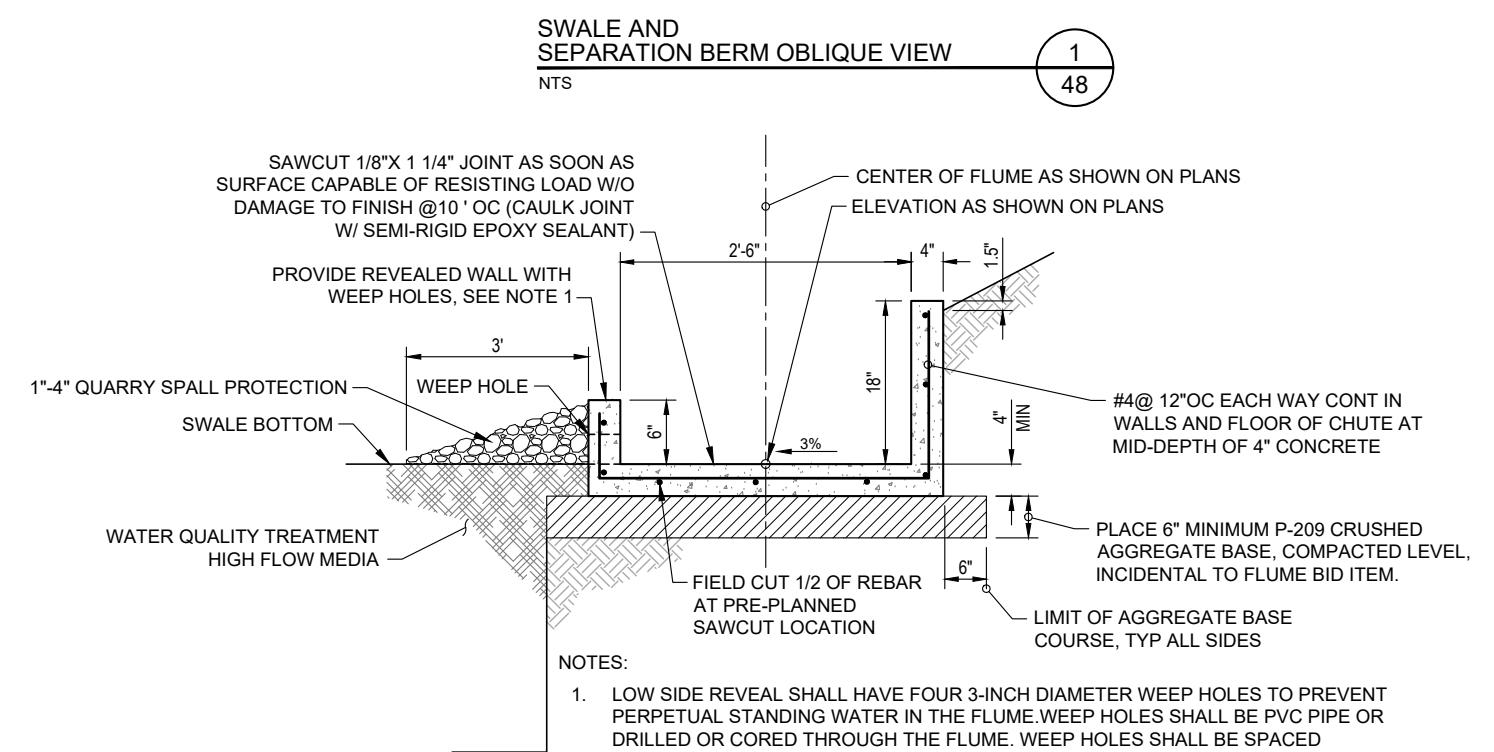
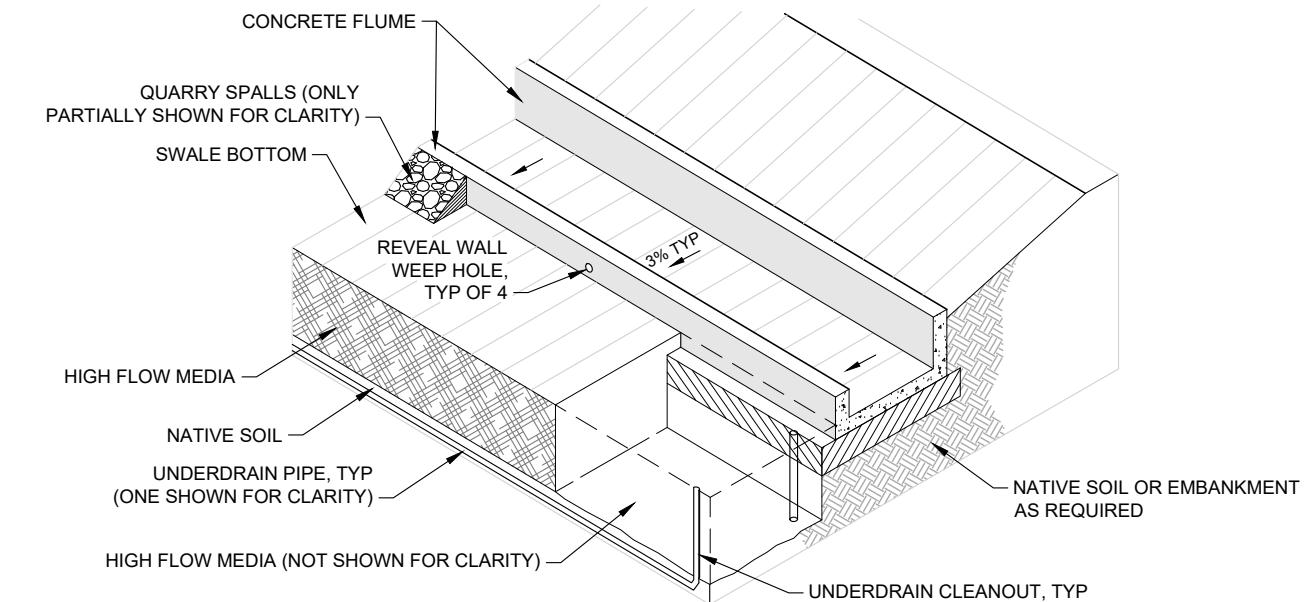
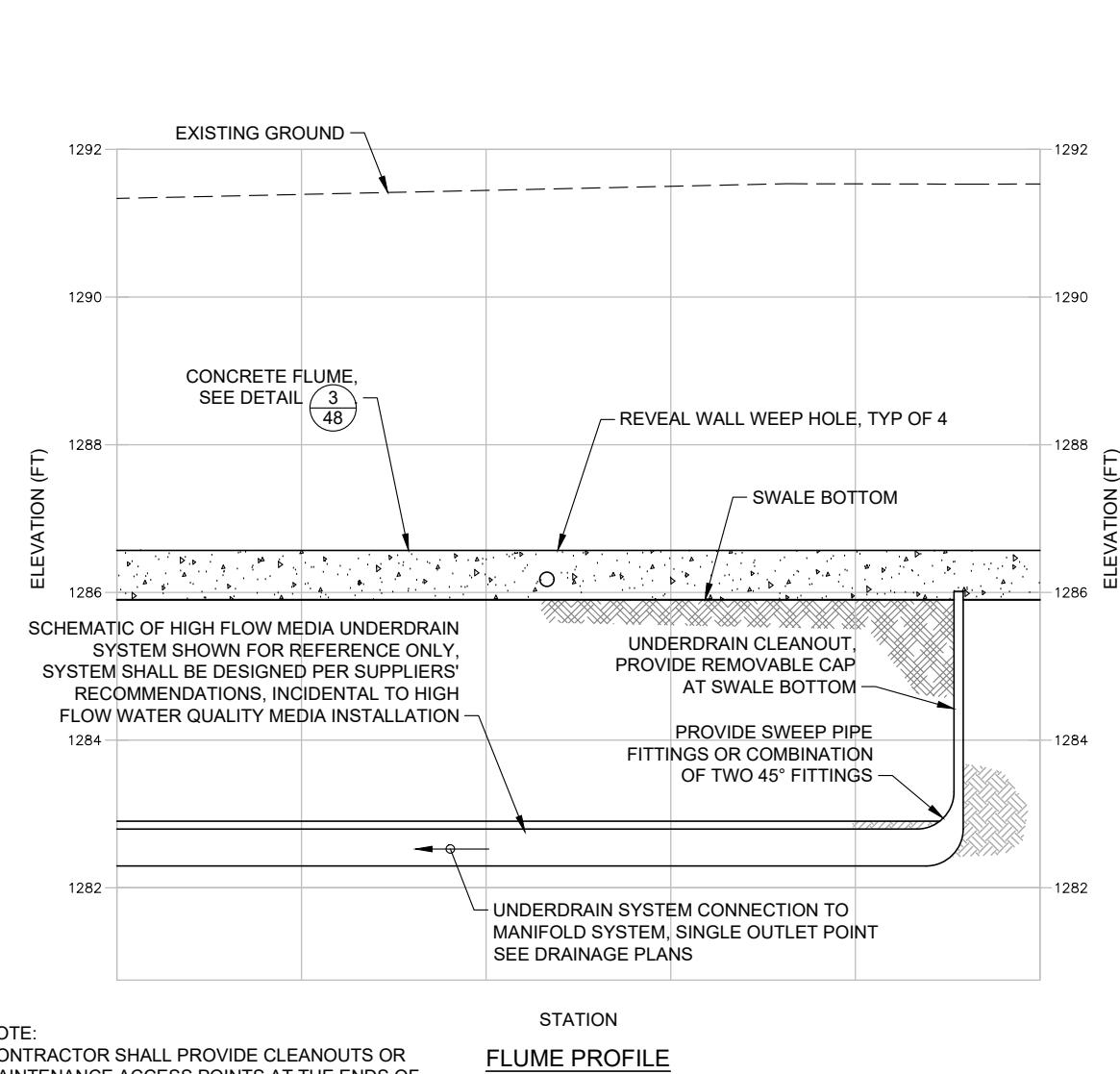


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REVISIONS: 5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

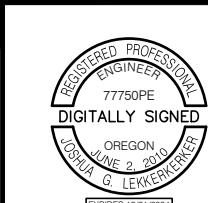
PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**47**  
OF **71**



CONCRETE FLUME DETAIL  
NTS

3  
48

REPLACE ENTIRE SHEET



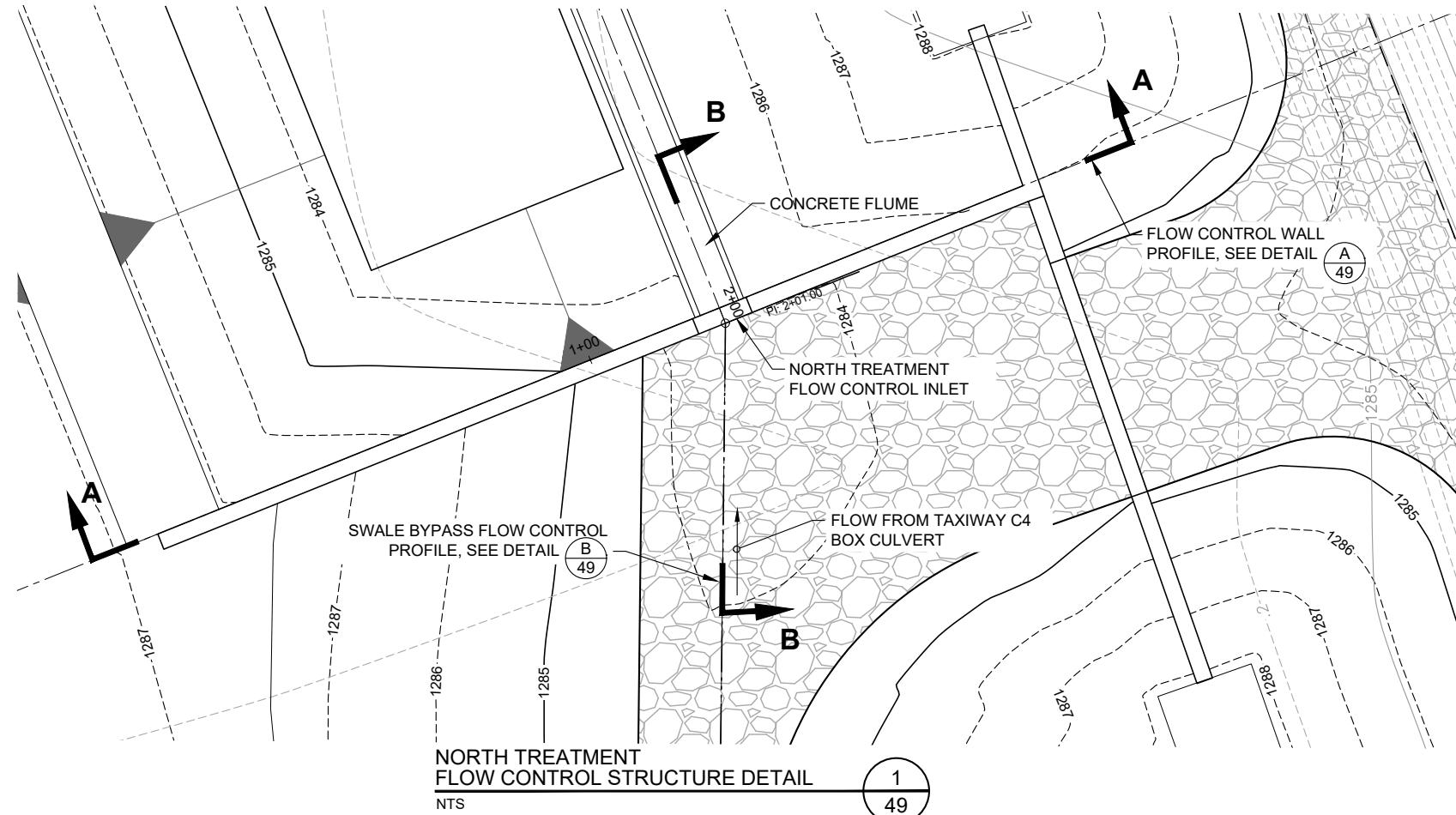
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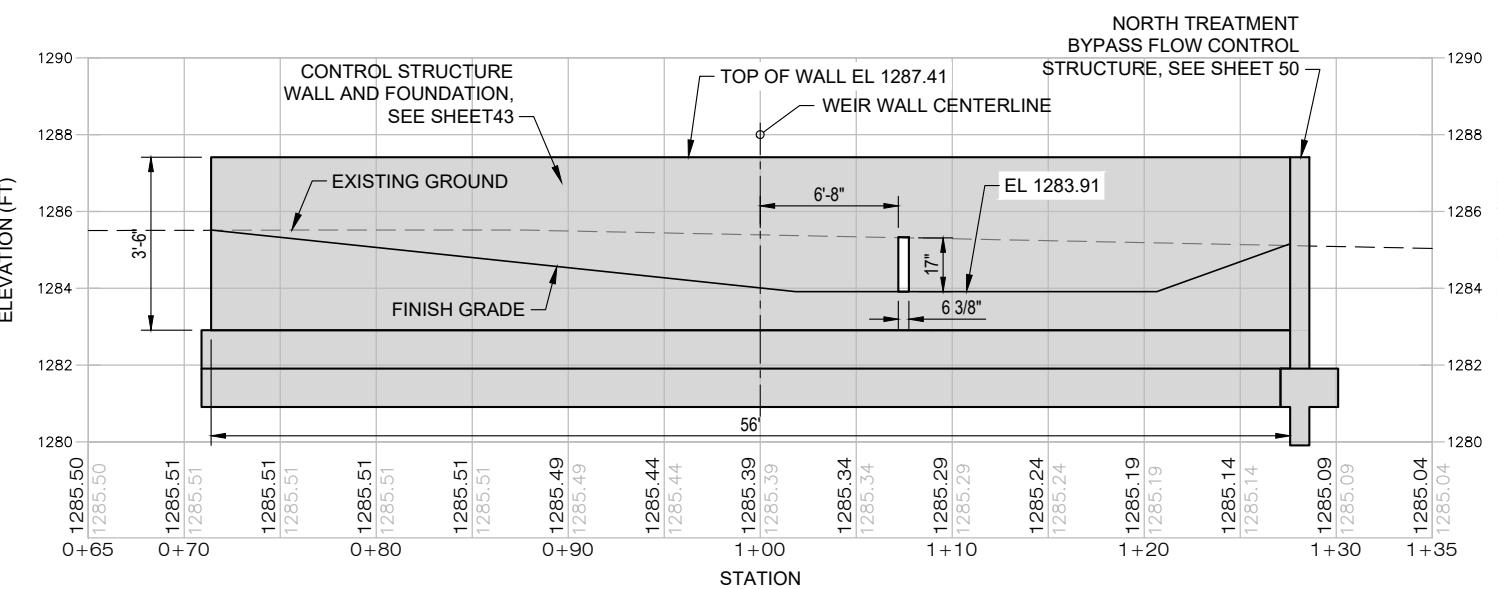
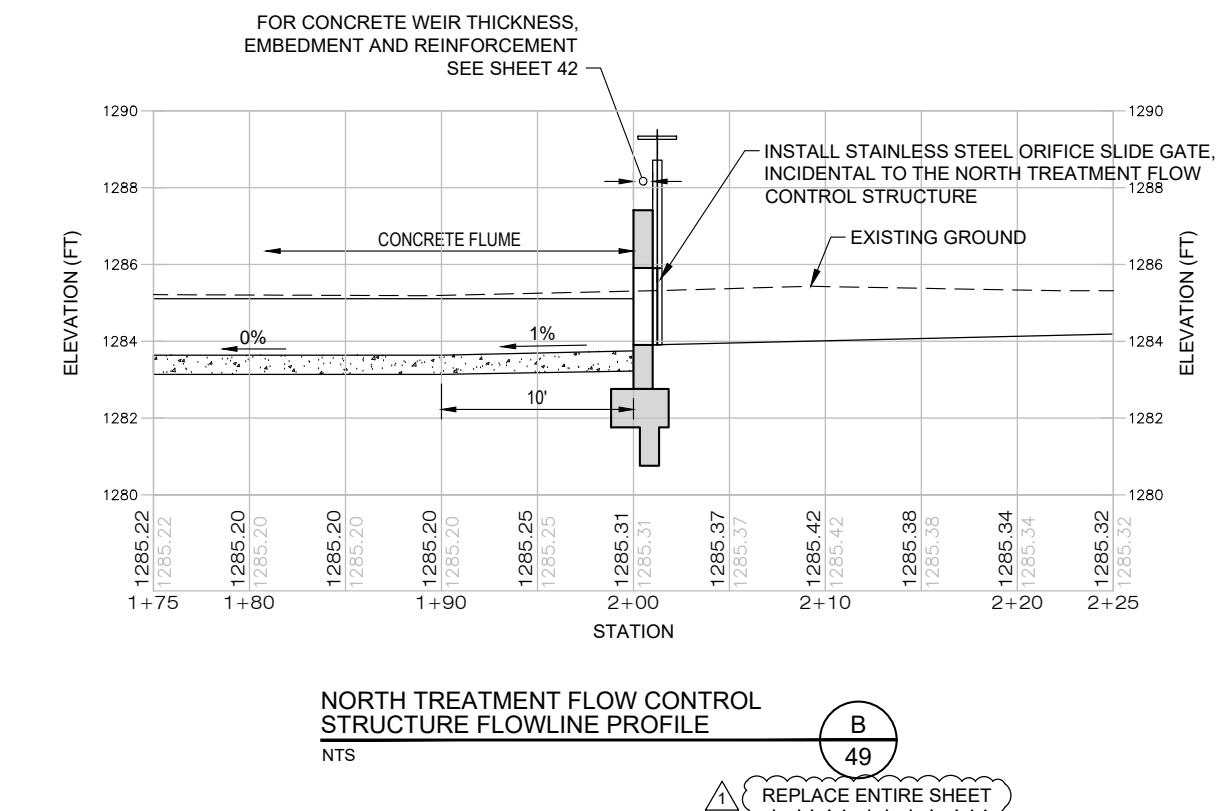
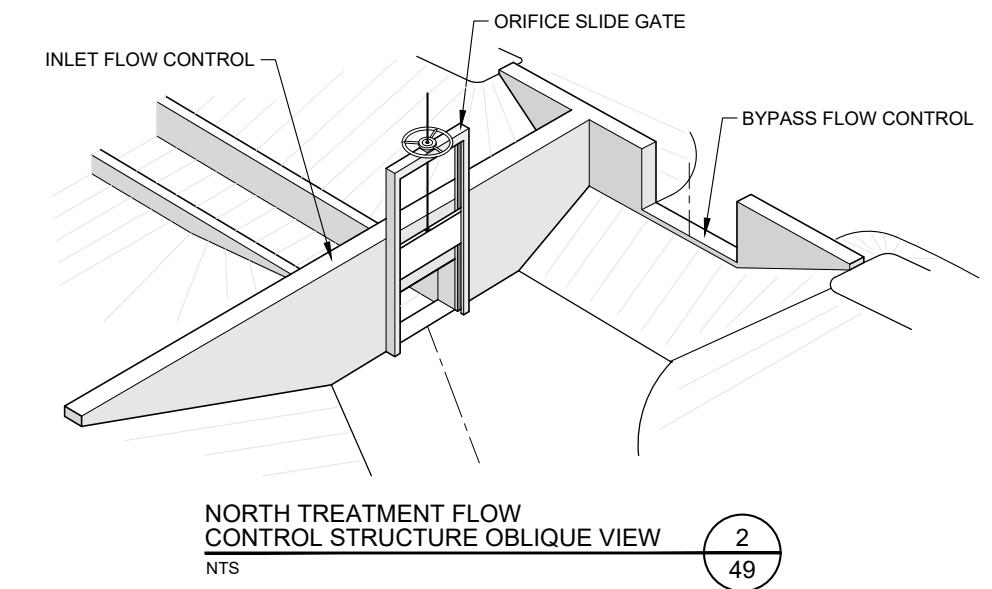
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**DRAINAGE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV\I-2020-W04**

SHEET NO.  
**48**  
OF **71**



NORTH  
0 5 10  
SCALE IN FEET



NORTH TREATMENT FLOW CONTROL  
STRUCTURE WALL PROFILE  
NTS

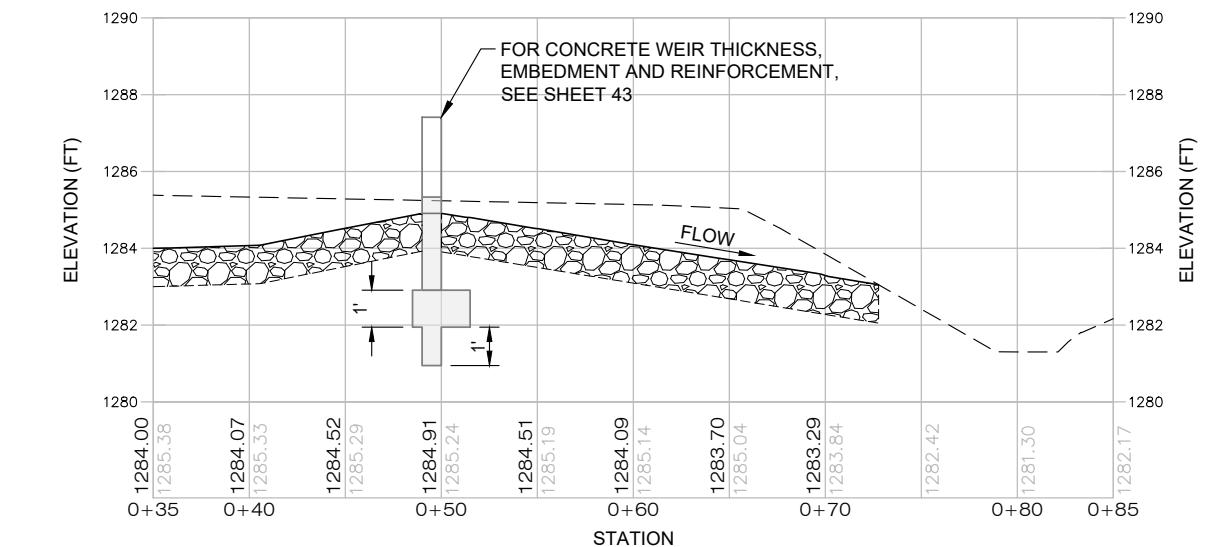
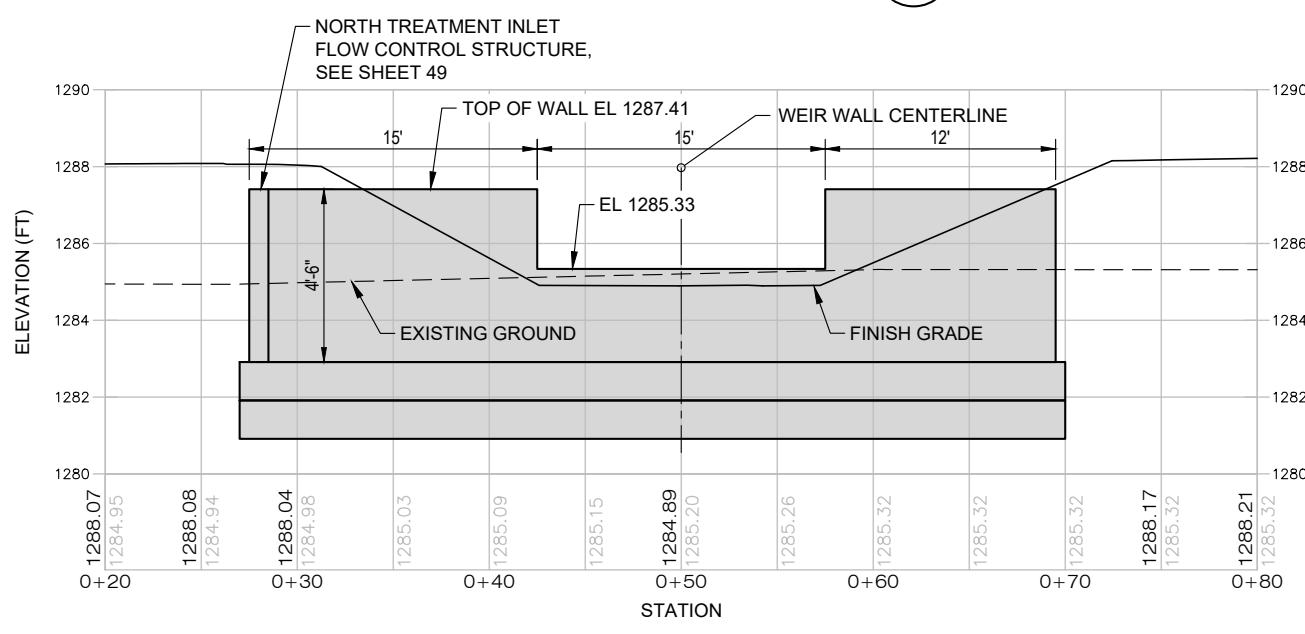
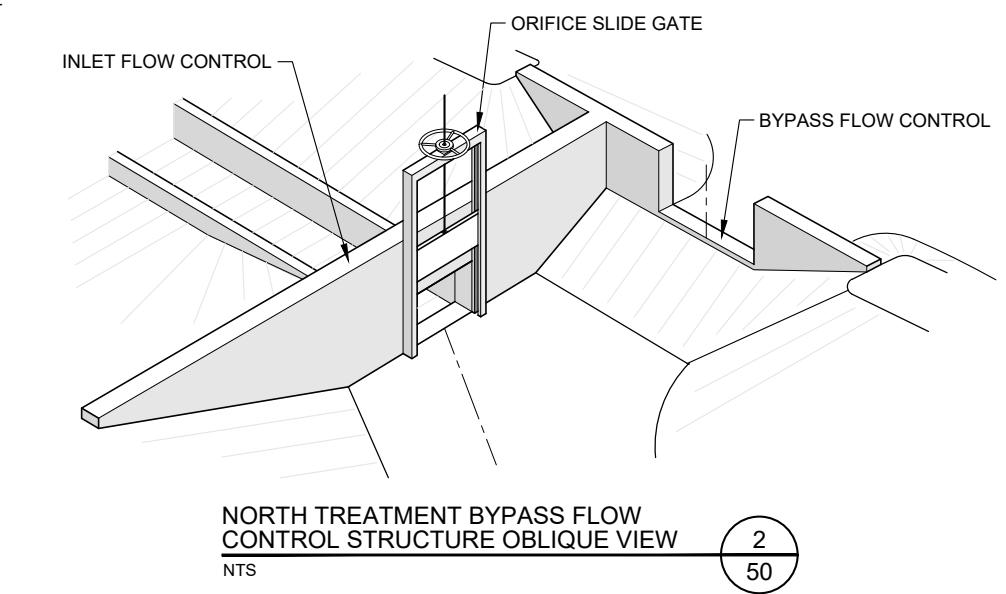
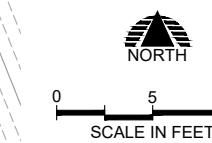
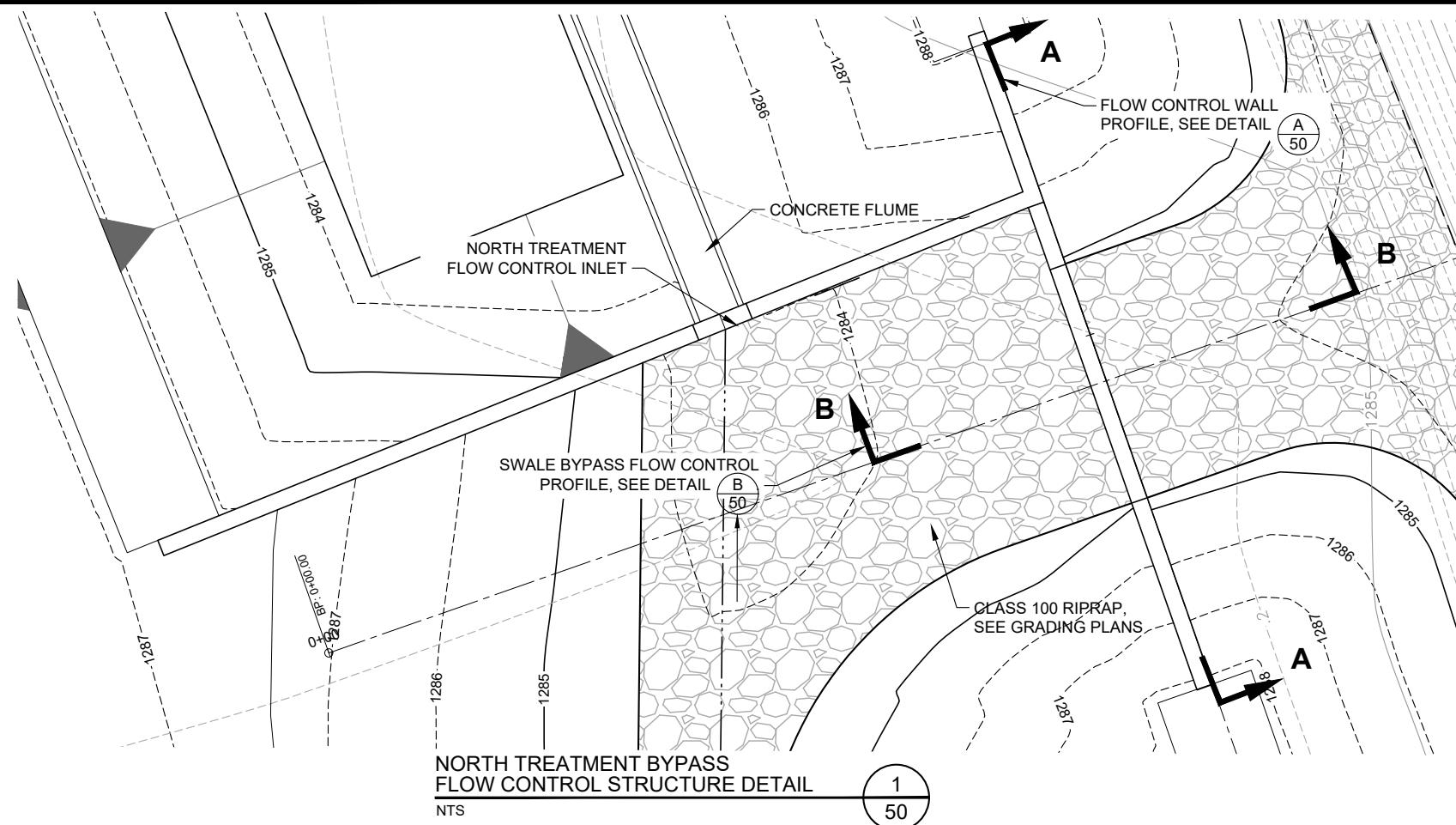


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REVISIONS:  
5/24/2024 JGL  
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SCALE: AS SHOWN

DATE: MAY 2024  
DESIGN: KJK  
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REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
NORTH TREATMENT INLET  
STRUCTURE DETAILS  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
RV-2020-W04  
SHEET NO.  
**49**  
OF 71



**NORTH TREATMENT FLOW CONTROL  
STRUCTURE WALL PROFILE**



DATE:	MAY 2023
DESIGN:	KJ
DRAWN:	JAV
CHECKED:	JG
REVISION NUMBER:	
SCALE: AS SHOWN	

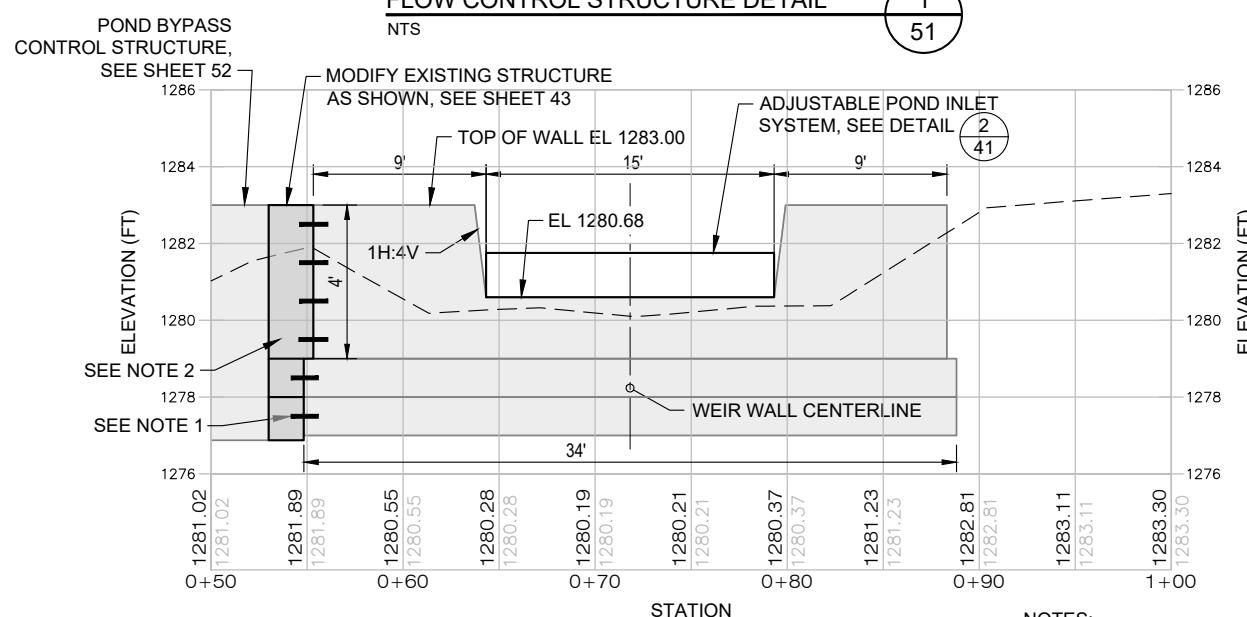
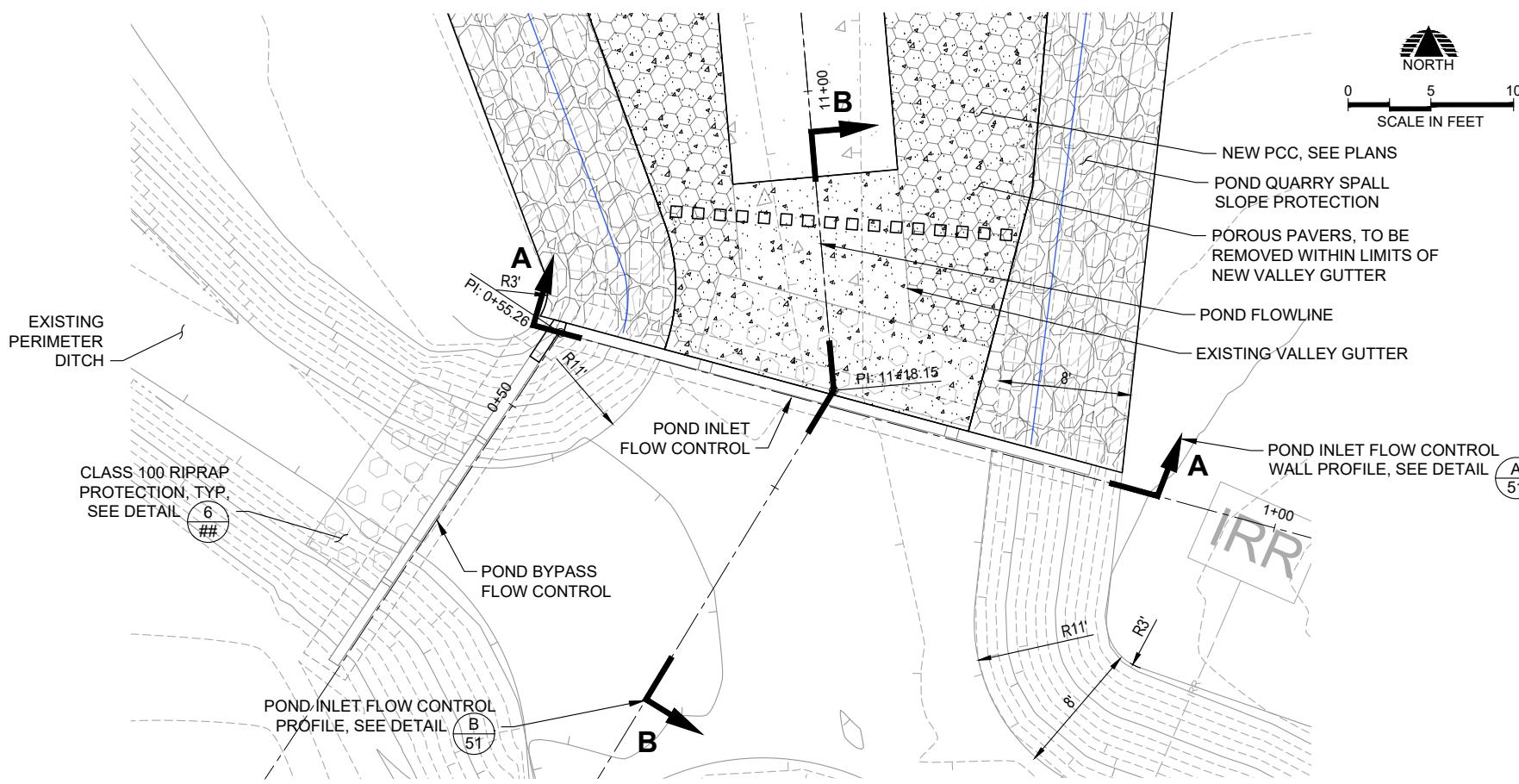
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

**NORTH TREATMENT BYPASS  
STRUCTURE DETAILS**

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**

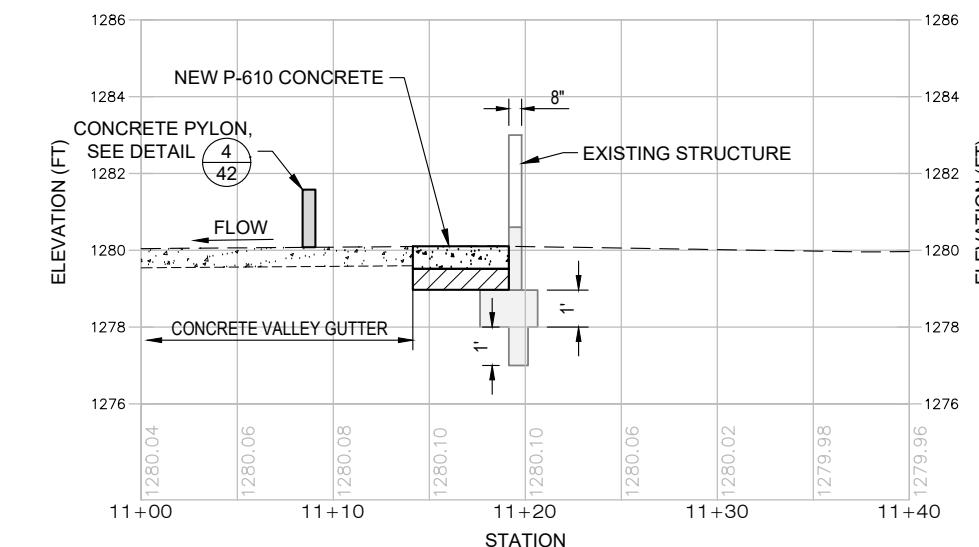
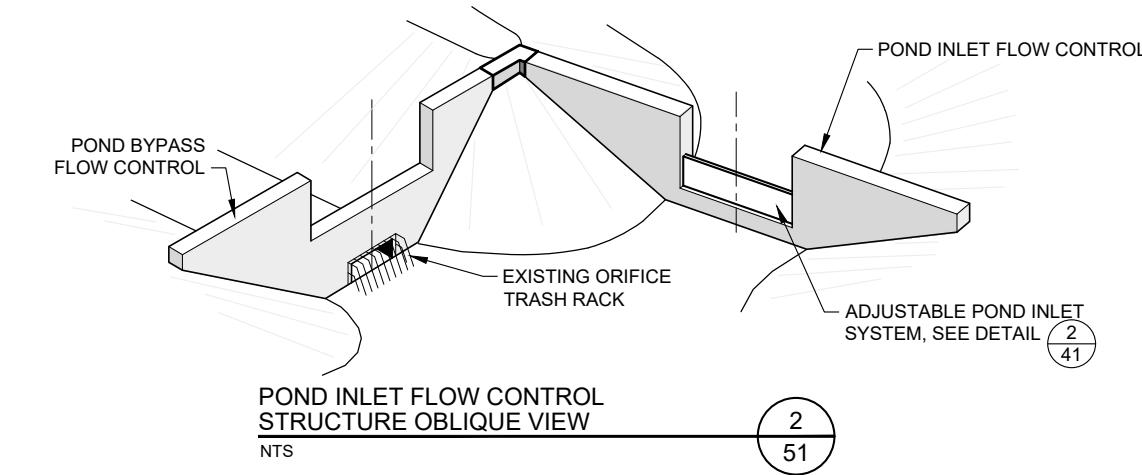
SHEET NO.  
**50**  
OF **71**



POND INLET FLOW CONTROL  
STRUCTURE WALL PROFILE  
NTS

NOTES:

1. INSTALL STEEL DOWELS BETWEEN NEW AND EXISTING CONCRETE, TYP, CENTER DOWELS ON KEY, SLAB FOUNDATION AND WALL, SEE CONTROL STRUCTURE DOWEL NOTES ON SHEET 43.
2. ALL WORK AND MATERIALS TO MODIFY THE EXISTING WALL AS SHOWN SHALL BE PAID UNDER THE POND INLET FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



POND INLET FLOW CONTROL  
STRUCTURE FLOWLINE PROFILE  
NTS

1 REPLACE ENTIRE SHEET

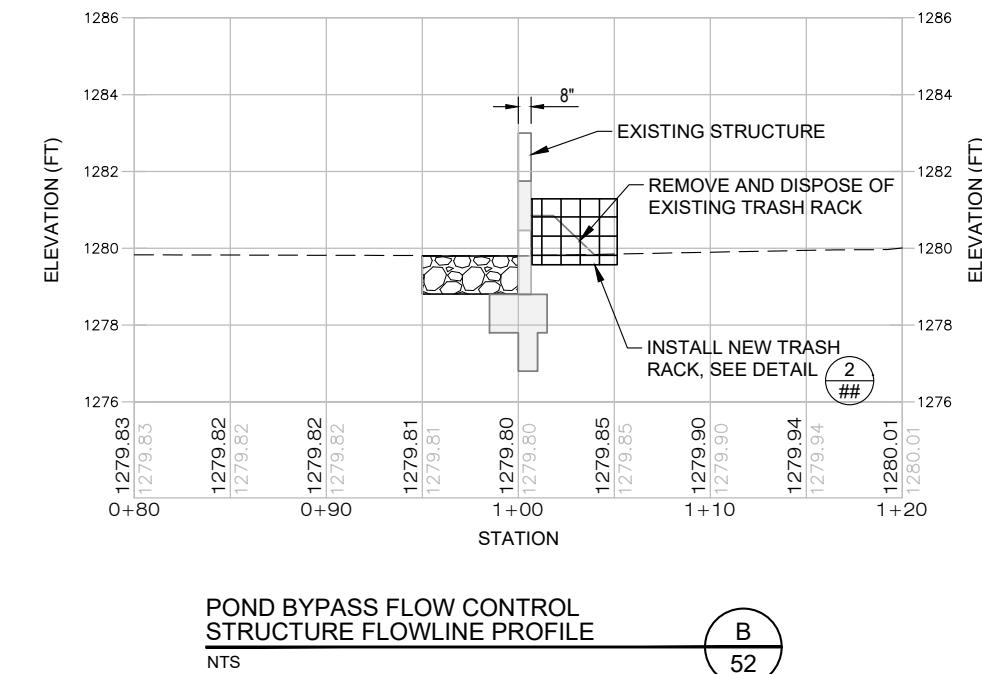
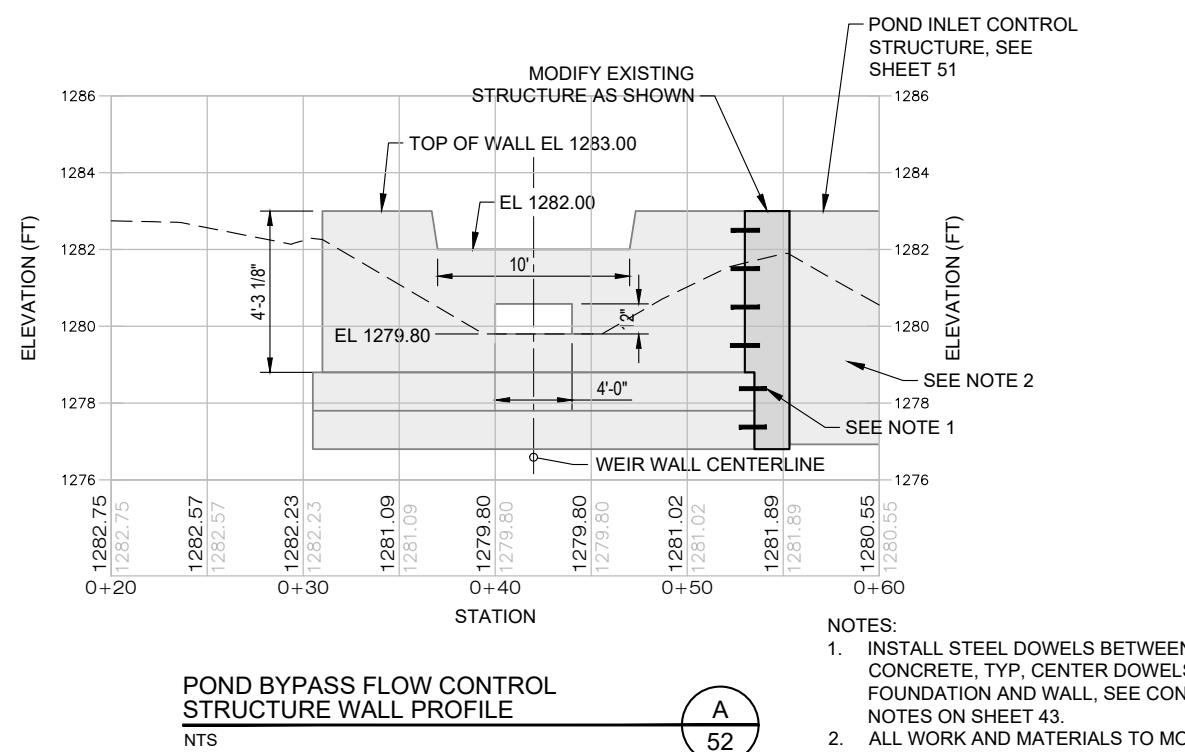
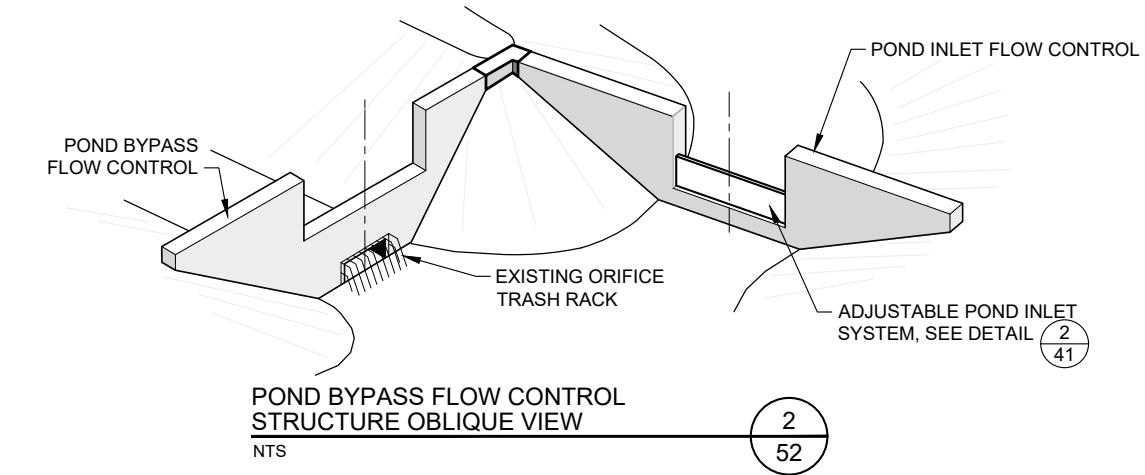
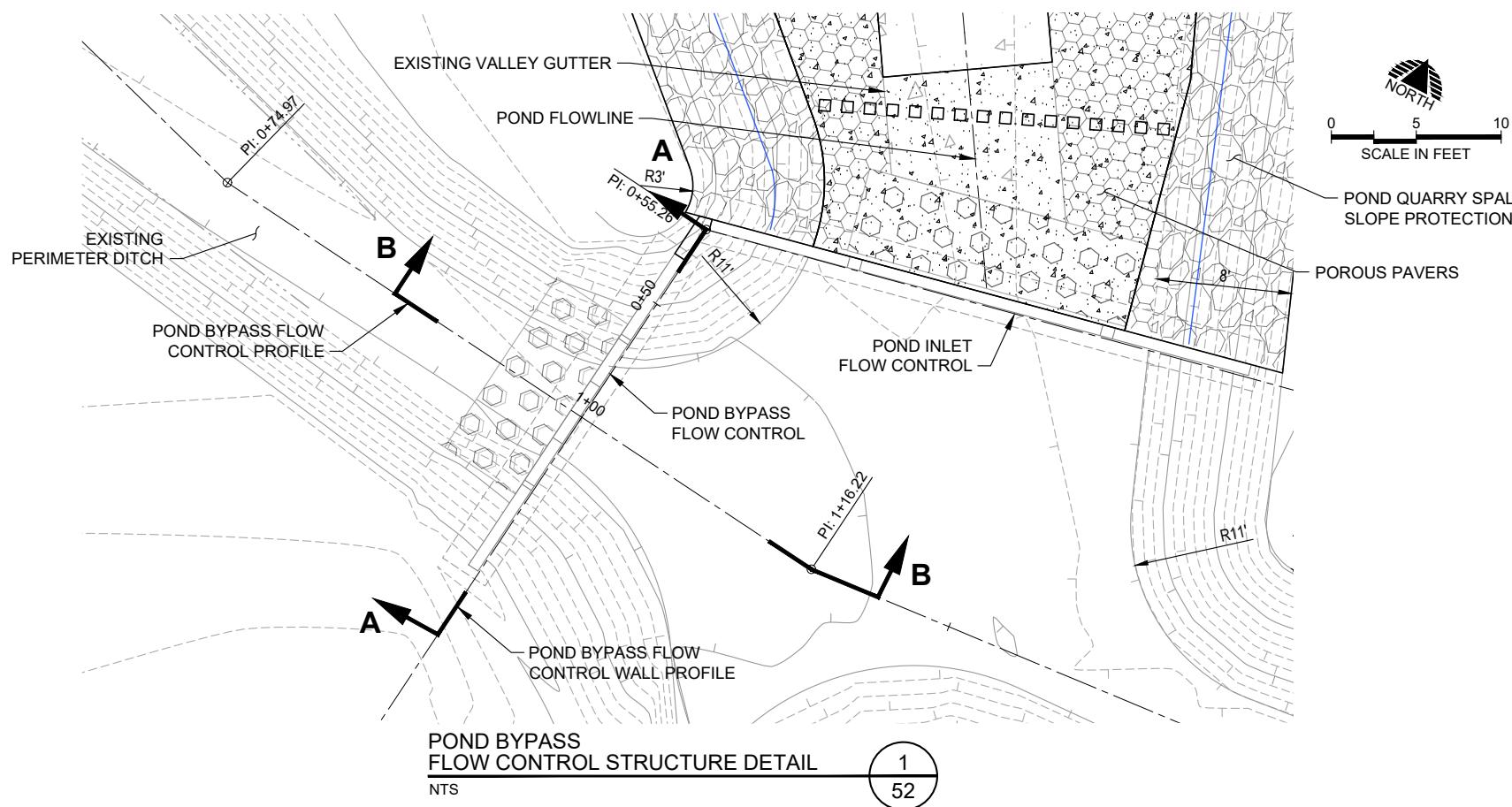


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DATE APPD.

DATE: MAY 2024  
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CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**POND INLET STRUCTURE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**51**  
OF **71**

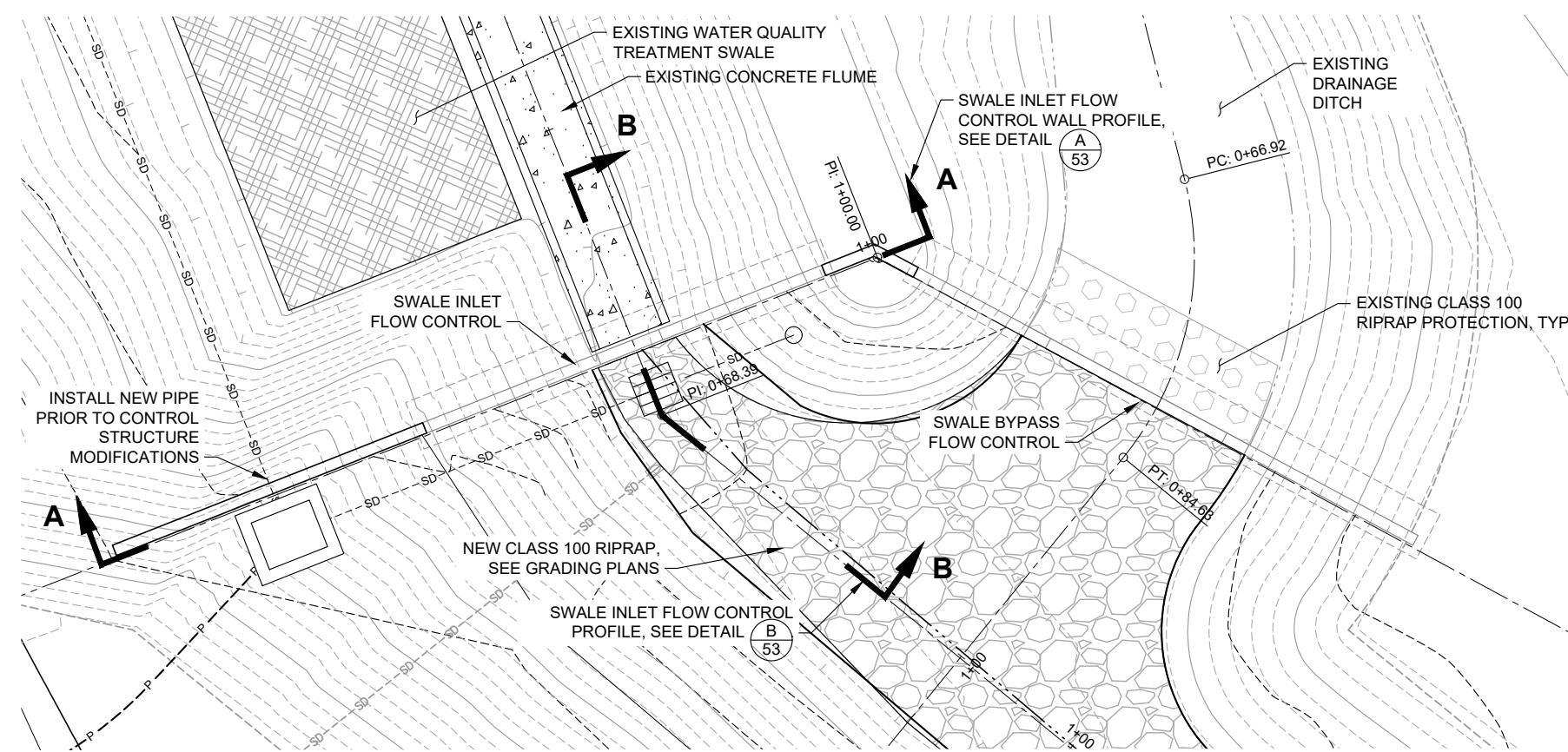


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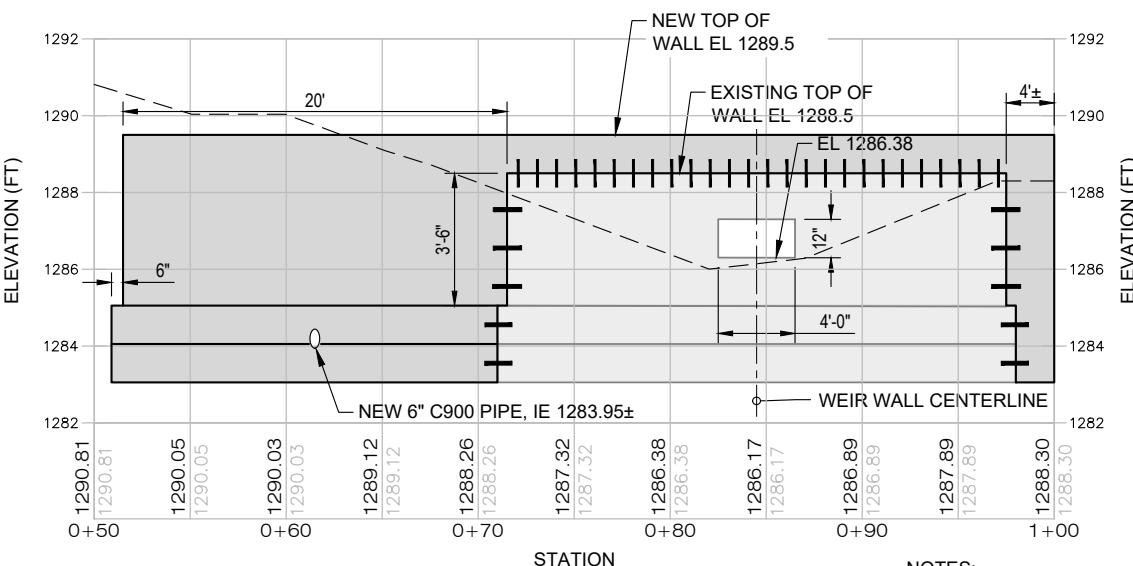
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**POND BYPASS STRUCTURE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**52**  
OF 71



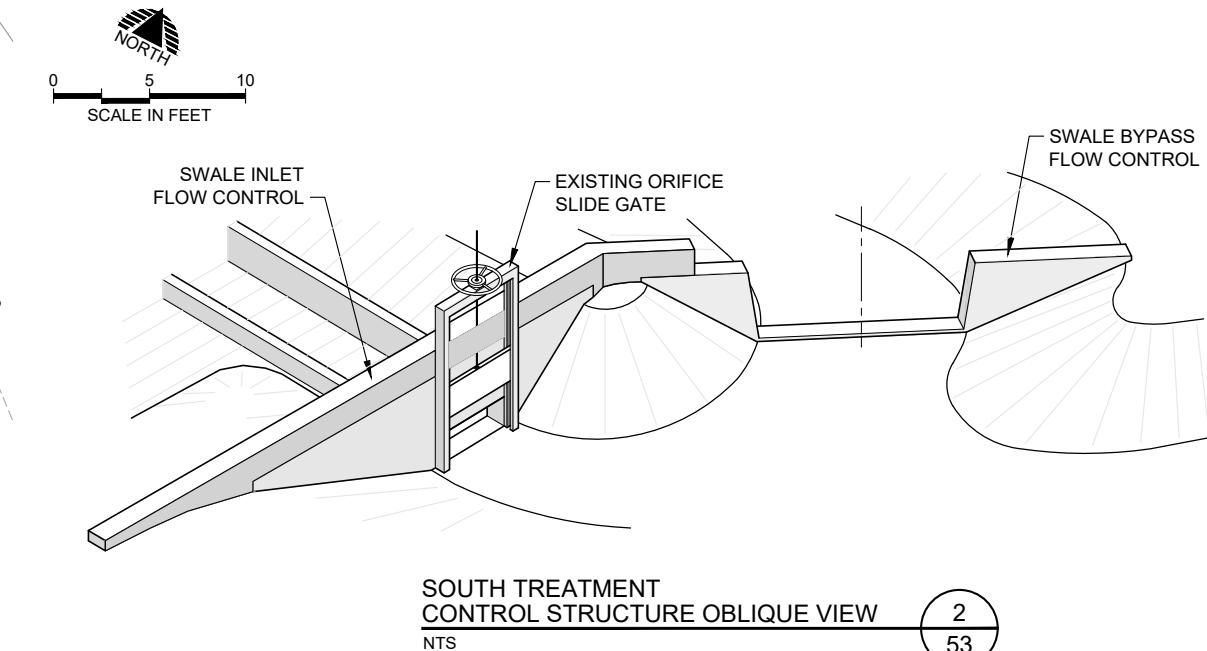
**SOUTH TREATMENT FLOW CONTROL STRUCTURE DETAIL**  
NTS



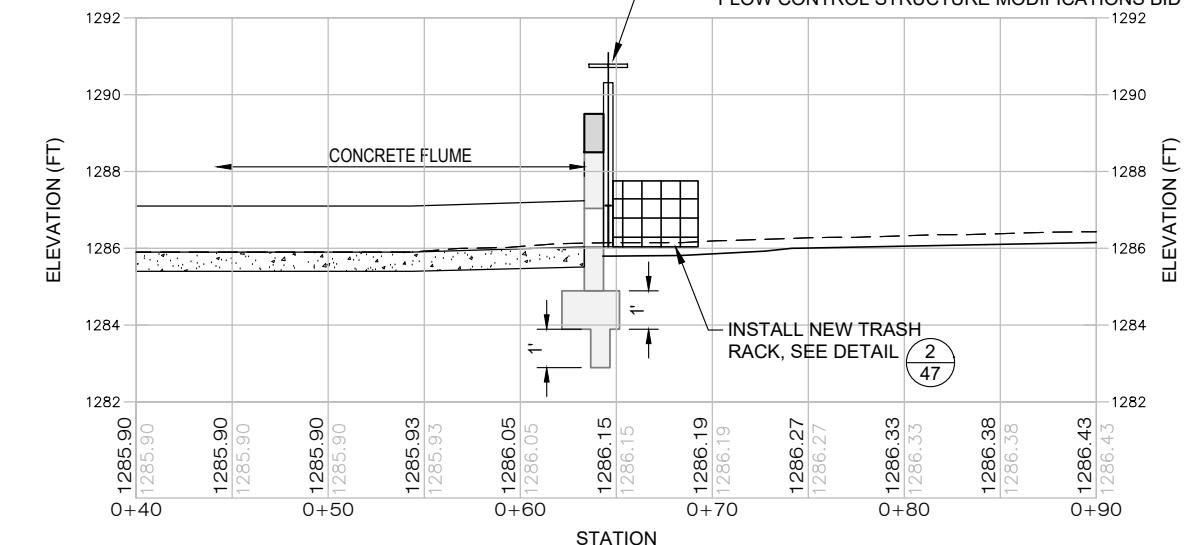
**SOUTH TREATMENT FLOW CONTROL STRUCTURE WALL PROFILE**  
NTS

NOTES:

1. INSTALL STEEL DOWELS BETWEEN NEW AND EXISTING CONCRETE, TYP, CENTER DOWELS ON KEY, SLAB FOUNDATION AND WALL, SEE CONTROL STRUCTURE DOWEL NOTES ON SHEET 43.
2. ALL WORK AND MATERIALS TO MODIFY THE EXISTING WALL AS SHOWN SHALL BE PAID UNDER THE POND BYPASS FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



**SOUTH TREATMENT CONTROL STRUCTURE OBLIQUE VIEW**  
NTS



**SOUTH TREATMENT FLOW CONTROL STRUCTURE FLOWLINE PROFILE**  
NTS

1 REPLACE ENTIRE SHEET



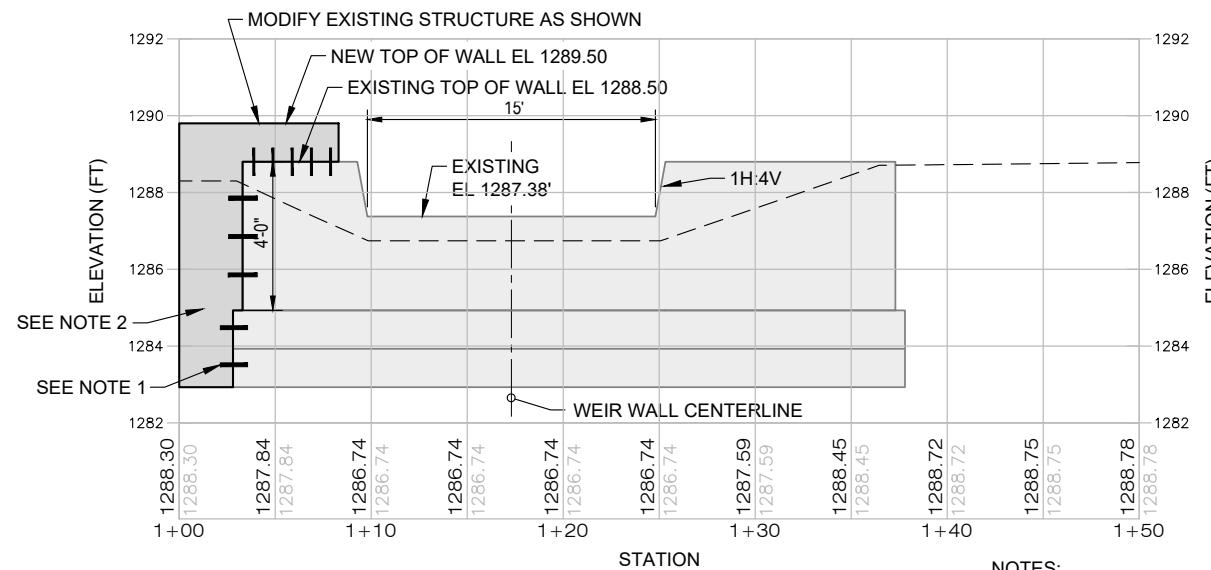
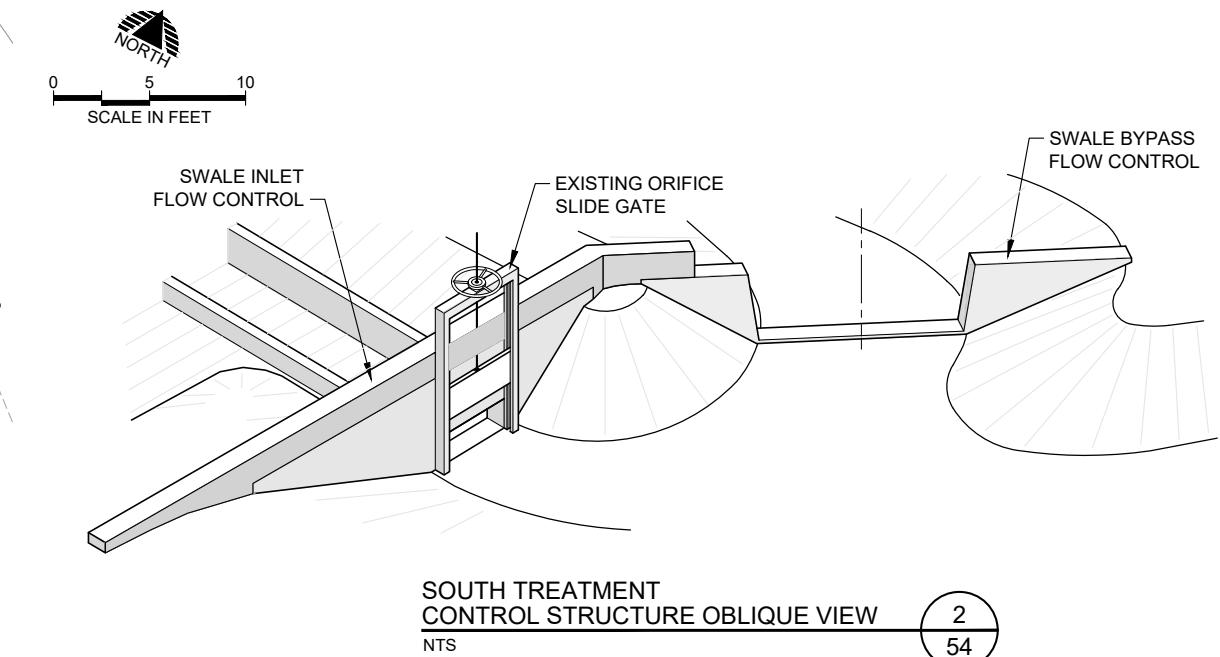
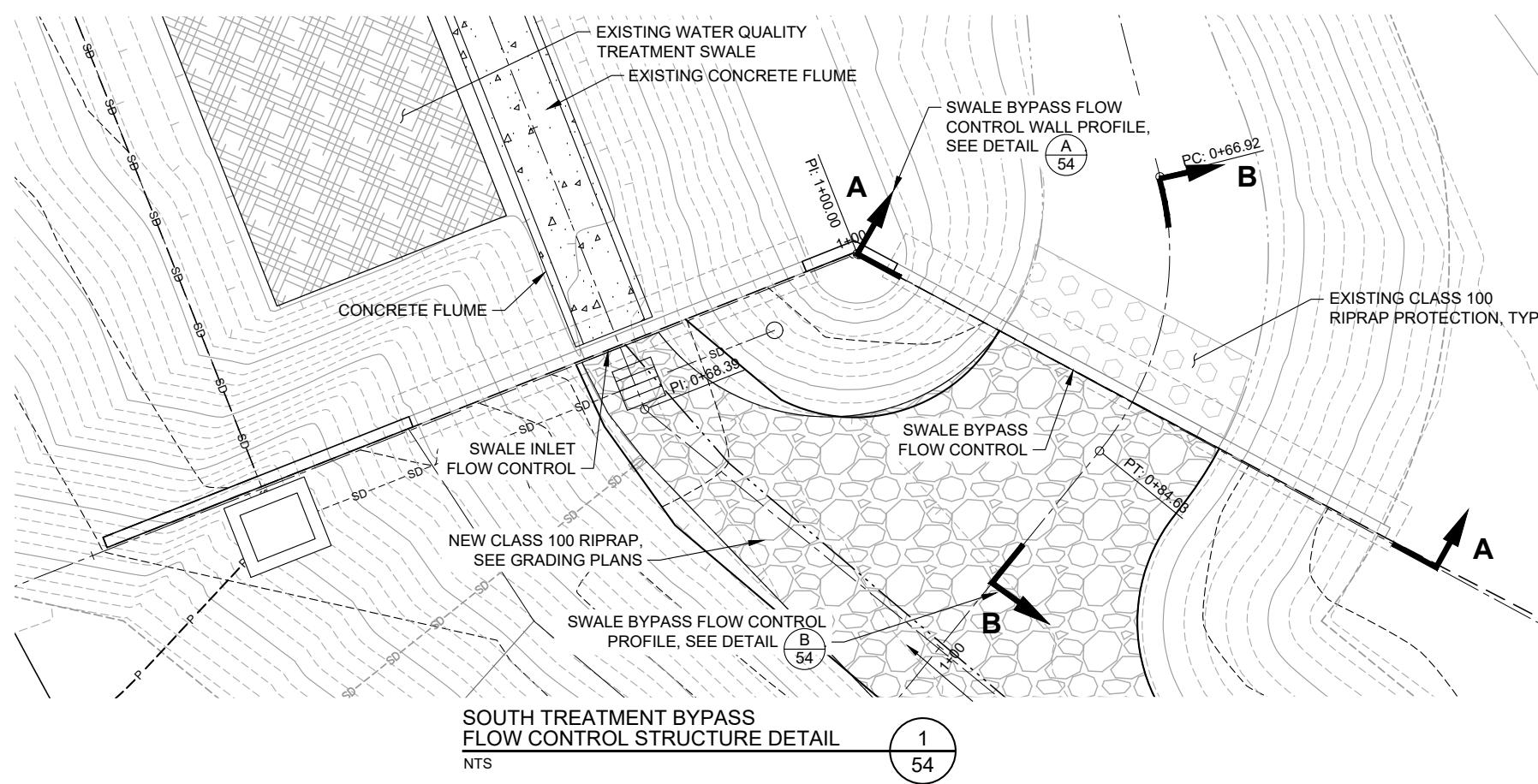
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REVISIONS:

5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**SWALE INLET STRUCTURE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**  
SHEET NO.  
**53**  
OF 71

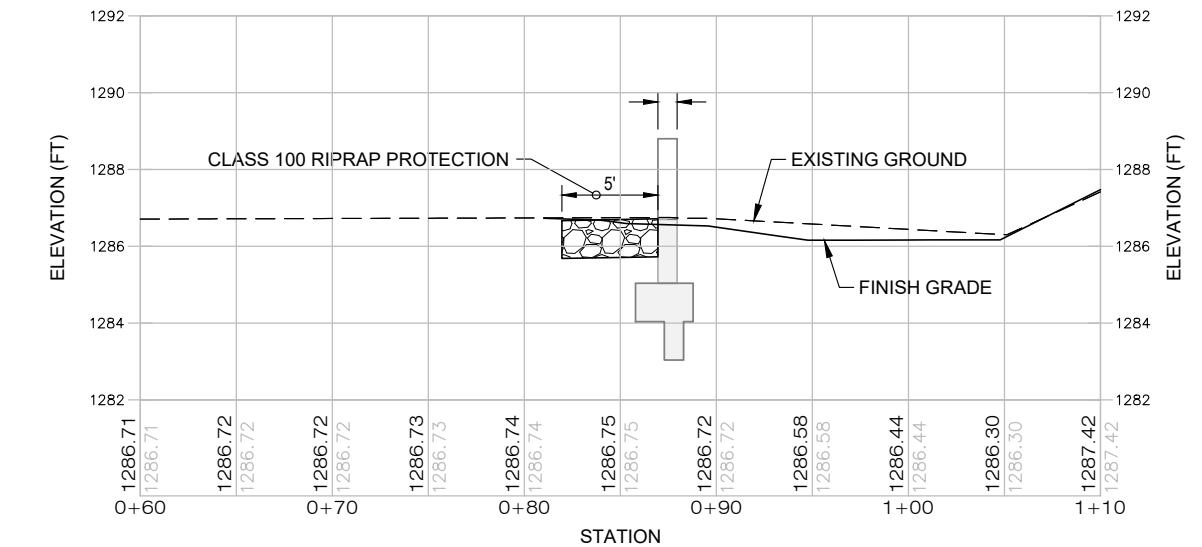


SOUTH TREATMENT BYPASS CONTROL  
STRUCTURE WALL PROFILE

NTS

NOTES:

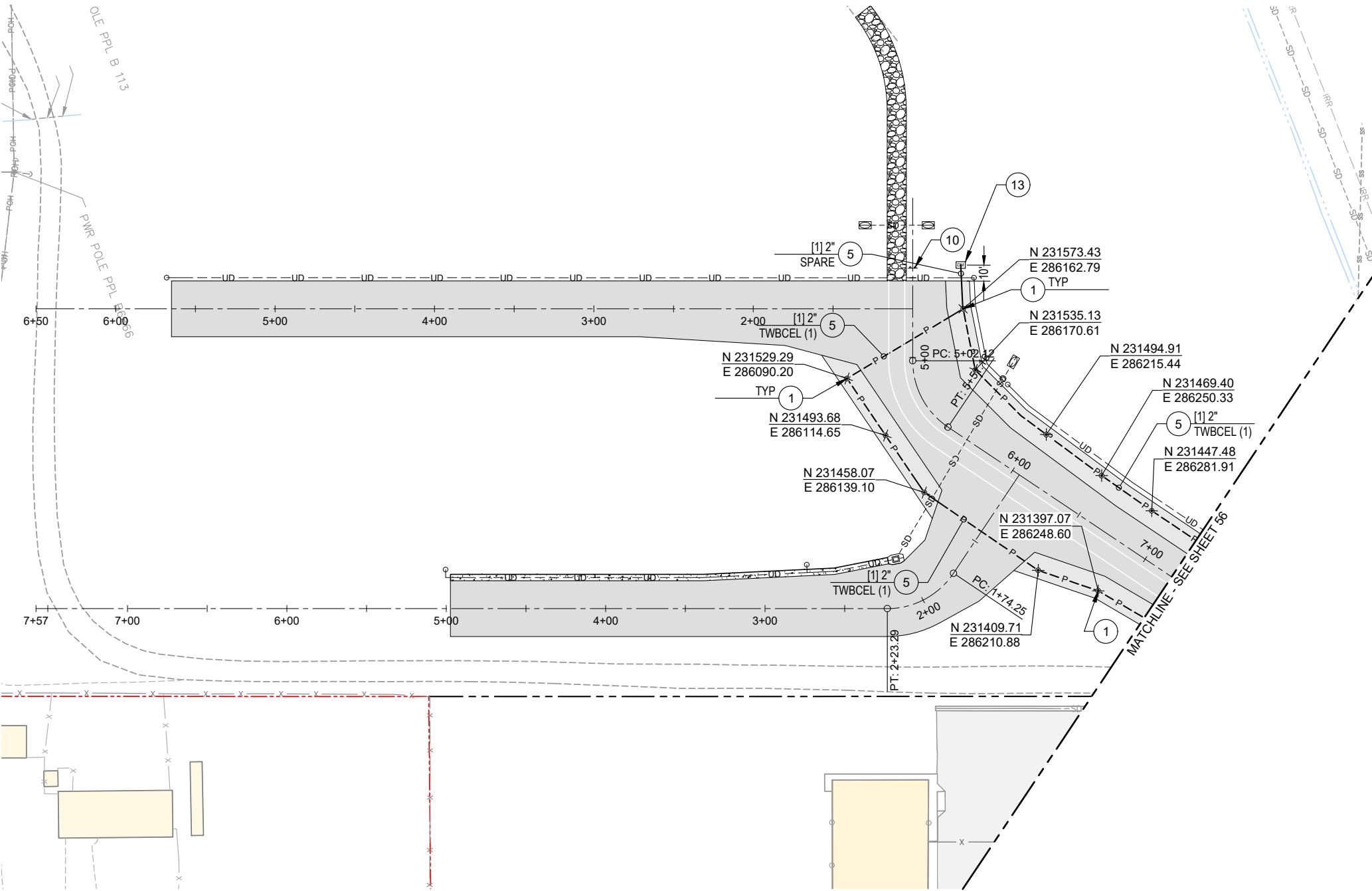
1. INSTALL STEEL DOWELS BETWEEN NEW AND EXISTING CONCRETE, TYP, CENTER DOWELS ON KEY, SLAB FOUNDATION AND WALL, SEE CONTROL STRUCTURE DOWEL NOTES ON SHEET 43.
2. ALL WORK AND MATERIALS TO MODIFY THE EXISTING WALL AS SHOWN SHALL BE PAID UNDER THE POND BYPASS FLOW CONTROL STRUCTURE MODIFICATIONS BID ITEM.



SOUTH TREATMENT BYPASS CONTROL  
STRUCTURE FLOWLINE PROFILE

NTS

REPLACE ENTIRE SHEET



A compass rose at the top indicates 'NORTH' with a vertical line and a half-circle of arrows. Below it is a horizontal scale bar with tick marks at 20, 40, and 80, labeled 'SCALE IN FEET'.

## ELECTRICAL CONSTRUCTION NOTES

- 1 INSTALL NEW LED ELEVATED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL  61
- 2 INSTALL NEW LED FLUSH-MOUNTED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL  61
- 3 REMOVE LIGHT AND TRANSFORMER AND INSTALL SOLID LID, SIMILAR TO DETAIL  64
- 4 INSTALL HANDHOLE, SEE DETAIL  60
- 5 INSTALL CONDUIT, SIZE AS NOTED, SEE DETAILS ON SHEET 59
- 6 REMOVE EXISTING CABLE AND INSTALL NEW CIRCUIT(S) IN EXISTING CONDUIT OR ELECTRICAL STRUCTURE, AS NOTED
- 7 EXPOSE AND CONNECT TO EXISTING CONDUIT
- 8 INSTALL GUIDANCE SIGN, SEE DETAIL  62 AND SHEET 58 FOR PANEL LEGEND
- 9 CONCRETE ENCASEMENT OF DUCTS, SEE DETAIL  60
- 10 INSTALL RETROREFLECTIVE TAXIWAY END SIGN, SEE DETAIL  63
- 11 EXTEND EXISTING GUIDANCE SIGN FOUNDATION AND INSTALL NEW GUIDANCE SIGN, SEE DETAIL  63
- 12 GUIDANCE SIGN PANEL REPLACEMENT, SEE SHEET 58
- 13 INSTALL CONCRETE DUCT MARKER, SEE DETAIL  65
- 14 INSTALL AUTOMATIC VALVE SYSTEM, SEE DETAIL  38

**A**  SIDE 'A'  
**2** GUIDANCE SIGN DESIGNATION, SEE SHEET 58 FOR LEGEND  
**B**  SIDE 'B'

## ELECTRICAL LEGEND

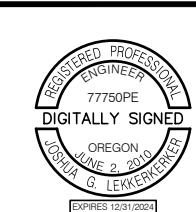
— C — NEW CIRCUITS TO BE PLACED IN EXISTING CONDUIT,  
REPLACE EXISTING CIRCUITS IN CONDUITS AS INDICATED

— P — NEW CONDUITS WITH NEW CIRCUITS, AND/OR NEW SPARE  
CONDUITS

(1) 2" (5) INDICATES NUMBER OF NEW CONDUITS AND SIZE

## CIRCUIT LEGEND

ABBREVIATION	DESCRIPTIONS	CABLE COLOR
TWBCEL	TAXIWAY B AND TAXIWAY C EDGE LIGHTS	TERRA COTTA
TWBCGS	TAXIWAY B AND TAXIWAY C GUIDANCE SIGN	GREY
TWAEL	TAXIWAY A EDGE LIGHTS	BLACK
VALVE PWR	THREE XX CONDUCTORS	TBD
(1)	INDICATES NUMBER OF NO.8 5KV CABLES PER CIRCUIT	



1	ADDENDUM NO. 1	5/24/2024	JGL
REVISIONS:		DATE	APPD.

DATE:	MAY 2023
DESIGN:	KJ
DRAWN:	JAW
CHECKED:	JG
REVISION NUMBER:	
SCALE: AS SHOWN	

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

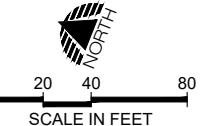
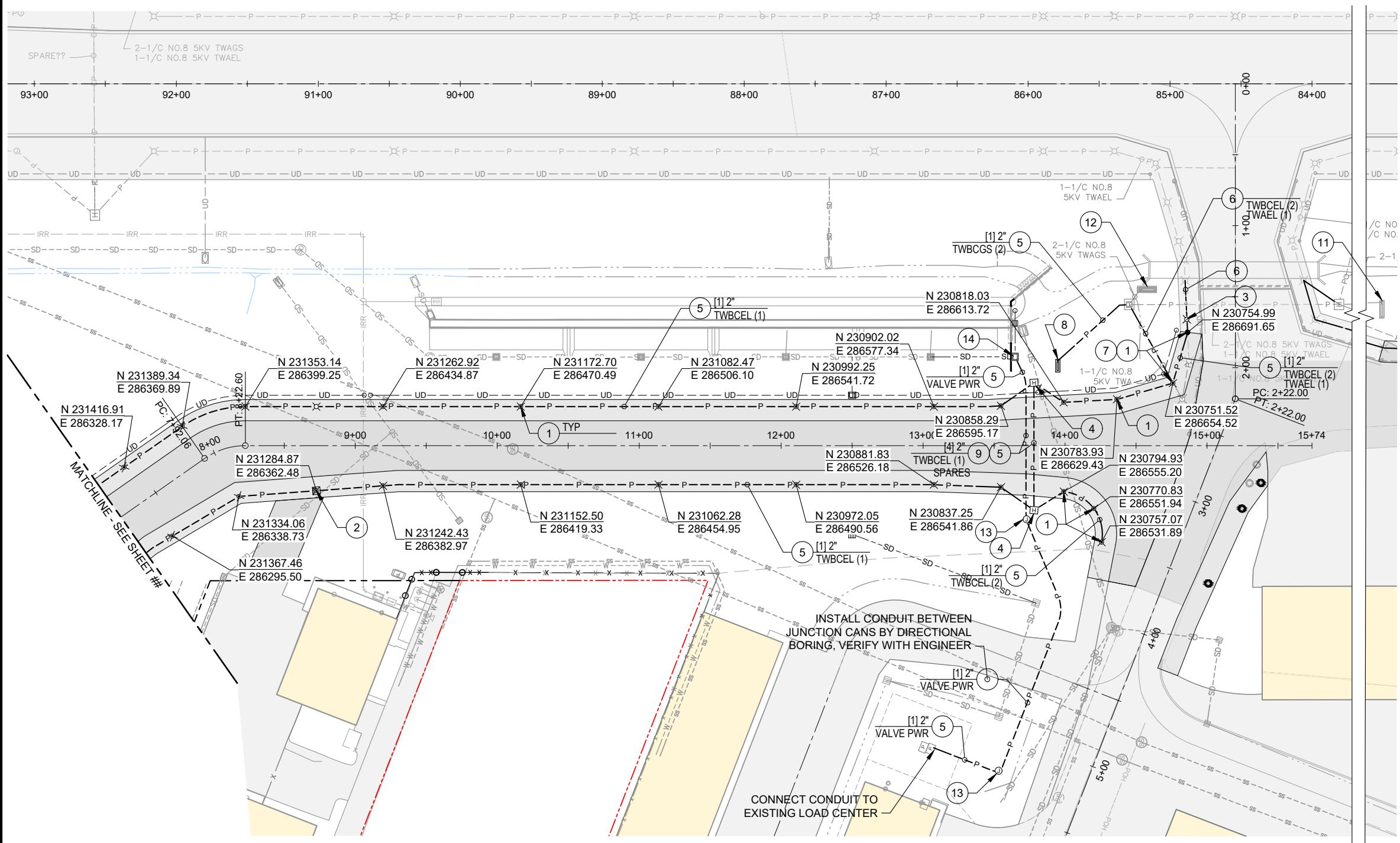
## ELECTRICAL PLAN

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV1-2020-W04**

SHEET NO.  
**55**

OF **71**



## ELECTRICAL CONSTRUCTION NOTES

- 1 INSTALL NEW LED ELEVATED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 1 61
- 2 INSTALL NEW LED FLUSH-MOUNTED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 3 61
- 3 REMOVE LIGHT AND TRANSFORMER AND INSTALL SOLID LID, SIMILAR TO DETAIL 1 64
- 4 INSTALL HANDHOLE, SEE DETAIL 1 60
- 5 INSTALL CONDUIT, SIZE AS NOTED, SEE DETAILS ON SHEET 59
- 6 REMOVE EXISTING CABLE AND INSTALL NEW CIRCUIT(S) IN EXISTING CONDUIT OR ELECTRICAL STRUCTURE, AS NOTED
- 7 EXPOSE AND CONNECT TO EXISTING CONDUIT
- 8 INSTALL GUIDANCE SIGN, SEE DETAIL 2 AND SHEET 58 FOR PANEL LEGEND
- 9 CONCRETE ENCASEMENT OF DUCTS, SEE DETAIL 2 60
- 10 INSTALL RETROREFLECTIVE TAXIWAY END SIGN, SEE DETAIL 1 63
- 11 EXTEND EXISTING GUIDANCE SIGN FOUNDATION AND INSTALL NEW GUIDANCE SIGN, SEE DETAIL 3 63
- 12 GUIDANCE SIGN PANEL REPLACEMENT, SEE SHEET 58
- 13 INSTALL CONCRETE DUCT MARKER, SEE DETAIL 2 65
- 14 INSTALL AUTOMATIC VALVE SYSTEM, SEE DETAIL 1 38

A - SIDE 'A'  
B - SIDE 'B'

## ELECTRICAL LEGEND

- C — NEW CIRCUITS TO BE PLACED IN EXISTING CONDUIT, REPLACE EXISTING CIRCUITS IN CONDUITS AS INDICATED
- P — NEW CONDUITS WITH NEW CIRCUITS, AND/OR NEW SPARE CONDUITS
- (1) 2" 5 INDICATES NUMBER OF NEW CONDUITS AND SIZE

## CIRCUIT LEGEND

ABBREVIATION	DESCRIPTIONS	CABLE COLOR
TWBCEL	TAXIWAY B AND TAXIWAY C EDGE LIGHTS	TERRA COTTA
TWBCGS	TAXIWAY B AND TAXIWAY C GUIDANCE SIGN	GREY
TWAEL	TAXIWAY A EDGE LIGHTS	BLACK
VALVE PWR	THREE XX CONDUCTORS	TBD
(1) 2" 5	INDICATES NUMBER OF NO.8 5KV CABLES PER CIRCUIT	

REPLACE ENTIRE SHEET



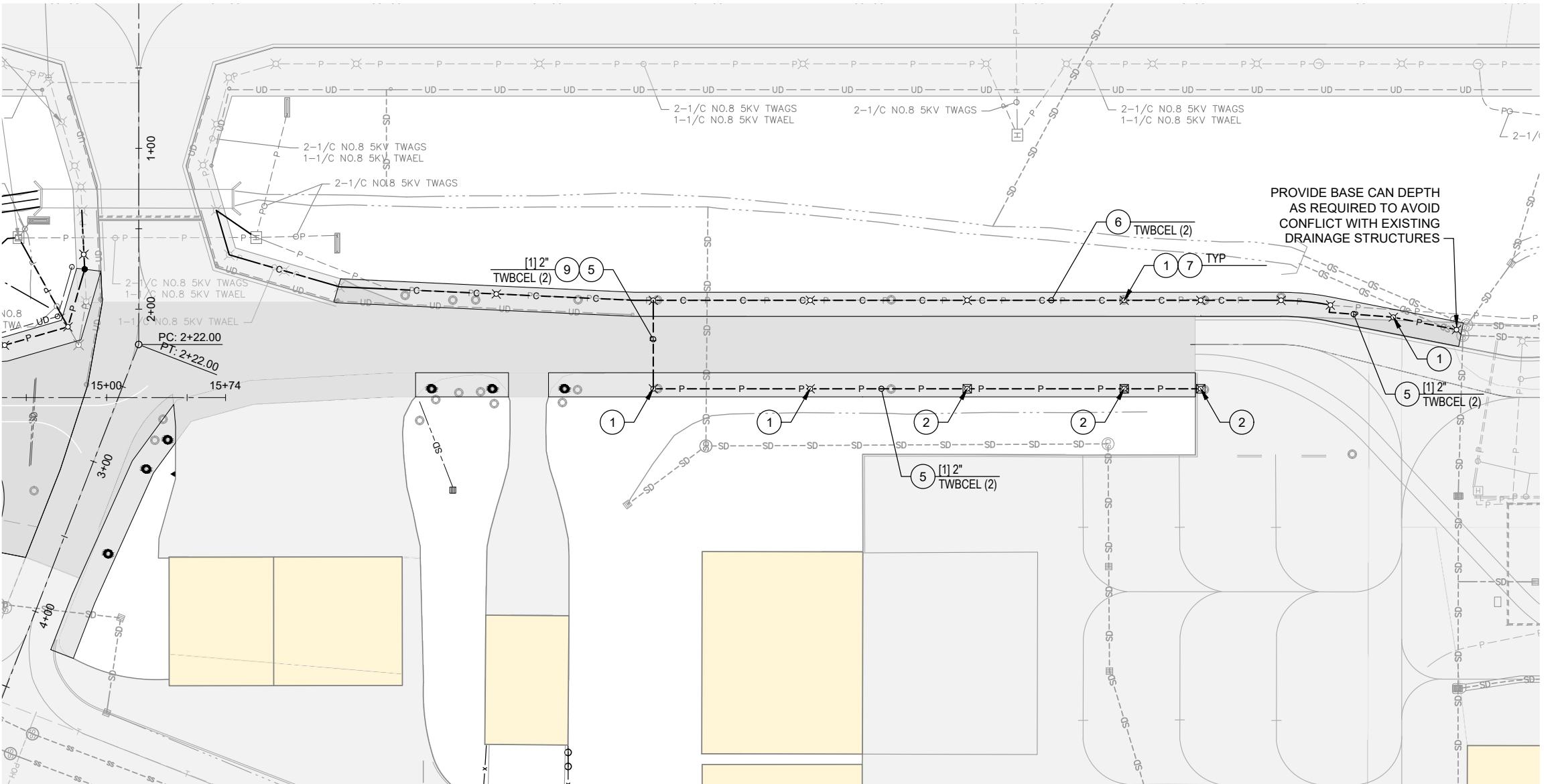
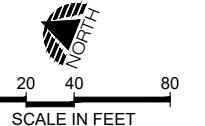
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REVISIONS:

5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**ELECTRICAL PLAN**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**56**  
OF **71**



## ELECTRICAL CONSTRUCTION NOTES

- 1 INSTALL NEW LED ELEVATED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 1 61
- 2 INSTALL NEW LED FLUSH-MOUNTED TAXIWAY EDGE LIGHT ON NEW BASE CAN, SEE DETAIL 3 61
- 3 REMOVE LIGHT AND TRANSFORMER AND INSTALL SOLID LID, SIMILAR TO DETAIL 1 64
- 4 INSTALL HANDBOULE, SEE DETAIL 1 60
- 5 INSTALL CONDUIT, SIZE AS NOTED, SEE DETAILS ON SHEET 59
- 6 REMOVE EXISTING CABLE AND INSTALL NEW CIRCUIT(S) IN EXISTING CONDUIT OR ELECTRICAL STRUCTURE, AS NOTED
- 7 EXPOSE AND CONNECT TO EXISTING CONDUIT
- 8 INSTALL GUIDANCE SIGN, SEE DETAIL 2 62 AND SHEET 58 FOR PANEL LEGEND
- 9 CONCRETE ENCASEMENT OF DUCTS, SEE DETAIL 2 60
- 10 INSTALL RETROREFLECTIVE TAXIWAY END SIGN, SEE DETAIL 1 63
- 11 EXTEND EXISTING GUIDANCE SIGN FOUNDATION AND INSTALL NEW GUIDANCE SIGN, SEE DETAIL 3 63
- 12 GUIDANCE SIGN PANEL REPLACEMENT, SEE SHEET 58
- 13 INSTALL CONCRETE DUCT MARKER, SEE DETAIL 2 65
- 14 INSTALL AUTOMATIC VALVE SYSTEM, SEE DETAIL 1 38

A ← SIDE 'A'  
B ← SIDE 'B'

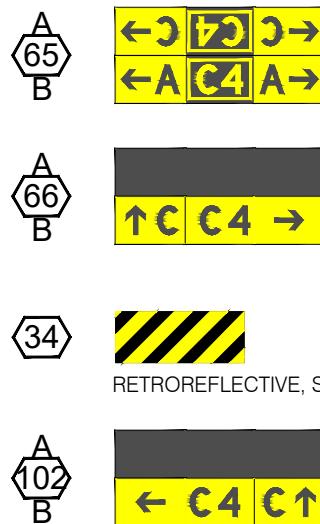
## ELECTRICAL LEGEND

- C — NEW CIRCUITS TO BE PLACED IN EXISTING CONDUIT, REPLACE EXISTING CIRCUITS IN CONDUITS AS INDICATED
- P — NEW CONDUITS WITH NEW CIRCUITS, AND/OR NEW SPARE CONDUITS
- (1) 2" 5 INDICATES NUMBER OF NEW CONDUITS AND SIZE

## CIRCUIT LEGEND

ABBREVIATION	DESCRIPTIONS	CABLE COLOR
TWBCEL	TAXIWAY B AND TAXIWAY C EDGE LIGHTS	TERRA COTTA
TWBCGS	TAXIWAY B AND TAXIWAY C GUIDANCE SIGN	GREY
TWAEL	TAXIWAY A EDGE LIGHTS	BLACK
VALVE PWR	THREE XX CONDUCTORS	TBD
(1)	INDICATES NUMBER OF NO.8 5KV CABLES PER CIRCUIT	

REPLACE ENTIRE SHEET



## SIGN TYPES

	L-858Y	Direction, Destination and Boundary signs - black legend on a yellow background.
	L-858R	Mandatory Instruction sign - 3/4" ±1/8" black outline on outside edge of white legend on red background.
	L-858L	Taxiway Location sign - yellow legend and border on a black background.
	L-858B	Runway Distance Remaining sign - white legend on a black background.
	L-858C	Taxiway Ending Marker sign.

**SIGN PANEL LEGEND NOTES:**

1. SIGN TO HAVE ALL PANELS REPLACED TO PROVIDE UNIFORM COLOR. EXISTING SIGN MANUFACTURERS OR SIZES VARY. CONTRACTOR SHALL VERIFY PRIOR TO ORDERING NEW PANELS
2. NEW SIGN AS SHOWN, SEE SPECIFICATIONS
3. CONTRACTOR SHALL VERIFY ALL PANEL SIZE AND LEGEND TEXT CONFIGURATION. ANY QUESTIONS OR DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR CLARIFICATION PRIOR TO ORDERING MATERIALS.
4. SIGN IS UNLIT, LOCATE PER PLAN, VERIFY LOCATION WITH ENGINEER.
5. NEW SIGN TO BE INSTALLED ON EXISTING FOUNDATION. SIGN SHOULD BE A MINIMUM LENGTH THAT ALLOWS ALL MESSAGE ELEMENTS TO BE SHOWING PER FAA SPECIFICATIONS.



1 ADDENDUM NO. 1	5/24/2024	JGL
REVISIONS:	DATE	APPD.

DATE: MAY 2024
DESIGN: KJK
DRAWN: JAW
CHECKED: JGL
REVISION NUMBER: 0

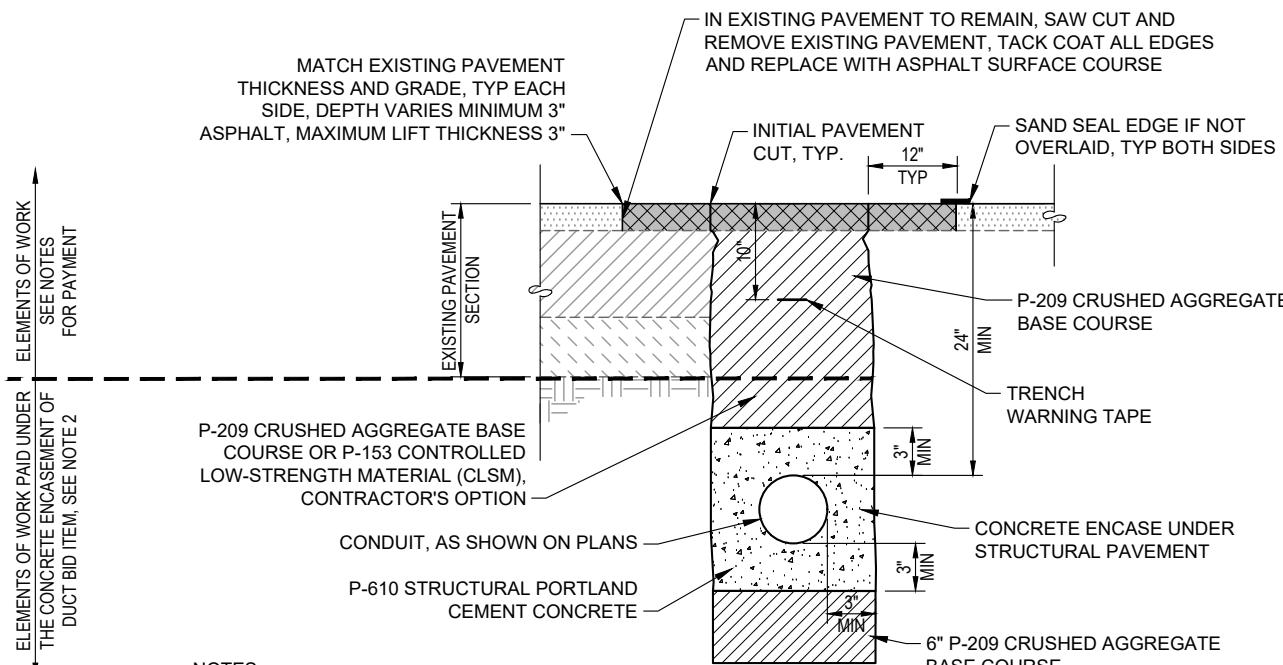
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

**GUIDANCE SIGN DETAILS**

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RVI-2020-W04**

SHEET NO.  
**58** OF **71**

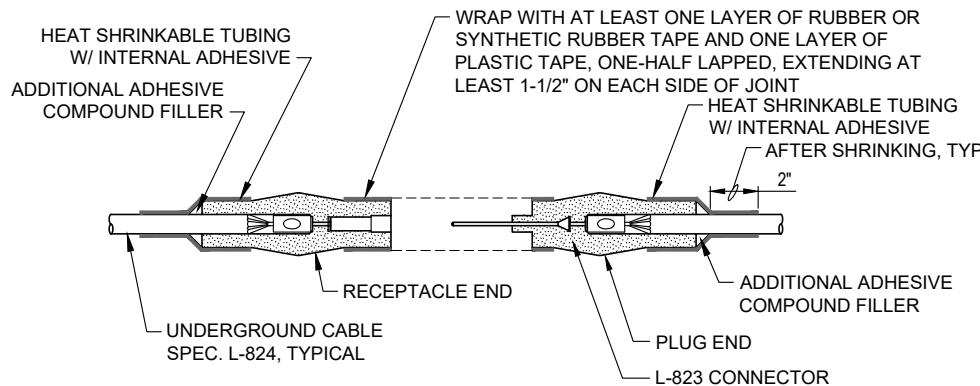


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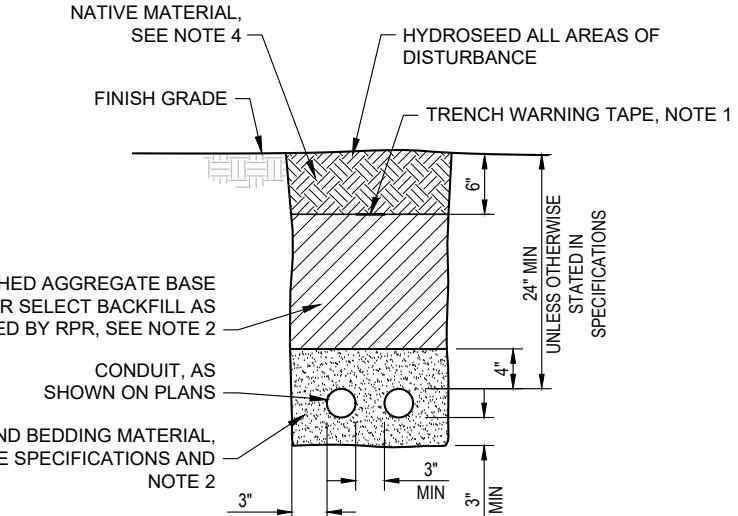
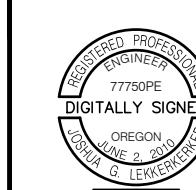
1. EXCAVATION, SAND BEDDING AND AGGREGATE BACKFILL CONSIDERED INCIDENTAL TO THE ELECTRICAL TRENCH, EXISTING PAVEMENT BID ITEM.
2. ALL CONDUIT AND CIRCUIT TO BE INSTALLED WITHIN THE LIMITS OF THE CONCRETE ENCASED DUCT SHALL BE PAID UNDER THE RESPECTIVE BID ITEM. ALL OTHER ELEMENTS OF WORK TO INSTALL CONCRETE ENCASED DUCT, INCLUDING TRENCHING AND BACKFILL, SHALL BE PAID UNDER THE CONCRETE ENCASEMENT OF DUCT BID ITEM. CONCRETE ENCASEMENT OF CONDUIT IS PAID FOR PER LINEAR FOOT OF TRENCH, REGARDLESS OF THE NUMBER OF CONDUITS INSTALLED, SEE SPECIFICATIONS.
3. INITIAL SAWCUT TO PROVIDE AREA OF PAVEMENT REMOVAL FOR TRENCHING. ADDITIONAL SAWCUT INTENDED TO COMPENSATE FOR PAVEMENT UNDERMINING. BACKFILL TO WITHIN 10" OF EXISTING SURFACE PRIOR TO MAKING ADDITIONAL SAWCUT, VERIFY WITH RPR.
4. PAVEMENT REMOVAL, ASPHALT TACK COAT AND ASPHALT SURFACE COURSE SHALL BE PAID UNDER THE RESPECTIVE BID ITEMS.
5. FOR HORIZONTAL EXTENT OF THE CONCRETE ENCASED DUCT, SEE DETAIL 3.60.

ELECTRICAL TRENCH,  
EXISTING PAVED AREA DETAIL  
NTS

3  
60  
1  
59



CABLE SPLICING - TYPE B  
NTS



P-209 CRUSHED AGGREGATE BASE COURSE OR SELECT BACKFILL AS APPROVED BY RPR, SEE NOTE 2

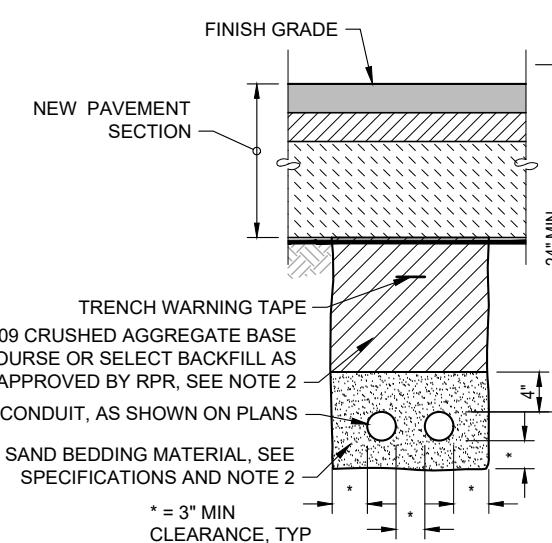
SAND BEDDING MATERIAL,  
SEE SPECIFICATIONS AND  
NOTE 2

NOTES:

1. WARNING TAPE NOT REQUIRED FOR DUCT(S) ENCASED IN CONCRETE.
2. BACKFILL CONSIDERED INCIDENTAL TO ELECTRICAL TRENCH NON-PAVED AREA BID ITEM.
3. 1-DUCT INSTALLATION SIMILAR. MORE THAN TWO DUCTS MAINTAIN HORIZONTAL AND VERTICAL SPACING.
4. STRIP AND STOCKPILE NATIVE TOP SOIL PRIOR TO TRENCHING, PLACE AND COMPACT TO A DENSE AND UNYIELDING CONDITION AS APPROVED BY THE RPR PRIOR TO HYDROSEEDING.

NON-PAVED AREA  
ELECTRICAL TRENCH DETAIL  
NTS

2  
59



P-209 CRUSHED AGGREGATE BASE COURSE OR SELECT BACKFILL AS APPROVED BY RPR, SEE NOTE 2

CONDUIT, AS SHOWN ON PLANS

SAND BEDDING MATERIAL, SEE  
SPECIFICATIONS AND NOTE 2

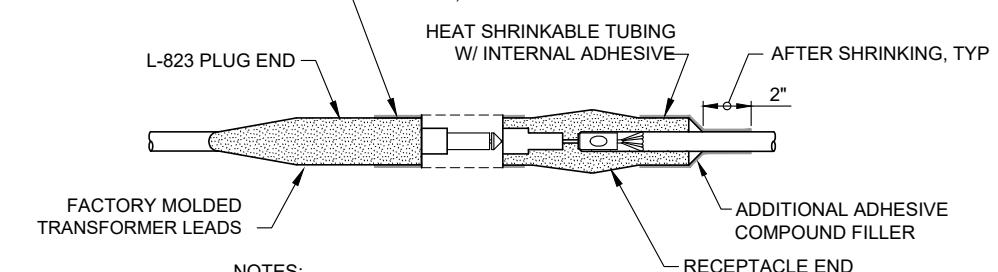
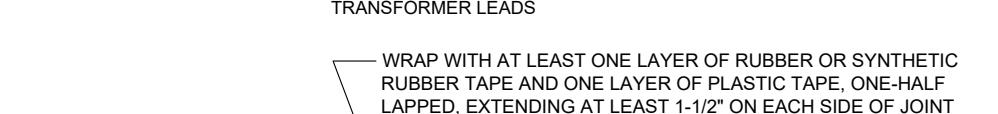
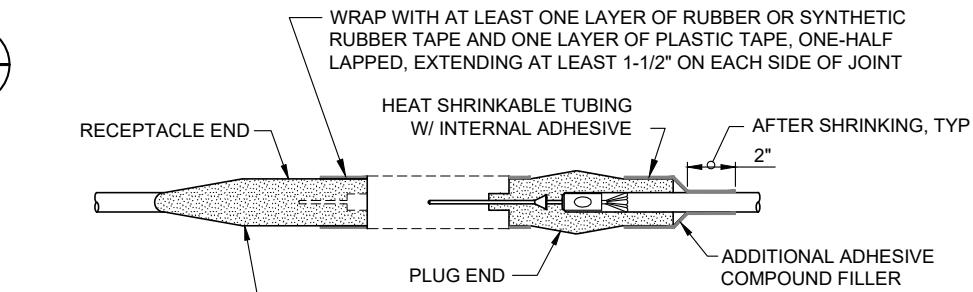
\* = 3" MIN  
CLEARANCE, TYP

NOTES:

1. WARNING TAPE NOT REQUIRED FOR DUCT(S) ENCASED IN CONCRETE.
2. BACKFILL CONSIDERED INCIDENTAL TO ELECTRICAL TRENCH, NEW PAVEMENT AREA BID ITEM.

PAVED AREA TRENCH DETAIL  
NTS

REPLACE ENTIRE SHEET



CABLE SPLICING - TYPE C  
NTS

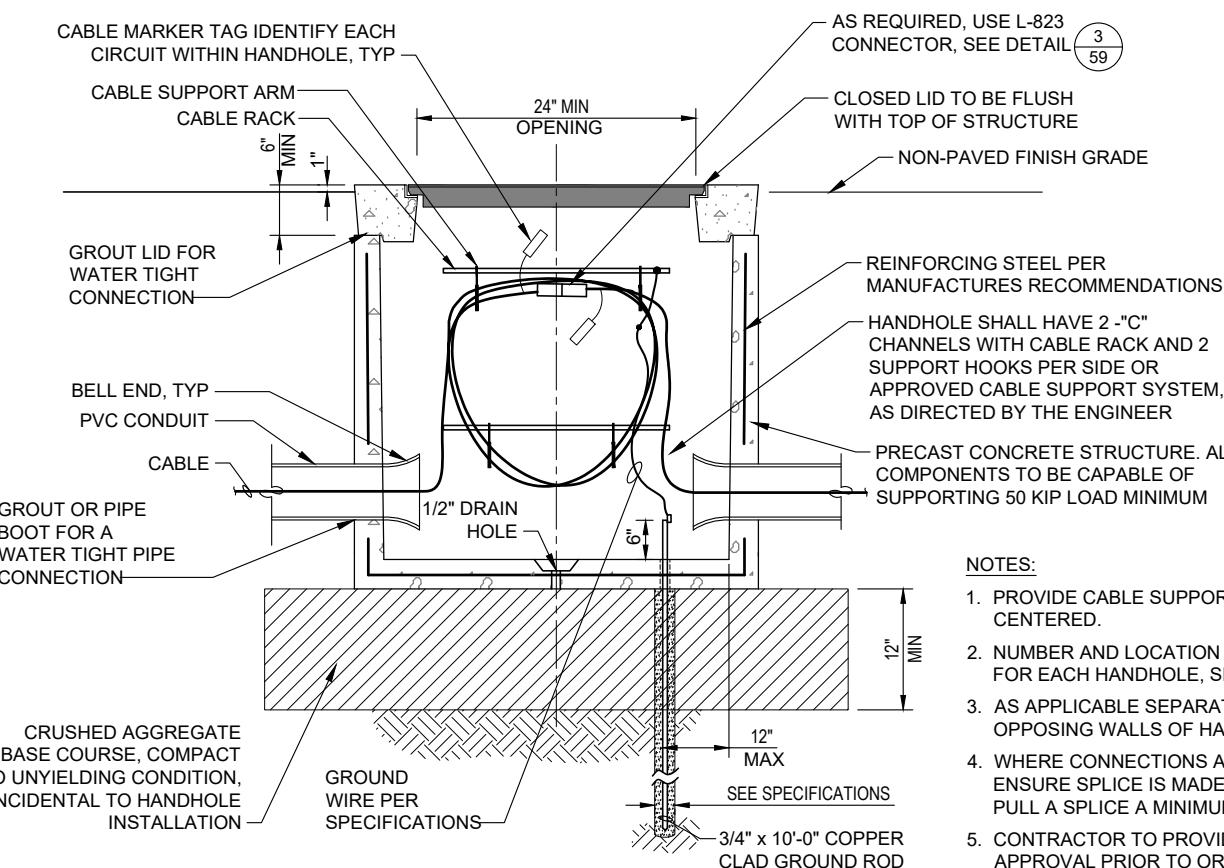
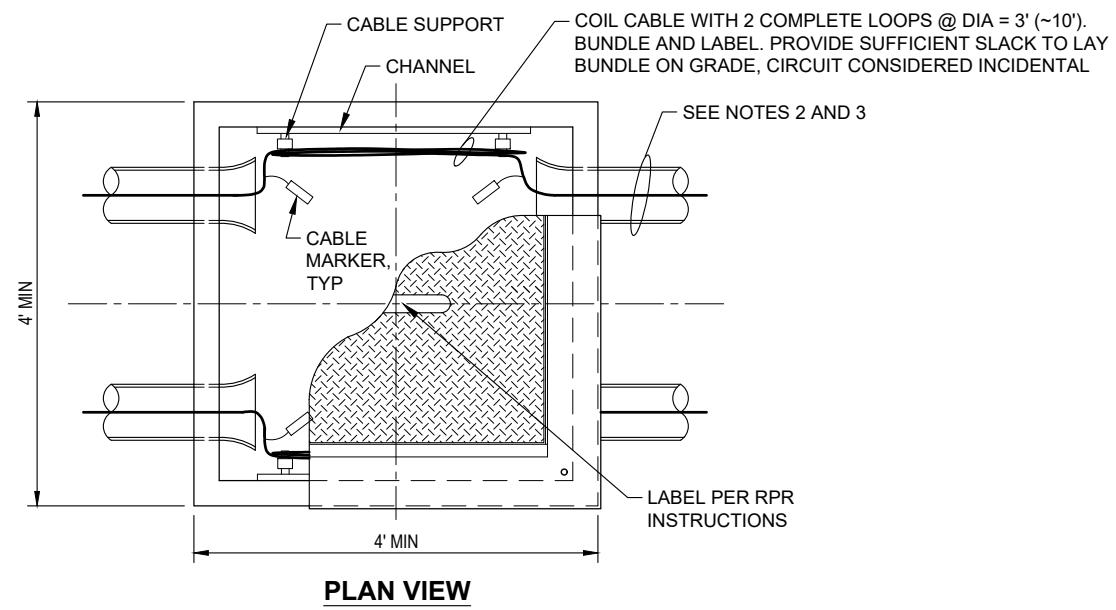
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59

DATE:	MAY 2024
DESIGN:	KJK
DRAWN:	JAW
CHECKED:	JGL
REVISION NUMBER:	0
SCALE:	AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**ELECTRICAL DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV1-2020-W04**  
SHEET NO.  
**59**  
OF **71**

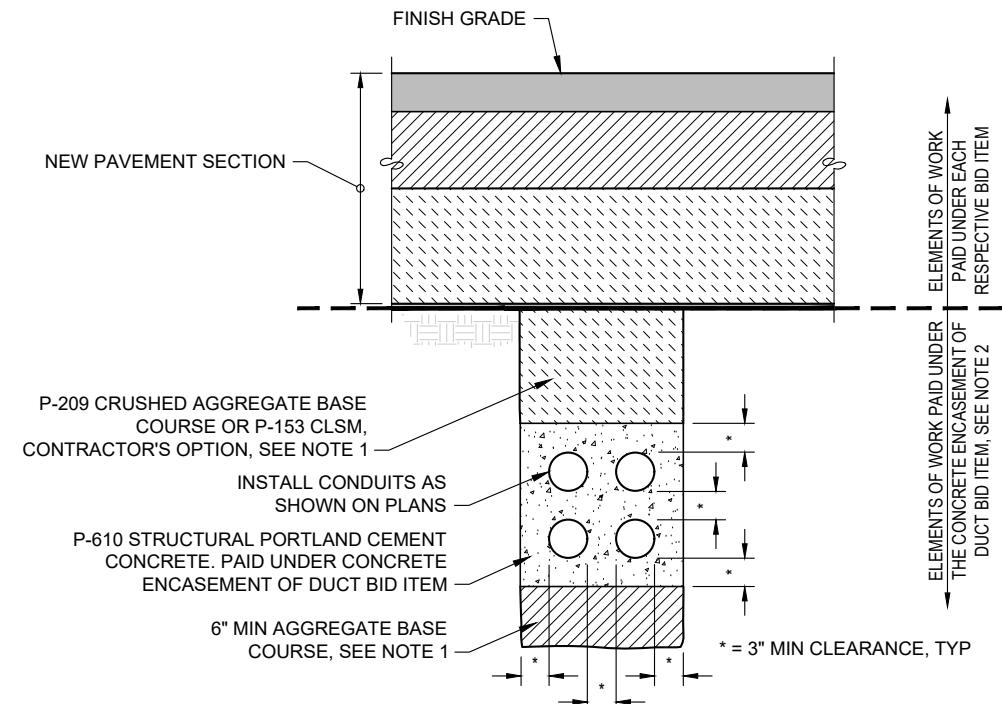
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REVISIONS: 5/24/2024 JGL  
DATE APPD.



**HANDHOLE INSTALLATION DETAIL** 1 60  
NTS

**NOTES:**

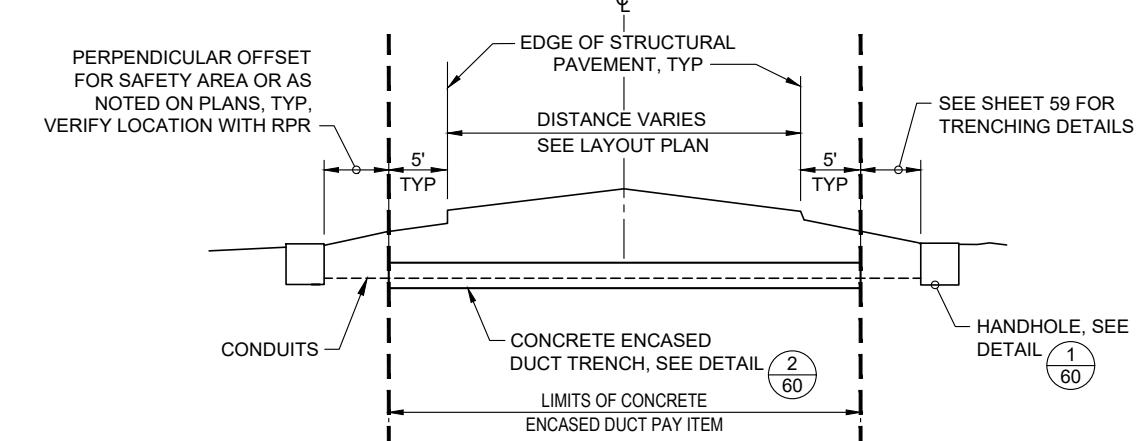
1. PROVIDE CABLE SUPPORT LEVEL, MINIMUM, WITH SPLICE CENTERED.
2. NUMBER AND LOCATION OF CONDUITS AND CABLE WILL VARY FOR EACH HANDHOLE, SEE PLANS.
3. AS APPLICABLE SEPARATE HIGH AND LOW VOLTAGE CABLES TO OPPOSING WALLS OF HANDHOLE OR AS DIRECTED BY THE RPR.
4. WHERE CONNECTIONS ARE MADE TO EXISTING CIRCUITS ENSURE SPLICE IS MADE IN HANDHOLE WITH ENOUGH SLACK TO PULL A SPLICE A MINIMUM 18" ABOVE HANDHOLE.
5. CONTRACTOR TO PROVIDE SHOP DRAWINGS FOR RPR APPROVAL PRIOR TO ORDERING STRUCTURES.
6. LID TO HINGE OPEN AWAY FROM TAXIWAY.
7. VERIFY NEW HANDHOLE LOCATION WITH RPR.



**NOTES:**

1. CONCRETE ENCASED DUCTS SHALL BE IN PLACE PRIOR TO CONSTRUCTING NEW PAVEMENT SECTION.
2. ALL CONDUIT AND CIRCUIT TO BE INSTALLED WITHIN THE LIMITS OF THE CONCRETE ENCASED DUCT SHALL BE PAID UNDER THE RESPECTIVE BID ITEM. ALL OTHER ELEMENTS OF WORK TO INSTALL CONCRETE ENCASED DUCT, INCLUDING TRENCHING AND BACKFILL, SHALL BE PAID UNDER THE CONCRETE ENCASEMENT OF DUCT BID ITEM. CONCRETE ENCASEMENT OF CONDUIT IS PAID FOR PER LINEAR FOOT OF TRENCH, REGARDLESS OF THE NUMBER OF CONDUITS INSTALLED, SEE SPECIFICATIONS.
3. FOR HORIZONTAL EXTENT OF THE CONCRETE ENCASED DUCT, SEE DETAIL 3 60

**CONCRETE ENCASED OF DUCT TRENCH DETAIL** 2 60  
NTS



**PAVEMENT CONDUIT CROSSING DETAIL** 3 60  
NTS

REPLACE ENTIRE SHEET

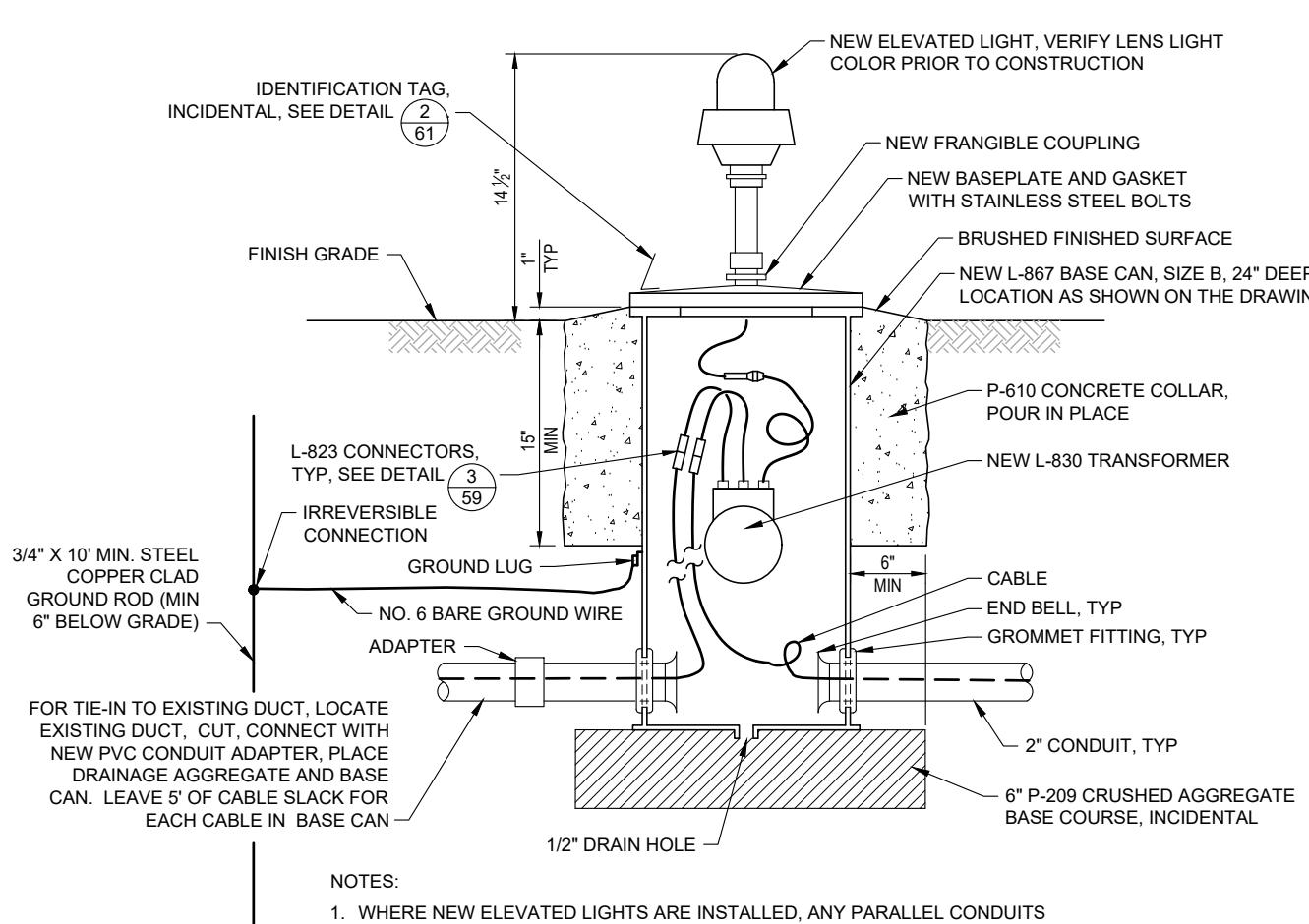


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DATE APPD.

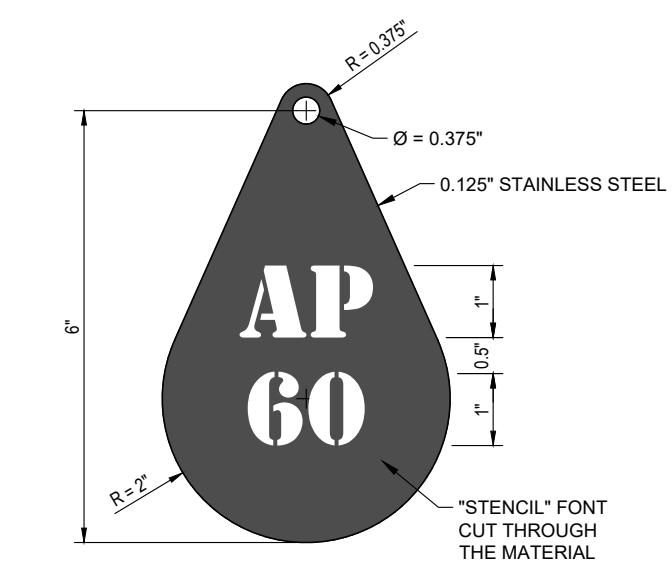
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DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**ELECTRICAL DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

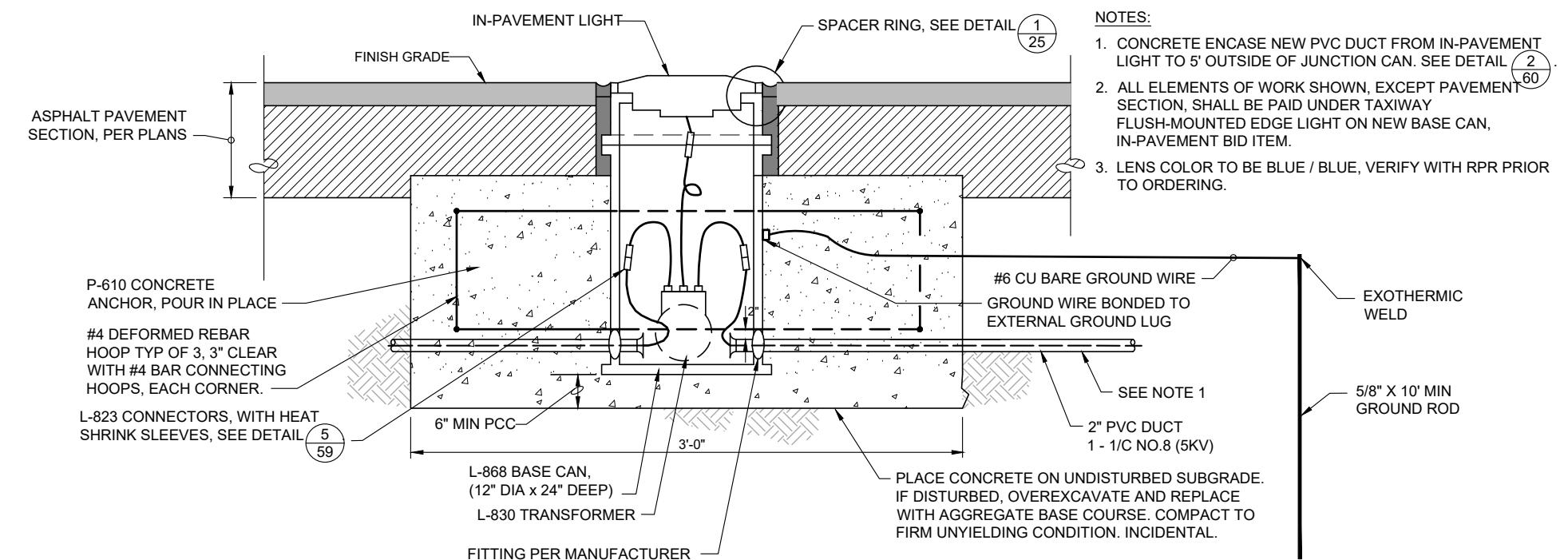
PROJECT NUMBER: RVI-2020-W04  
SHEET NO. 60 OF 71



ELEVATED LIGHT INSTALLATION DETAIL 1-61



IDENTIFICATION TAG DETAIL 2-61



TAXIWAY IN-PAVEMENT LIGHT DETAIL 3-61

REPLACE ENTIRE SHEET



ADDENDUM NO. 1  
REVISIONS: 5/24/2024 JGL  
DATE APPD.

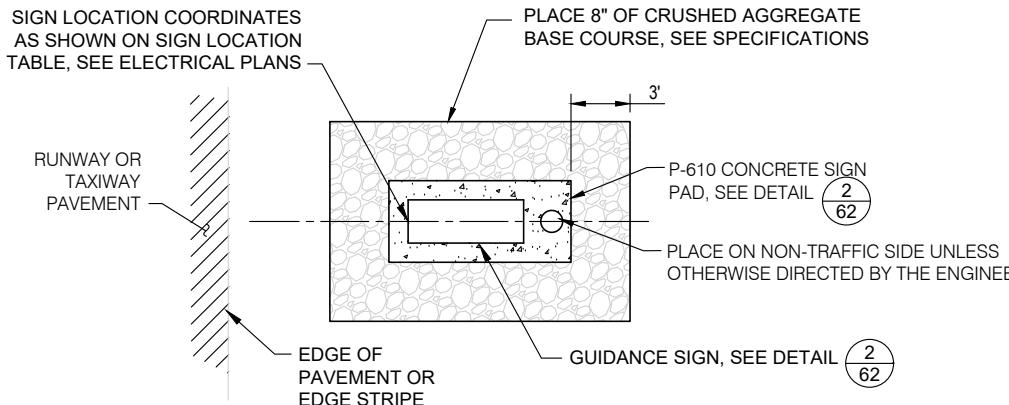
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SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**ELECTRICAL DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

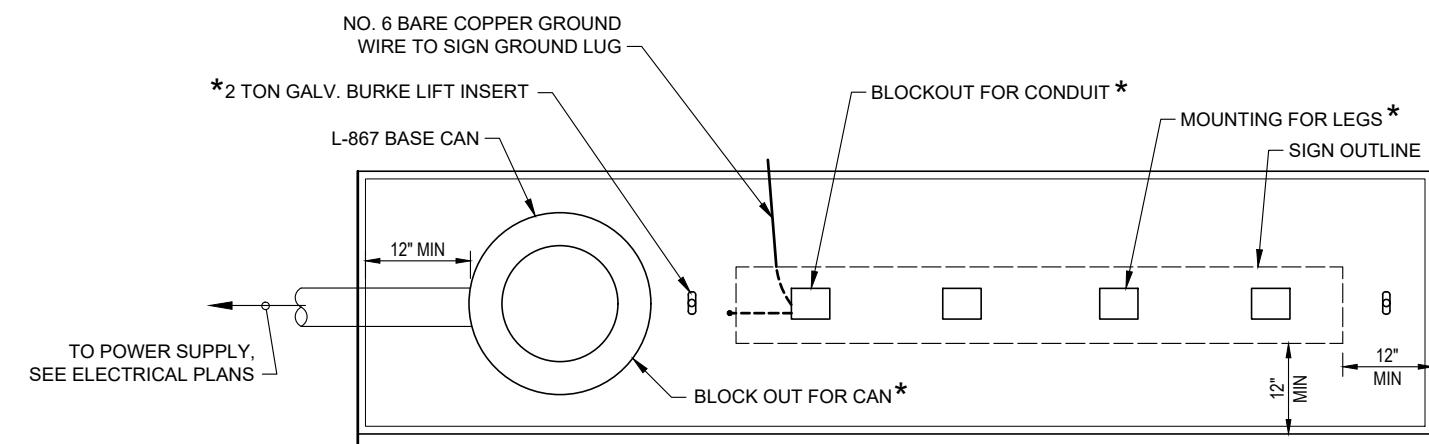
PROJECT NUMBER: RVI-2020-W04  
SHEET NO. 61 OF 71

L-858 GUIDANCE SIGN LAYOUT NOTES:

1. ORIENTATION OF SIGN AND SIGN PAD TO BE VERIFIED WITH RPR IN THE FIELD AFTER LOCATION IS IDENTIFIED AND PRIOR TO CONSTRUCTION.
2. ALL WORK AND MATERIALS, TO INCLUDE 2-INCH CONDUIT AND CABLE, WITHIN THE LIMITS SHOWN IN THE SIGN INSTALLATION PLAN. SIGN, BASE CAN, UNCLASSIFIED EXCAVATION, PCC, NEW GRADING, FABRIC AND CRUSHED AGGREGATE BASE COURSE FOR SIGN INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE TAXIWAY GUIDANCE SIGN BID ITEM. NO SEPARATE PAYMENT SHALL BE MADE.

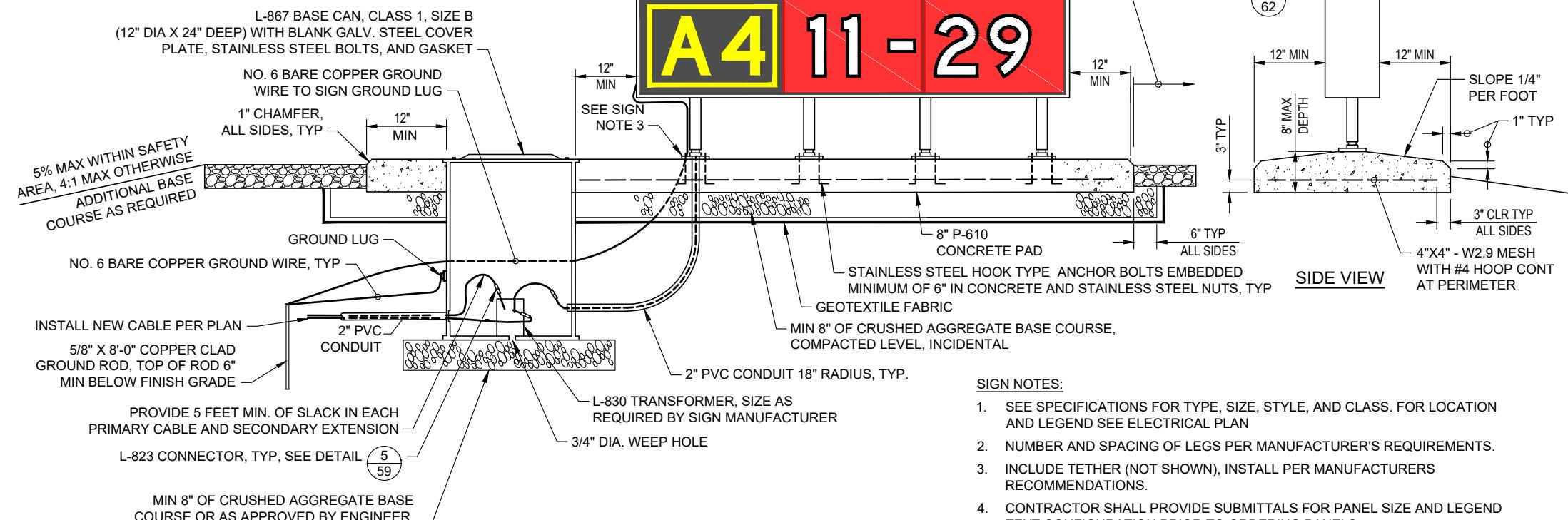


L-858 GUIDANCE SIGN LAYOUT PLAN  
NTS 1 62



PLAN VIEW  
NOTE:  
\* IF PRECAST PAD IS USED

ALL CONDUIT AND CABLE  
PAID UNDER  
RESPECTIVE BID ITEM



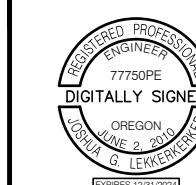
SIGN NOTES:

1. SEE SPECIFICATIONS FOR TYPE, SIZE, STYLE, AND CLASS. FOR LOCATION AND LEGEND SEE ELECTRICAL PLAN
2. NUMBER AND SPACING OF LEGS PER MANUFACTURER'S REQUIREMENTS.
3. INCLUDE TETHER (NOT SHOWN), INSTALL PER MANUFACTURER'S RECOMMENDATIONS.
4. CONTRACTOR SHALL PROVIDE SUBMITTALS FOR PANEL SIZE AND LEGEND TEXT CONFIGURATION PRIOR TO ORDERING PANELS.

REPLACE ENTIRE SHEET

L-858 GUIDANCE SIGN  
INSTALLATION DETAIL  
NTS

2 62



ADDENDUM NO. 1  
REVISIONS:

5/24/2024 JGL  
DATE APPD.

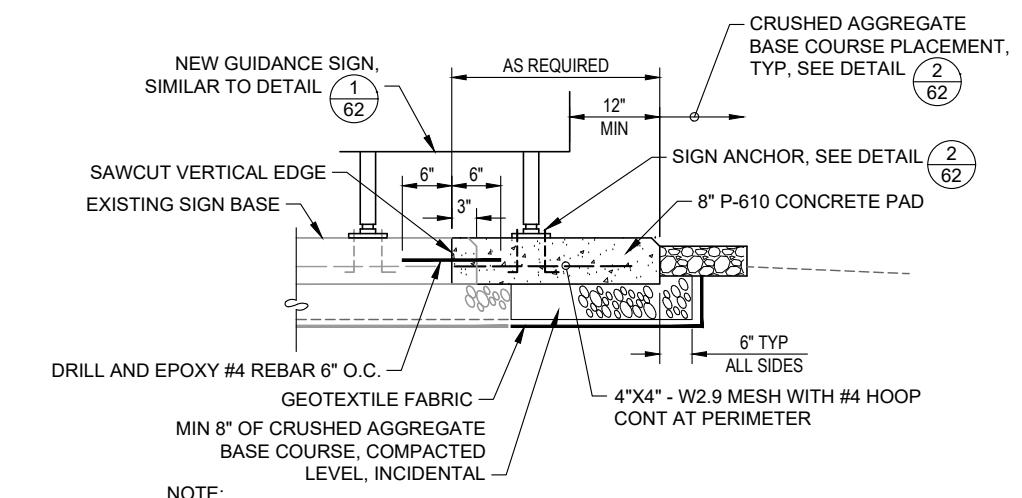
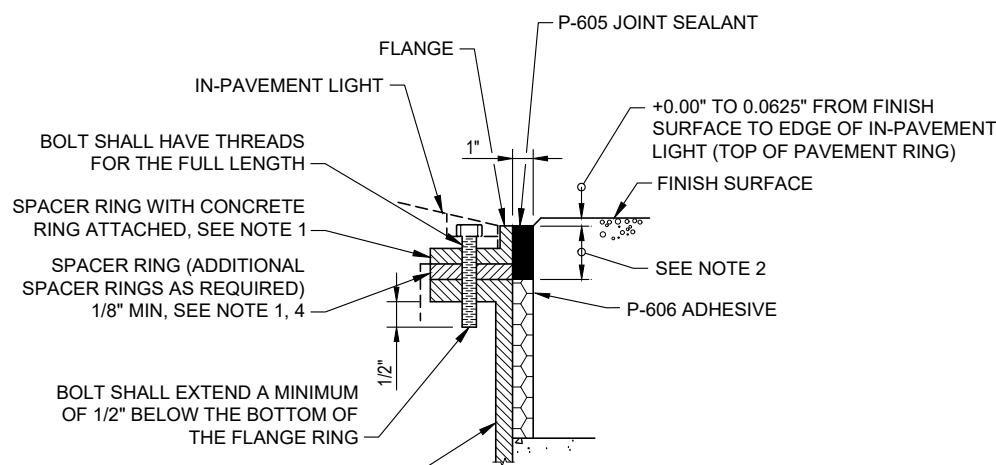
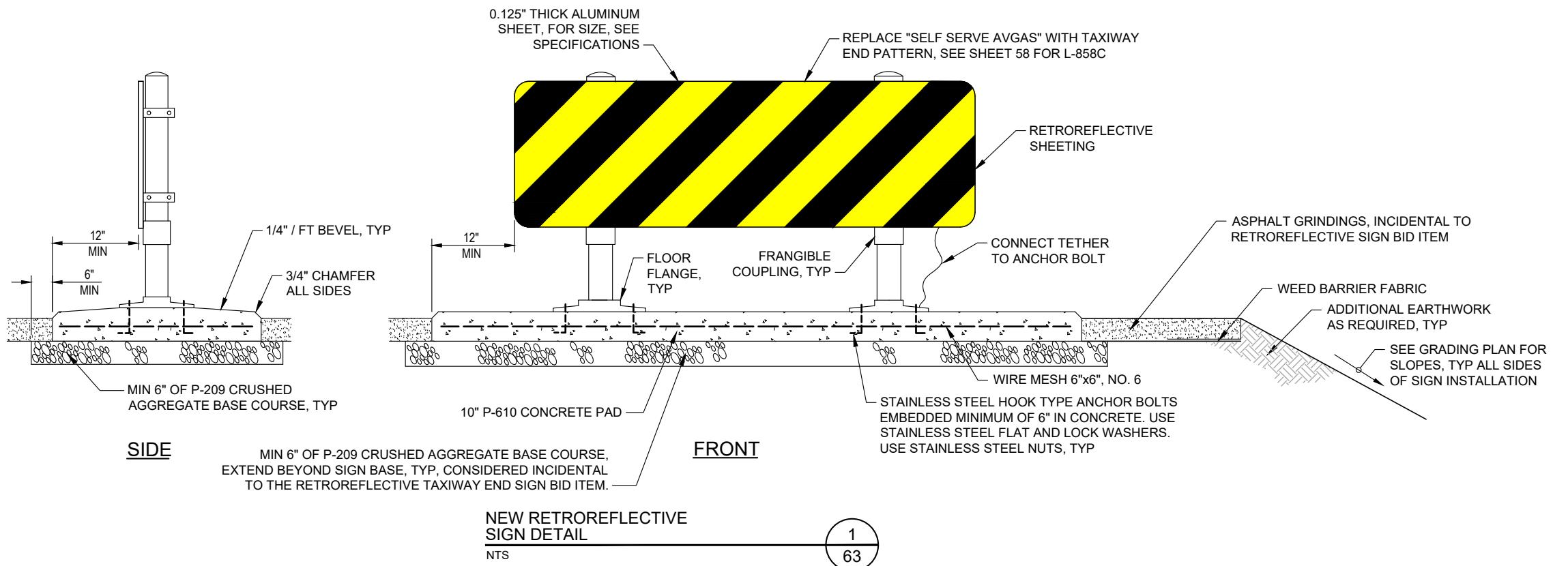
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REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

ELECTRICAL DETAILS

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
RVI-2020-W04  
SHEET NO.  
62  
OF 71



REPLACE ENTIRE SHEET

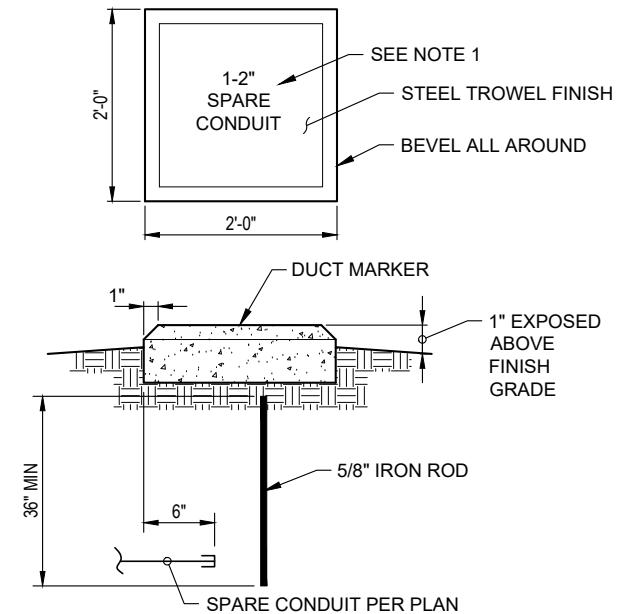
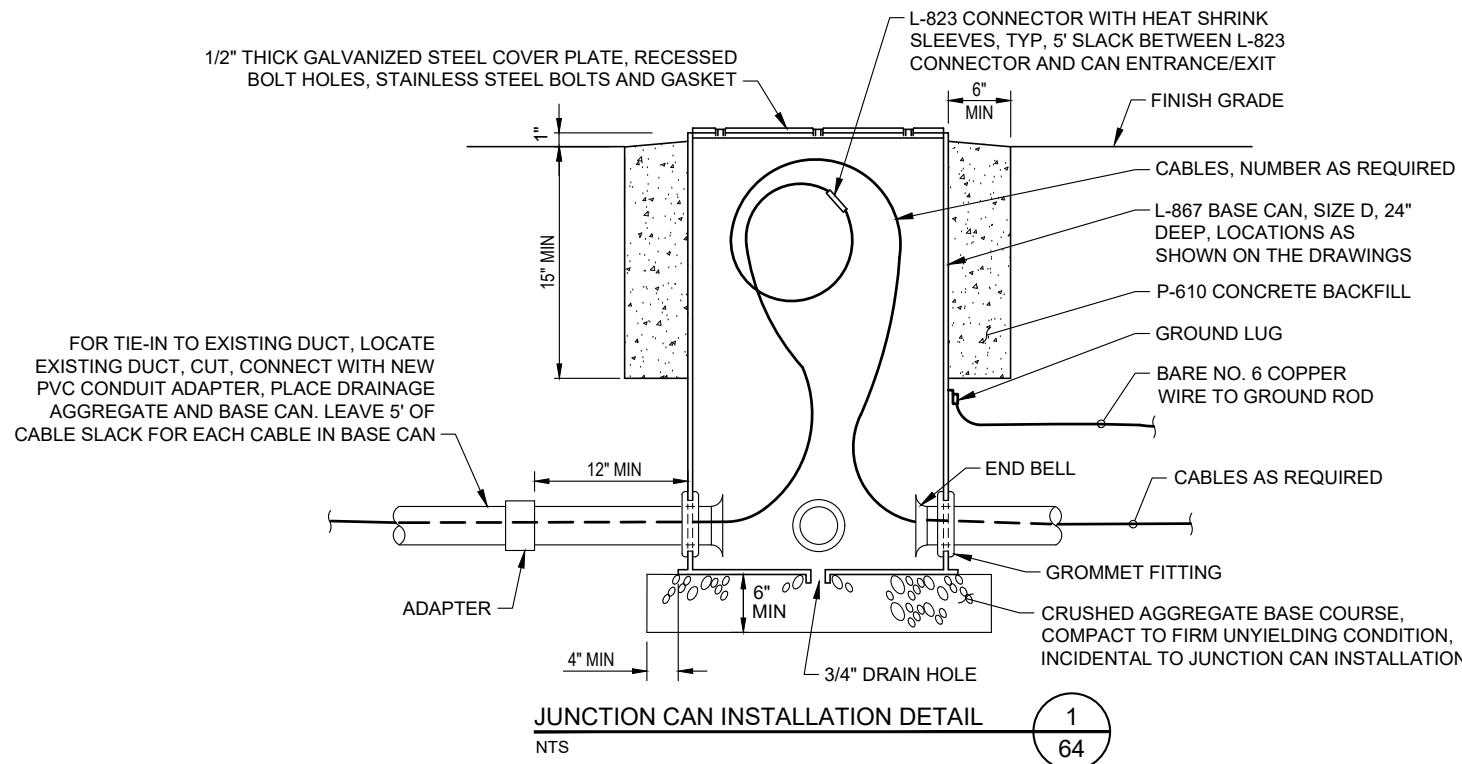


ADDENDUM NO. 1  
REVISIONS: 5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**ELECTRICAL DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

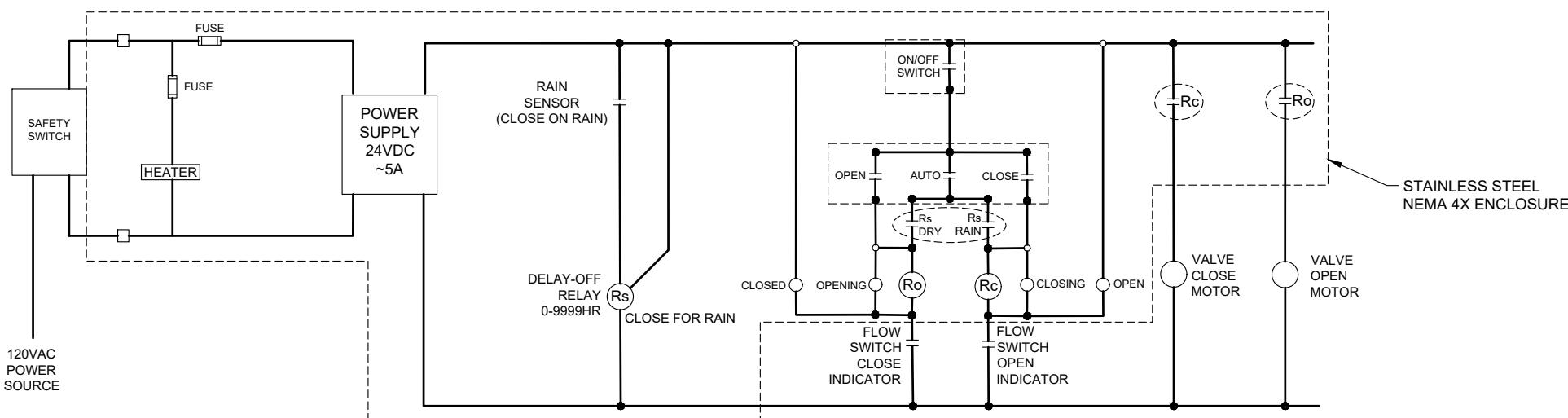
PROJECT NUMBER:  
**RV-2020-W04**  
SHEET NO.  
**63**  
OF **71**



**NOTES:**

1. VERIFY TEXT AND SYMBOL WITH ENGINEER
2. DUCT AND CABLE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE 2-INCH PVC CONDUIT BID ITEM.

SPARE CONDUIT MARKER DETAIL  
NTS



**GENERAL NOTES:**

1. POWER SUPPLY PROVIDES 24VDC SOURCE FOR RAIN SENSOR, RELAYS, AND VALVE MOTOR.
2. RAIN SENSOR CLOSES UPON SENSING RAIN.
3. DELAY-OFF RELAY CLOSES WHEN RAIN SENSOR DETECTS RAIN AND IS DELAYED UP TO 9999 HOURS WHEN RAIN STOPS.
4. OPEN-AUTO-CLOSE SWITCH FOR MANUAL VALVE OPERATION.
5. ON/OFF SWITCH FOR CONTROL DEFEAT.
6. WHEN RAIN IS DETECTED, Rs CLOSES ENERGIZING Rc RELAY, Rc RELAY ACTIVATES CLOSE MOTOR, VALVE CLOSES.
7. WHEN RAIN STOPS, AFTER DELAY, Rs ACTIVATES Ro RELAY ACTIVATES OPEN MOTOR, VALVE OPENS.
8. VALVE USES 24VDC CONTROL AND HAS STATUS CONTACTS USED FOR CONTROL.
9. LIGHTS ARE PROVIDED TO INDICATE VALVE STATE AND CLOSE OR OPEN IN PROGRESS.

DRAINAGE VALVE CONTROLLER  
NTS

REPLACE ENTIRE SHEET

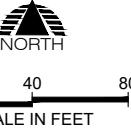
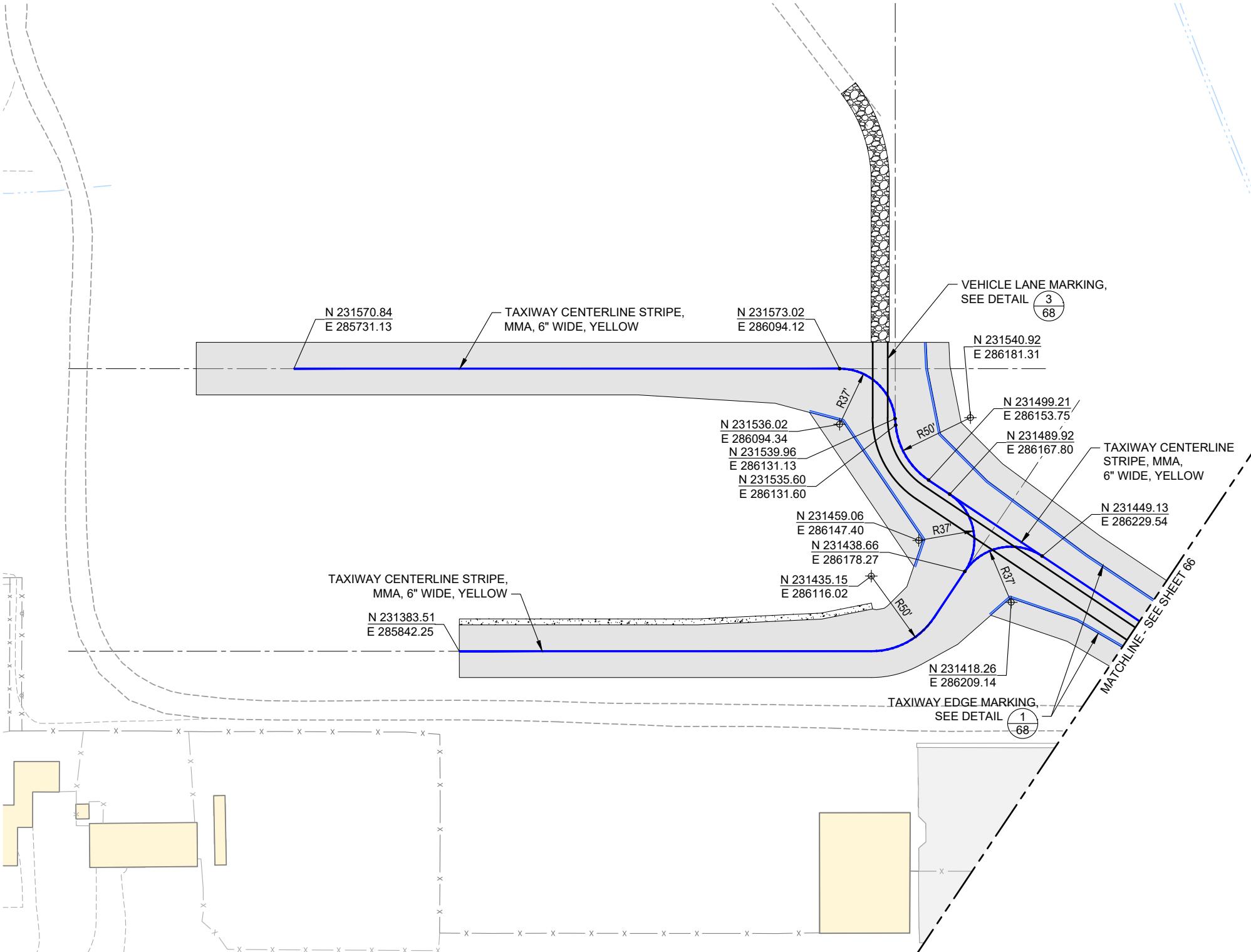


1 ADDENDUM NO. 1	5/24/2024	JGL
REVISIONS:	DATE	APPD.

DATE: MAY 2024  
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CHECKED: JGL  
REVISION NUMBER: 0

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**ELECTRICAL DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

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**RVI-2020-W04**  
SHEET NO.  
**64**  
OF **71**



SCALE IN FEET

## LEGEND



1	ADDENDUM NO. 1	5/24/2024	JP
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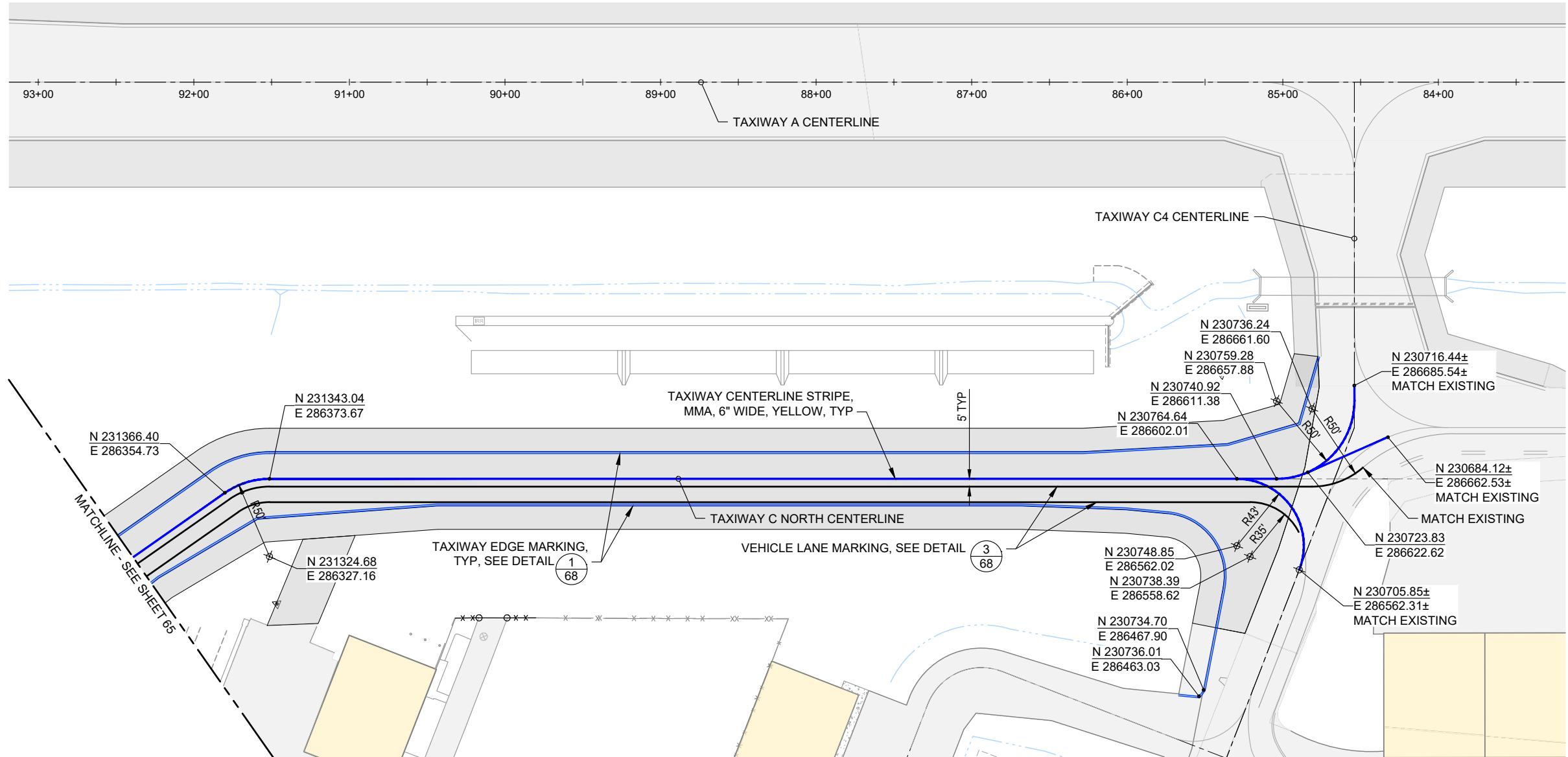
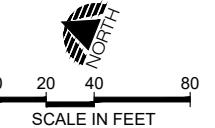
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

# PAVEMENT MARKING AND RETROREFLECTIVE MARKER PLAN

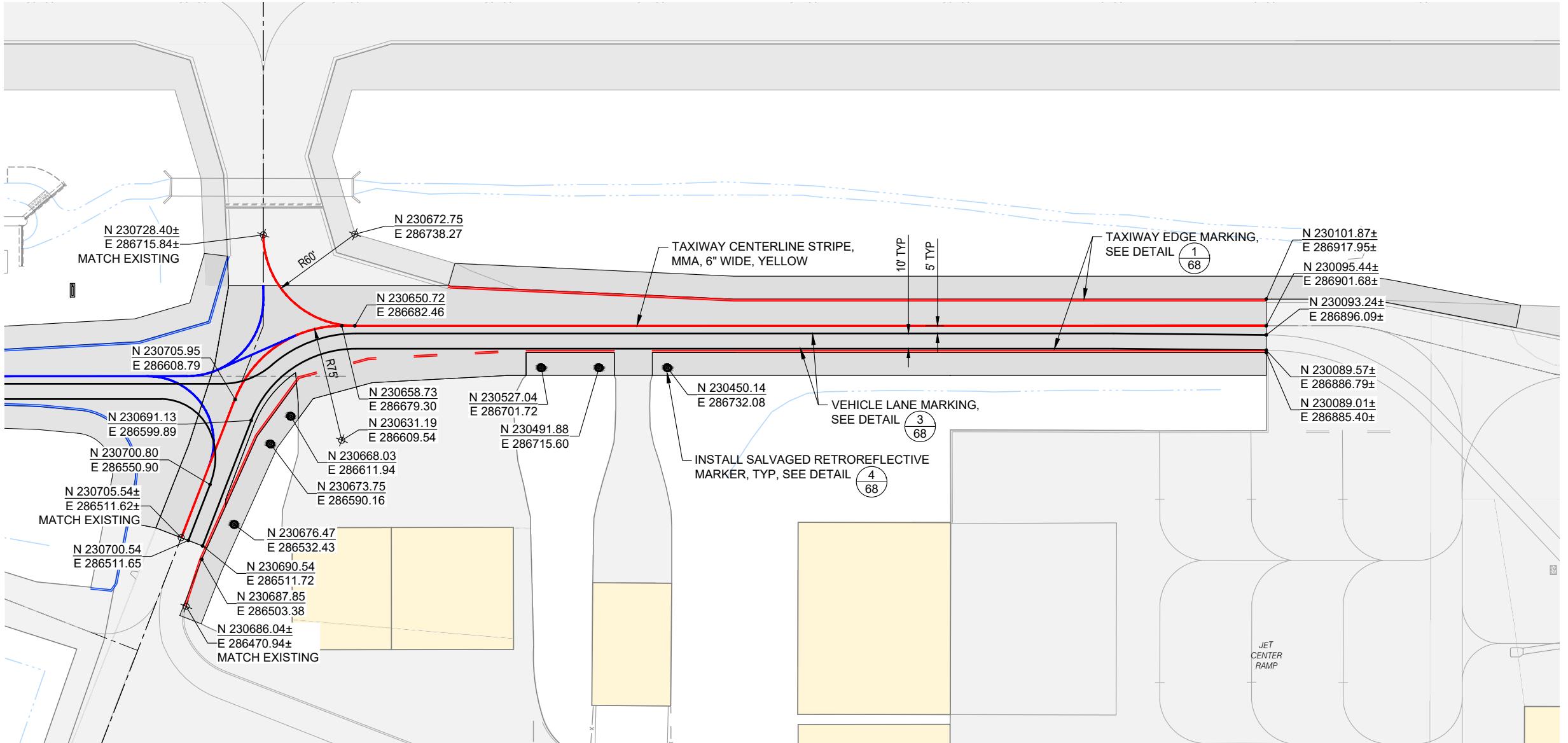
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV1-2020-W04**

SHEET NO.  
**65**  
OF **71**



REPLACE ENTIRE SHEET



REPLACE ENTIRE SHEET



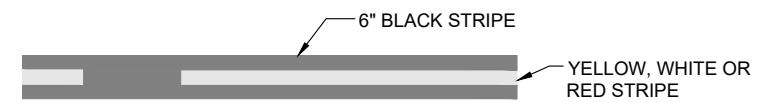
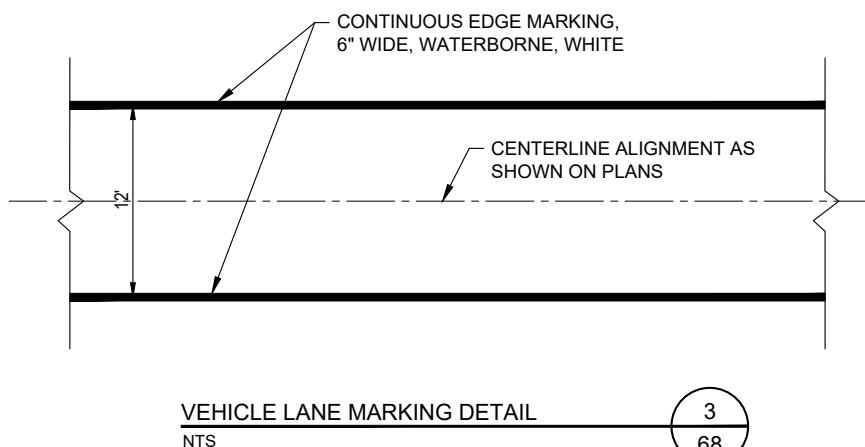
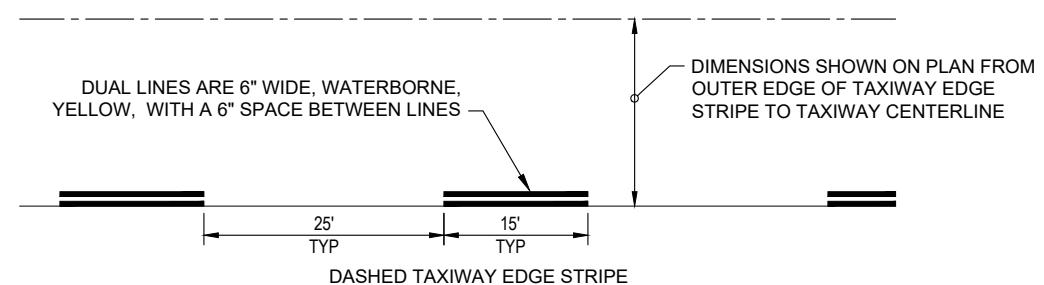
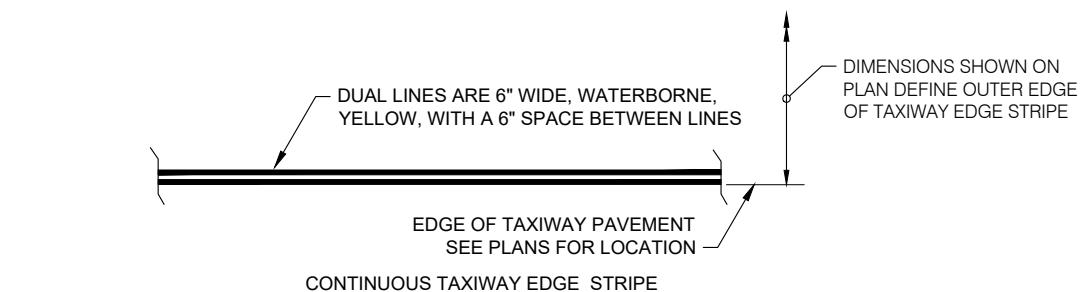
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REVISIONS:	DATE	APPD.

DATE: MAY 2024	DESIGN: KJK
DRAWN: JAW	CHECKED: JGL
REVISION NUMBER: 0	
SCALE: AS SHOWN	

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**PAVEMENT MARKING AND  
RETROREFLECTIVE MARKER PLAN  
- ADDITIVE**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
**RV-2020-W04**

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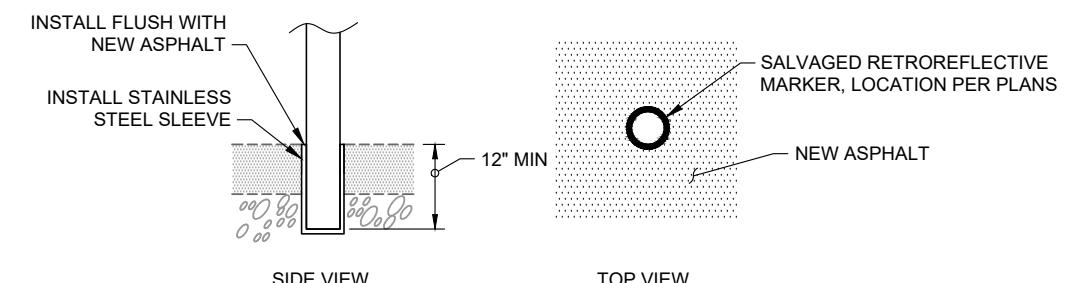
## NOTE:

1. ALL MARKING ON CONCRETE PAVEMENT SHALL HAVE A BLACK BORDER.
2. BLACK BORDER MARKING SHALL MATCH MATERIAL TYPE OF STRIPE AS SHOWN ON PLANS.
3. ALL BLACK MARKINGS SHALL BE WATERBORNE.

## MARKINGS OUTLINED IN BLACK DETAIL

NTS

2 68



## NOTE:

1. CONTRACTOR SHALL CORE PAVEMENT TO INSTALL SLEEVE TO ALLOW FOR RETROREFLECTIVE MARKER REMOVAL.
2. MARKER SHOULD FIT SNUGGLY WITHIN THE SLEEVE AND SHALL BE RIGID WHEN EXPOSED TO HIGH SPEED WINDS.
3. IF SALVAGED RETROREFLECTIVE MARKERS ARE UNABLE TO MEET MINIMUM REQUIREMENTS, BRING TO THE ATTENTION OF THE RPR.
4. ALL WORK TO REINSTALL THE RETROREFLECTIVE MARKERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE SALVAGED RETROREFLECTIVE MARKER PLACEMENT BID ITEM.

## RETROREFLECTIVE MARKER ENLARGED PLAN VIEW DETAIL

NTS

4 68

REPLACE ENTIRE SHEET



ADDENDUM NO. 1	5/24/2024	JGL
REVISIONS:	DATE	APPD.

DATE:	MAY 2024
DESIGN:	KJK
DRAWN:	JAW
CHECKED:	JGL
REVISION NUMBER:	0

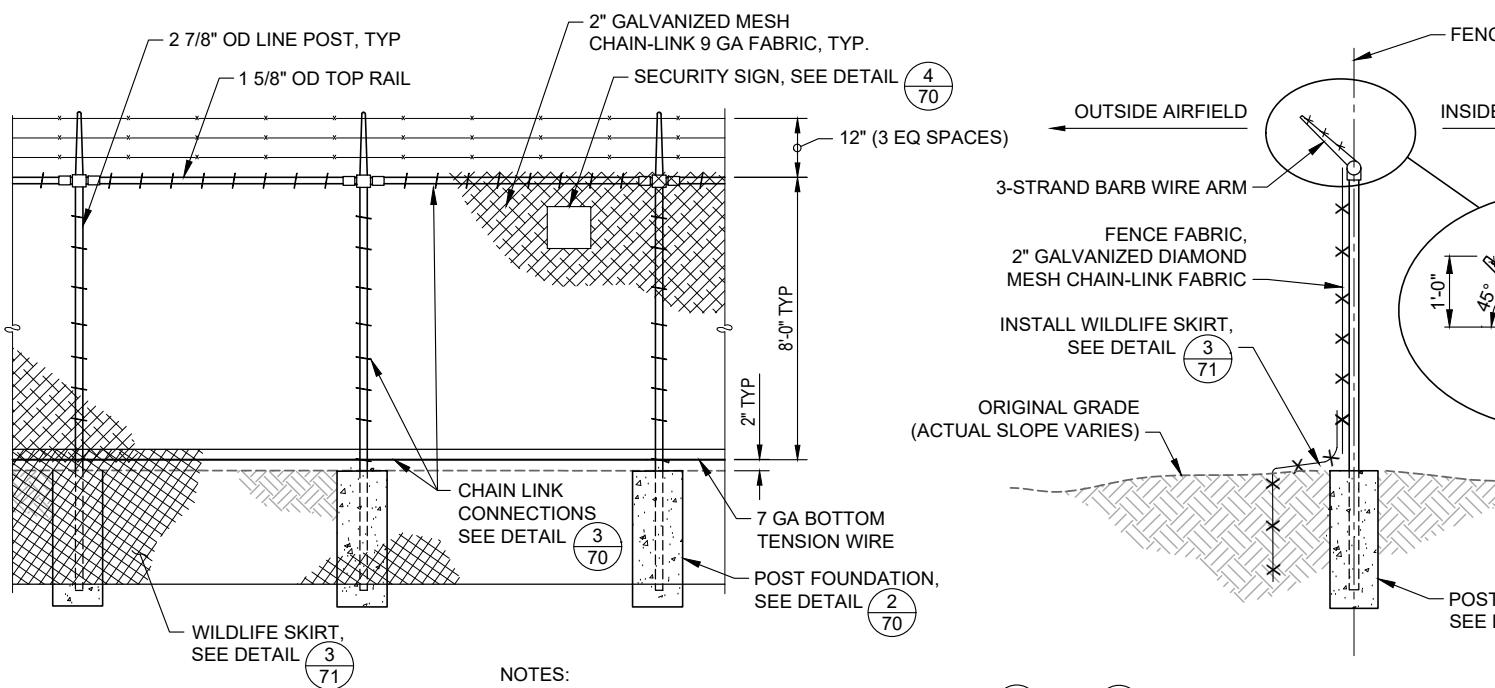
ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

**MARKING DETAILS**

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

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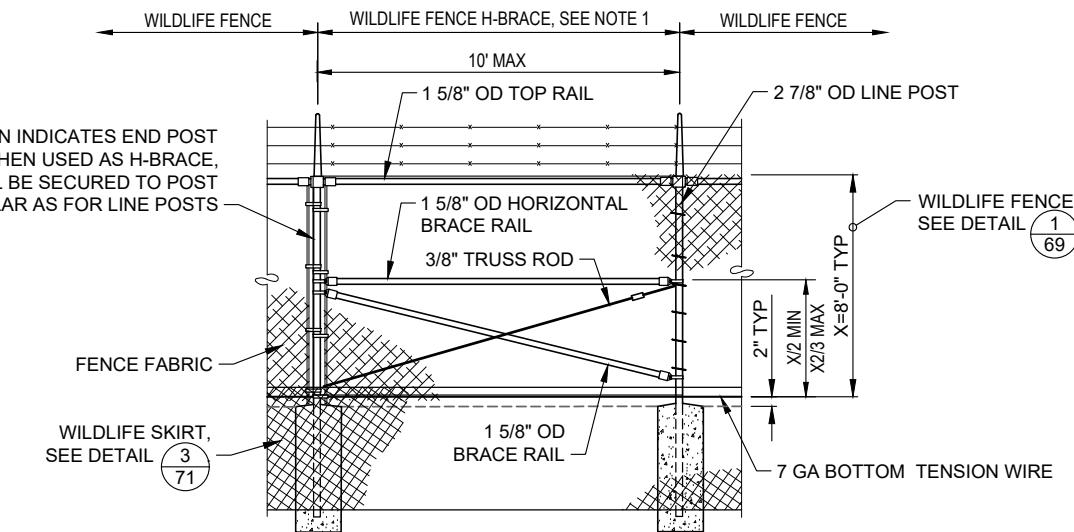
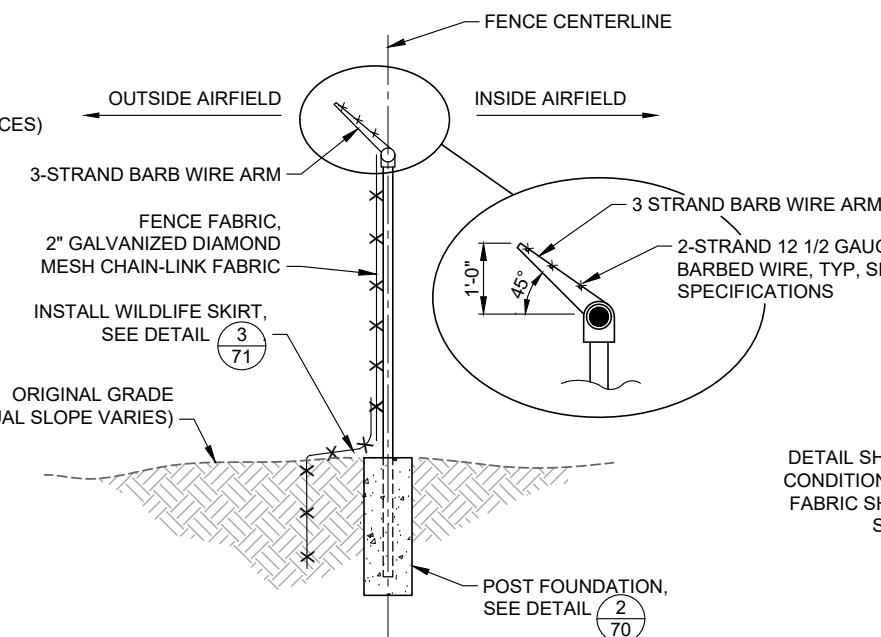
NOTES:

1. FOR CORNERS AND ANGLE POINTS, SEE DETAILS 2 69 AND 4 71.
2. H-BRACES TO BE PROVIDED AT ANGLE POINTS IN FENCE ALIGNMENT AND EITHER SIDE OF GATES.
3. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE-COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.
4. CONTRACTOR SHALL FIELD STAKE PROPOSED FENCE ALIGNMENT AND PROPERTY LINE PRIOR TO CONSTRUCTION. FENCE ALIGNMENT ADJUSTMENTS MAY BE REQUIRED, VERIFY WITH ENGINEER.

WILDLIFE FENCE DETAIL

NTS

1  
69



NOTES:

1. ALL BRACING, HARDWARE, AND INCIDENTALS NECESSARY TO INSTALL H-BRACE AS SHOWN AND REQUIRED SHALL BE PAID UNDER THE DOUBLE SWING GATE AND FENCE LINE LUMP SUM BID ITEM.
2. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE-COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.

WILDLIFE FENCE H-BRACE DETAIL

NTS

2  
69

REPLACE ENTIRE SHEET



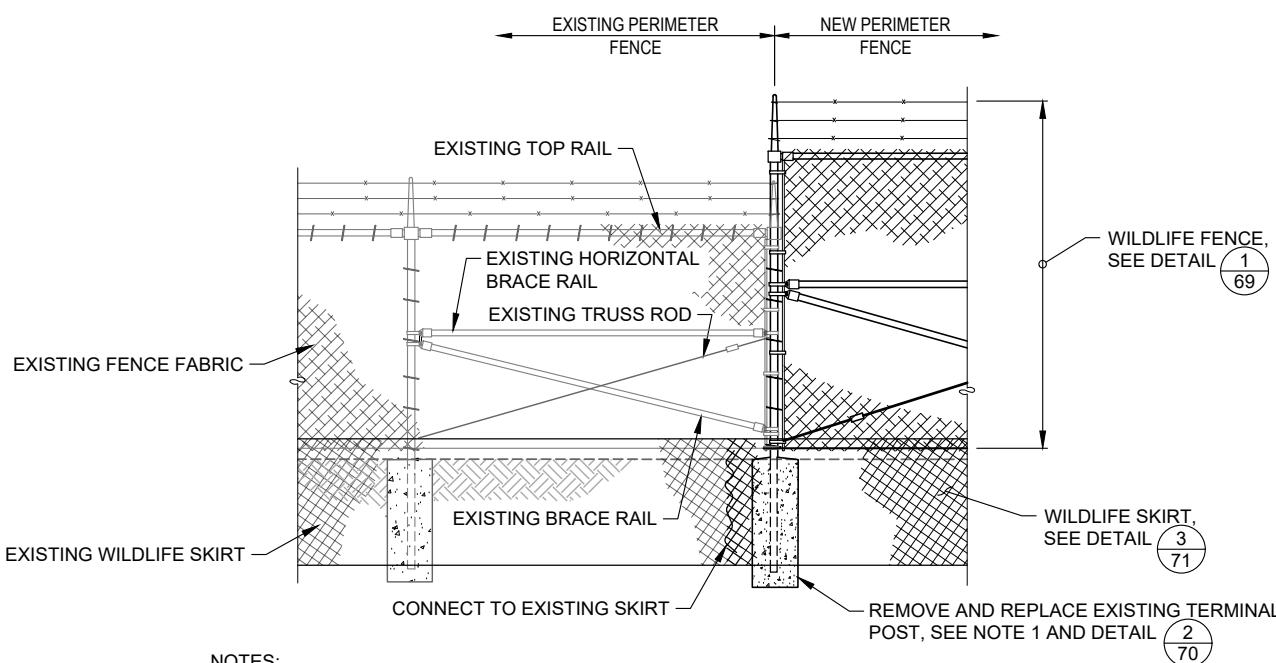
ADDENDUM NO. 1  
REVISIONS:

5/24/2024 JGL  
DATE APPD.

DATE: MAY 2024  
DESIGN: KJK  
DRAWN: JAW  
CHECKED: JGL  
REVISION NUMBER: 0  
SCALE: AS SHOWN

ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**FENCE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

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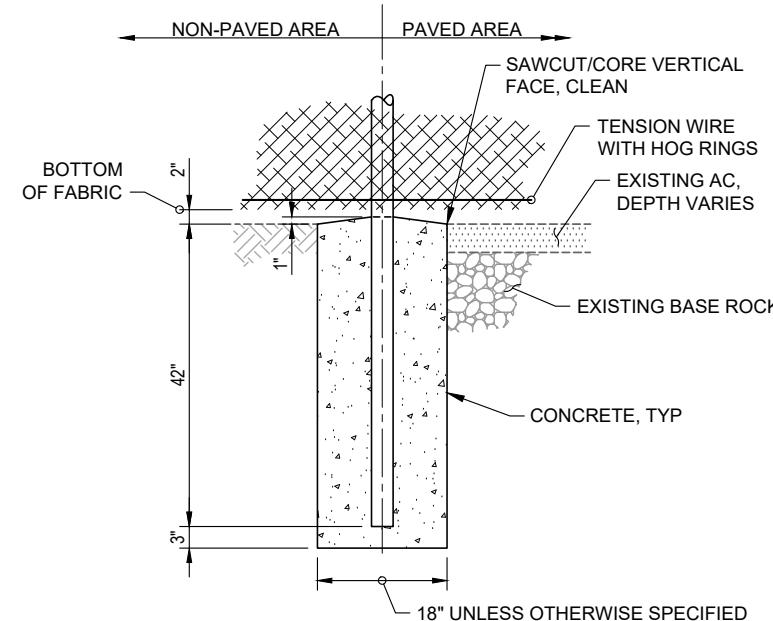
NOTES:

1. CONTRACTOR SHALL DETACH EXISTING FENCE FABRIC, BARB WIRE, AND BRACING IN ORDER TO REMOVE AND REPLACE EXISTING TERMINAL POST. REMOVAL OF EXISTING POST AND FOUNDATION SHALL BE PAID UNDER THE FENCE REMOVAL BID ITEM. REATTACHMENT OF EXISTING FENCE MATERIAL SHALL BE PAID UNDER THE DOUBLE SWING GATE LUMP SUM BID ITEM.

CONNECTION TO EXISTING FENCE DETAIL

NTS

1  
70



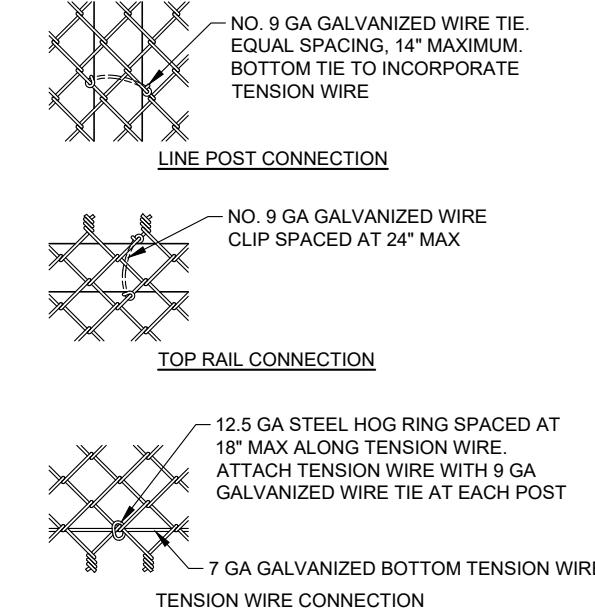
NOTES:

1. A SONOTUBE MAY BE REQUIRED FOR CONCRETE FORMING IN UNEVEN TERRAIN CONDITIONS, INCIDENTAL.
2. IN AREAS OF PAVEMENT, CONTRACTOR SHALL SAWCUT AND REMOVE EXISTING PAVEMENT PRIOR TO FOUNDATION EXCAVATION. FINISH CONCRETE FLUSH WITH AC SURFACE, INCIDENTAL.

POST FOUNDATION DETAIL

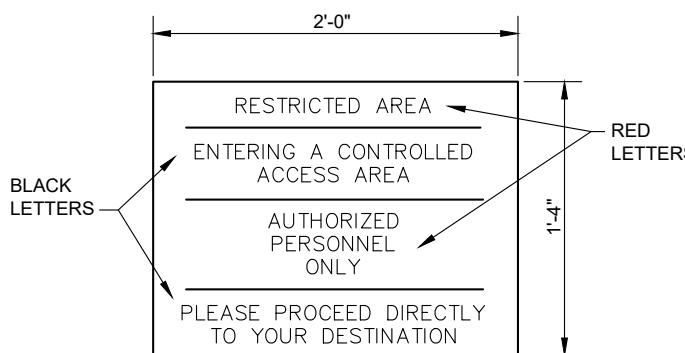
NTS

2  
70



CONNECTION DETAIL  
NTS

3  
70



NOTES:

1. VERIFY COLOR SCHEME WITH ENGINEER.
2. FASTEN SIGN SECURELY TO THE GATE, LOCATION TO BE DETERMINED BY THE ENGINEER.
3. FOR SIGN ATTACHMENT, SEE DETAIL 6 70

AUTOMATED GATE SIGN DETAIL

NTS

4  
70

34C

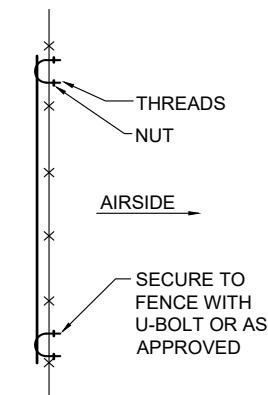
NOTES:

1. ONE SIGN TO BE LOCATED ON EACH SIDE OF EVERY VEHICLE GATE.
2. SEE ENGINEER FOR GATE NUMBERS.
3. MATCH EXISTING SIGN SIZE AND COLORS.
4. FOR SIGN ATTACHMENT, SEE DETAIL 6 70

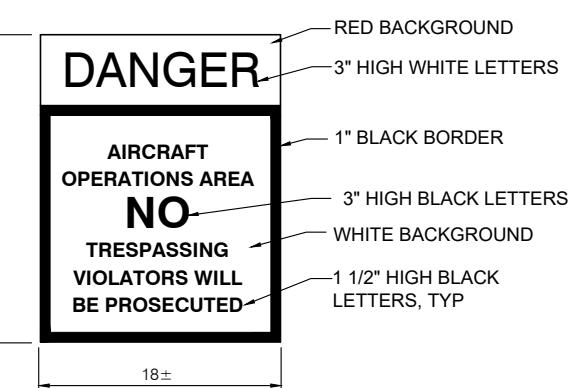
GATE NUMERAL SIGN DETAIL

NTS

5  
70



SIDE VIEW



FACE VIEW

NOTES:

1. VERIFY SIGN DIMENSIONS WITH SIGN MANUFACTURER.
2. SIGN TO BE PLACED ON ALL GATES AND AT A MAXIMUM OF 100' INTERVALS ALONG FENCE.
3. INSTALLATION WITH VANDAL PROOF HARDWARE PER MANUFACTURERS RECOMMENDATIONS, AS APPROVED BY THE ENGINEER.

FENCE SIGN DETAIL  
NTS

6  
70

REPLACE ENTIRE SHEET

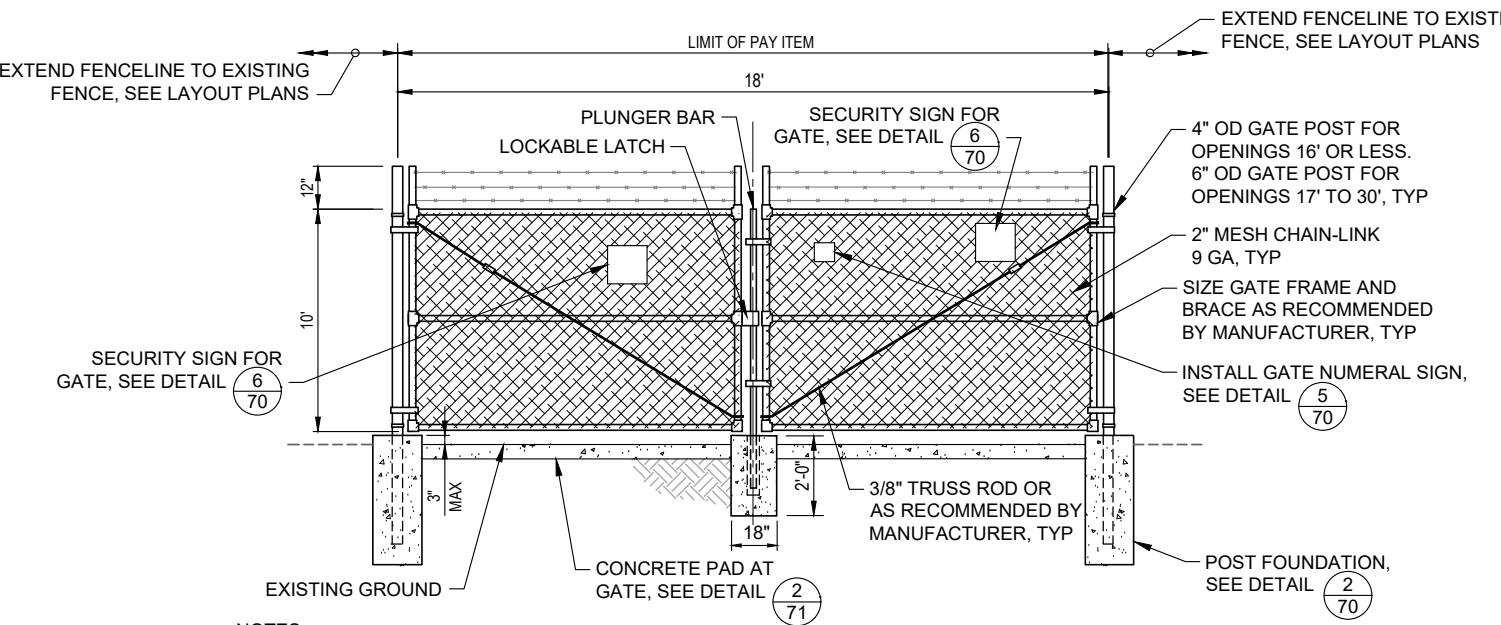


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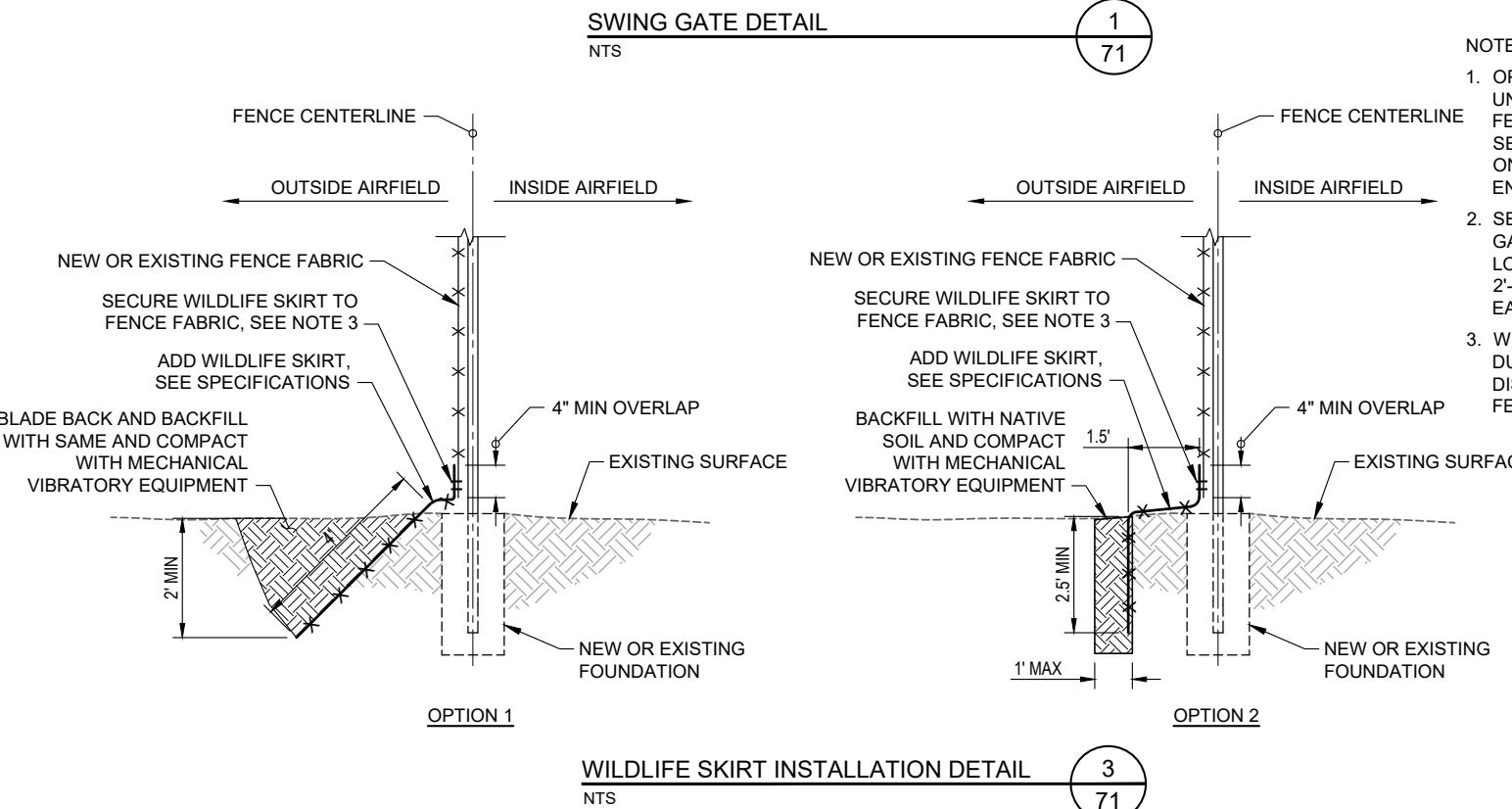
ROGUE VALLEY INTERNATIONAL - MEDFORD TAXIWAY C EXTENSION AND NORTHWEST TAXILANES  
**FENCE DETAILS**  
PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

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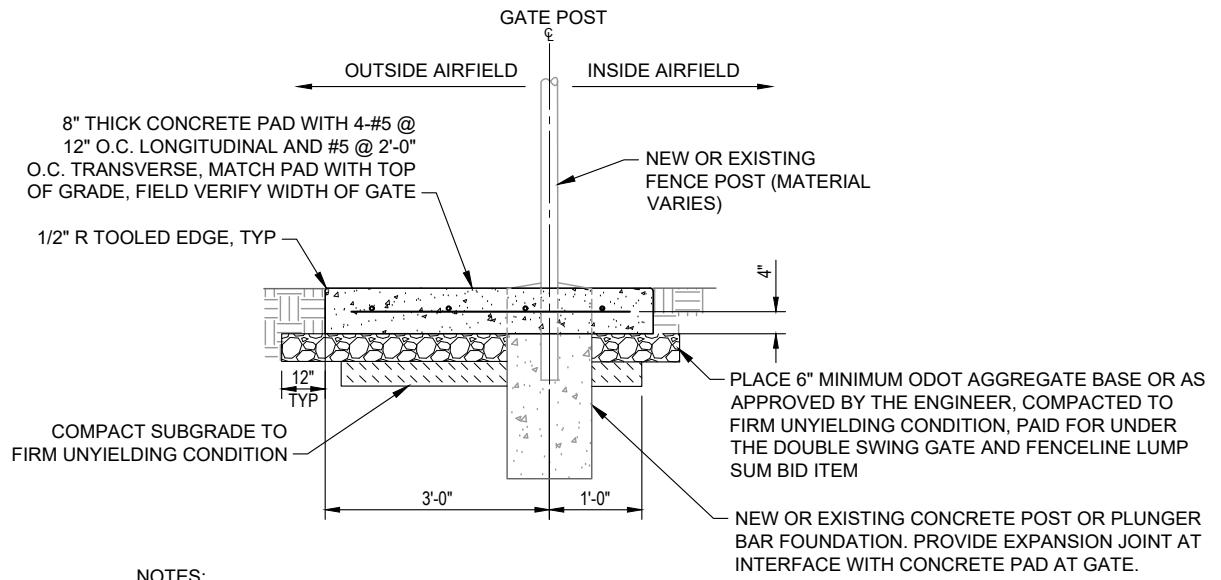
## NOTES:

1. HEAVY DUTY LOCK MANUFACTURED BY BEST LOCK WITH 3 KEYS SHALL BE PROVIDED FOR EACH GATE INSTALLED, INCIDENTAL TO BID ITEM. PROVIDE A BEST LOCK 7 PIN CORE SERIES, COORDINATE CORE SERIES WITH OWNER PRIOR TO ORDERING. PROVIDE H- BRACE SECTION EITHER SIDE OF GATE, SEE DETAIL 2 69
2. CONTRACTOR SHALL GRADE A SMOOTH TRANSITION COMPATIBLE WITH THE GATE ENTRANCE FOR THE COMPLETE SWING ARC IN THE OPENING DIRECTION IN NONPAVED AREAS. ADDITIONAL MATERIALS AND WORK SHALL BE PAID FOR UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM.
3. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE -COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.



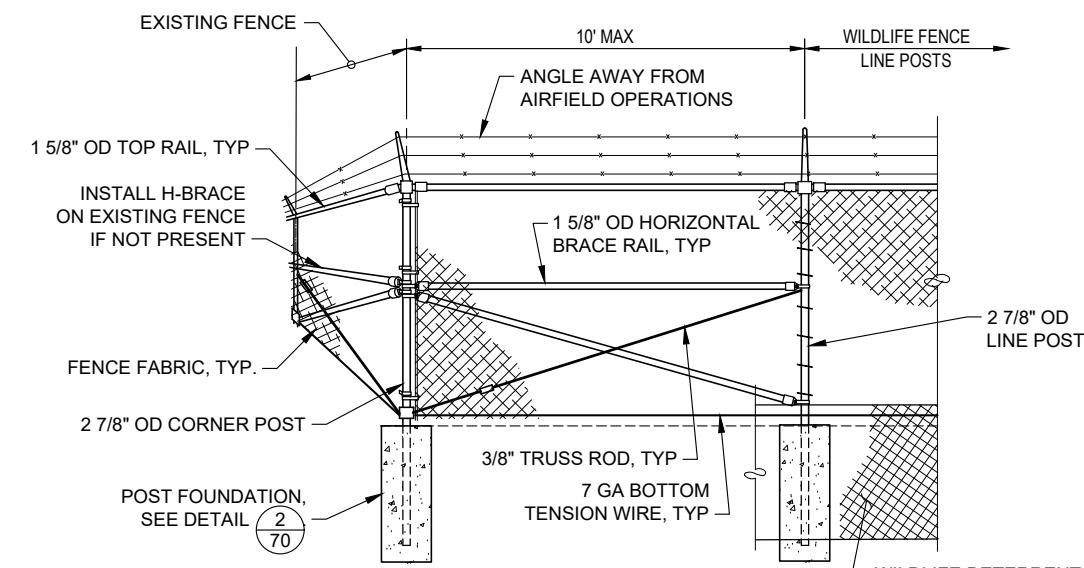
## NOTES:

1. OPTION 1 AND 2 INSTALLATION TO BE PAID UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM. OPTION SELECTION SHALL BE DETERMINED BASED ON LOCAL SITE CONDITIONS, VERIFY WITH ENGINEER PRIOR TO CONSTRUCTION.
2. SECURE WILDLIFE SKIRT TO FENCE USING 9 GA GALVANIZED STEEL HOG RINGS AT LOWEST FENCE POINT. ATTACH FABRIC 2'-0" O.C. ALONG FENCE AND WITHIN 2" EACH SIDE OF POSTS.
3. WILDLIFE SKIRT FABRIC SHALL BE REMOVED DURING FENCE REMOVAL OPERATIONS AND DISPOSED OF OFFSITE, PAID UNDER THE FENCE REMOVAL BID ITEM.



## NOTES:

1. GATE CONCRETE PAD TO BE INSTALLED ON ALL NEW GATES. ALL WORK, MATERIALS AND INCIDENTALS SHALL BE PAID FOR UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM.

CONCRETE PAD AT GATE DETAIL  
NTS2  
71

## NOTES:

1. ALL POSTS, BRACING, HARDWARE, CHAIN-LINK FABRIC, CONCRETE, AND INCIDENTALS NECESSARY TO PROVIDE ITEM AS REQUIRED SHALL BE PAID FOR UNDER THE DOUBLE SWING GATE AND FENCELINE LUMP SUM BID ITEM.
2. ALL WELDED METAL-TO-METAL CONNECTIONS SHALL BE WIRE BRUSHED AND DOUBLE-COAT SPRAY GALVANIZED USING COLD GALVANIZING COMPOUND APPLIED TO THE HEATED SURFACE.

WILDLIFE FENCE CORNER DETAIL  
NTS - SOME ITEMS NOT SHOWN FOR CLARITY4  
71

REPLACE ENTIRE SHEET



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ROGUE VALLEY INTERNATIONAL - MEDFORD  
TAXIWAY C EXTENSION AND NORTHWEST TAXILANES

**GATE DETAILS**

PRECISION APPROACH ENGINEERING, INC.  
AIP NO. 3-41-0037-063

PROJECT NUMBER:  
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